REV NÓ I APPROVED BY GUARD/

REV NO I APPROVED BY BUREAU JOF EXPLOSIVES A. F. Grasomuck

LOADING AND BRACING IN MILVAN CONTAINERS[⊕] OF PALLETIZED 2.75" ROCKET (4 PER 63" LONG BOX) (15-BOX PALLE

LOADING AND BRACING SPECIFICATIONS SET FORTH WITHIN THIS DRAWING ARE APPLICABLE TO LOADS THAT ARE TO BE SHIPPED BY TRAILER/CONTAINER-ON-FLAT-CAR (T/COFC) RAIL CARRIER SERVICE. THESE SPECIFICATIONS MAY ALSO BE USED FOR LOADS THAT ARE TO BE MOVED BY MOTOR OR WATER OR AIR CARRIERS. SEE THE "SPECIAL T/COFC NOTES" BELOW.

ONLY MILVAN CONTAINERS WHICH HAVE BEEN MODIFIED TO INCLUDE A MECHANICAL LOAD-BRACING SYSTEM THAT SATISFIES THE REQUIREMENTS OF THE BUREAU OF EXPLOSIVES PAMPHLET 6C WILL BE USED FOR THE MOVEMENT OF AMMUNITION BY T/COFC SERVICE. CAUTION: OTHER REQUIREMENTS OF PAMPHLET 6C ALSO APPLY.

MAXIMUM LOAD WEIGHT CRITERIA:

THE ITEMIZED LOAD WEIGHTS ARE CONTROLLED BY EQUIPMENT CAPABILITY FACTORS. THE ITEMIZED LOAD WEIGHTS ARE CONTROLLED BY EQUIPMENT CAPABILITY FACTORS.
ALSO, THESE LISTED LOAD WEIGHTS IDENTIFY THE MAXIMUM COMBINED WEIGHT OF
AMMUNITION LADING UNITS AND DUNNAGE THAT CAN BE PLACED INTO ONE (1)
MILYAN CONTAINER WITHOUT VIOLATING ONE OR MORE OF THE "CAPABILITY FACTORS". SEE NOTES 1 AND 2.

39,100 LBS IN 20-FT CONTAINER (W/O CHASSIS) ABOARD CONTAINERSHIP.
39,100 LBS IN CONTAINER ON 20-FT CHASSIS WITH DOUBLE BOGIE. SEE NOTE 3.
25,300 LBS IN CONTAINER ON 20-FT CHASSIS WITH SINGLE BOGIE. SEE NOTE 4.
21,300 LBS IN EACH CONTAINER ON 40-FT CHASSIS (COUPLED WITH DOUBLE 1)

BOGIE), SEE NOTE 3.

19,300 LBS IN 20-FT CONTAINER (W/O CHASSIS) ABOARD FIXED-WING AIRCRAFT.

39,100 LBS IN 20-FT CONTAINER (W/O CHASSIS) FOR ROTARY-WING AIRCRAFT. SEE NOTE 5.

NOTE 1: DUNNAGE INCLUDES MATERIALS, OTHER THAN COMPONENTS OF THE MECHANICAL LOAD-BRACING SYSTEM, USED TO BLOCK AND BRACE A LOAD.

NOTE 2: ALTHOUGH THE HEAVIEST MAXIMUM LOAD IS DELINEATED ON PAGES 2 AND 3, PROVISIONS ARE INCLUDED WITHIN THIS DRAWING SO THAT THE BASIC LOAD CAN BE ADJUSTED TO SATISFY A LESSER QUANTITY OF LADING UNITS. ADDITIONAL INSTRUCTIONS ARE UNDER THE "REDUCED-LOAD PROVISIONS"SECTION ON PAGE 2.

MOTE 3: DEPENDING ON TRANSPORTATION ROUTING, IT MAY BE NECESSARY TO REDUCE THE LOAD WEIGHT TO SATISFY "WEIGHT LAWS" OF CERTAIN STATES. ALSO, IT MAY BE NECESSARY TO REDUCE THE LOAD WEIGHT TO SATISFY OTHER WEIGHT RESTRIC-TIONS IMPOSED ON THE MILVAN SYSTEM.

BY SPECIAL AUTHORITY, IT MAY BE POSSIBLE TO MOVE HEAVIER LOADS ON NOTE 4: BY SPECIAL AUTHORITY, II MAY BE FOSSI SINGLE BOGIE CHASSIS WITHIN AN INSTALLATION.

NOTE 5: IT WILL BE NECESSARY TO REDUCE WEIGHT OF SOME LOADS TO BE MOVED BY ROTARY-WING AIRCRAFT, DEPENDING ON "LIFT" CAPABILITY OF THE SCHEDULED AIRCRAFT.

> THIS DRAWING, INCLUDING REVISION I, SUPERSEDES INTERIM PROCEDURAL DRAWINGS D-AMXSV-4239, DATED JUNE 1971, AND D-AMXSV-4290, DATED JUNE 1971.

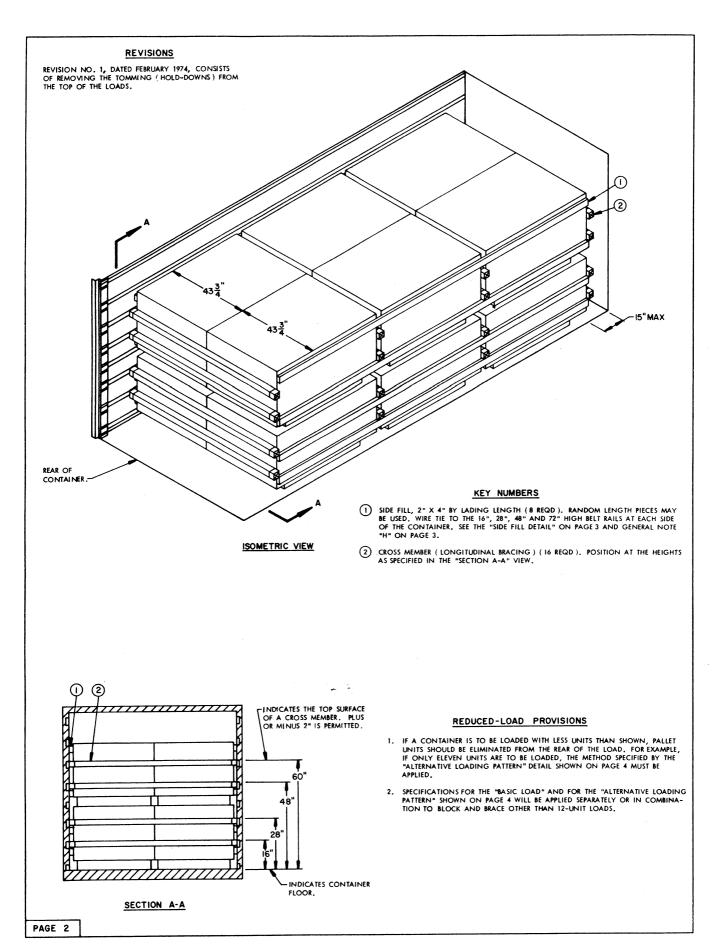
> > DO NOT SCALE

SPECIAL T/COFC NOTES:

- A. CAUTION: LOADED CONTAINERS MUST BE ON CHASSIS EQUIPPED WITH TWO BOGIE
 ASSEMBLIES WHEN BEING MOVED IN TOFC SERVICE, REGARDLESS OF LOAD WEIGHT
 WITHIN THE CONTAINERS.
- B. LOAD LIMITS OF T/COFC RAIL CARS MUST NOT BE EXCEEDED, NOR WILL A CAR
 BE LOADED SO THAT THE TRUCK UNDER ONE END OF THE CAR CARRIES MORE THAN ONE-HALF OF THE LOAD LIMIT FOR THAT CAR.
- C. CHASSIS CONTAINERS COUPLED INTO A 40-FOOT TRAILER CONFIGURATION MUST BE PLACED AT THE B-END OF A TOFC RAIL CAR. THE REAR END OF THE 40-FOOT UNIT WILL OVER-HANG THE END OF THE CAR IF IT IS PLACED AT THE A-END. TWENTY-FOOT AND 40-FOOT UNITS CAN BE LOADED ON THE SAME CAR.

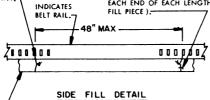
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PROJECT FSA 124A-71



TIE WIRE, NO. 14 GAGE WIRE 18 LONG. WIRE TO FORM A COMPLETE LOOP THRU HOLE IN BELT RAIL AND AROUND FILL PIECE, BRING THE ENDS TOGETHER AND TWIST TAUT. RE-QUIRED NEAR EACH END OF A FILL PIECE AND EVERY 48" OF FILL PIECE LENGTH AS SHOWN.

RETAINER NAIL, PARTIALLY DRIVEN 10d NAIL BENT OVER THE WIRE TO PREVENT LONGITUDINAL MOVEMENT OF PIECE MARKED () (1 REQD NEAR EACH END OF EACH LENGTH OF SIDE



FILL PIECE, 2" X 4" MATERIAL (8 REQD). RANDOM LENGTH
PIECES MAY BE USED. REQUIRED
AT THE 16", 28", 48" AND 72"
HIGH BELT RAILS AT EACH SIDE OF THE CONTAINER. SEE GENERAL

(GENERAL NOTES CONTINUED)

- A STAGGERED NAILING PATTERN WILL BE USED WHEREVER POSSIBLE WHEN NAILS ARE DRIVEN INTO JOINTS OF DUNNAGE ASSEMBLIES OR WHEN LAMINATING DUNNAGE. ADDITIONALLY, THE NAILING PATTERN FOR AN UPPER PIECE OF LAMINATED DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN THROUGH ONTO OR RIGHT BESIDE A NAIL IN A LOWER PIECE.
- PORTIONS OF THE CONTAINER DEPICTED WITHIN THIS DRAWING, SUCH AS ONE OF THE SIDE WALLS, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.

	BILL OF MATERIAL			
LUMBER	LINEAR FEET	BOARD FEET		
2" × 4"	130	87		
NAILS	NO. REQD	POUNDS		
10d (3")	16	1/4		
WIRE, NO. 14 GAGE	60' REQD	1 LB		
CROSS MEMBER		16 REQD		

MATERIAL SPECIFICATIONS

LUMBER ----- : SEE TM 743-200-1, DUNNAGE LUMBER; FED SPEC MM-L-751.

NAILS -----: COMMON, CEMENT COATED OR CHEMICALLY ETCHED; FED SPEC FF-N-105.

ALT: ANNULAR-RING TYPE NAIL OF THE SAME SIZE.

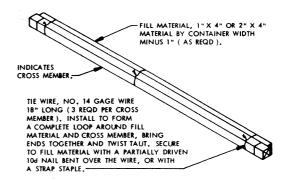
WIRE ----- : FED SPEC QQ-W-461.

STAPLE, STRAP : COMMERCIAL GRADE.

GENERAL NOTES

- THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AMCR 740-13 AND AUGMENTS TM 743-200-1 (CHAPTER 5).
- THE SPECIFIED OUTLOADING PROCEDURES ARE APPLICABLE TO THE 15-BOX PALLET UNIT OF 2.75" ROCKET, WHEN PACKED FOUR ROCKETS PER 63" LONG WOODEN BOX. SUBSEQUENT REFERENCE TO PALLET UNIT MEANS THE PALLET WITH AMMU-NITION ITEMS. SEE PAGE 4 FOR DETAIL OF PALLET UNIT, CAUTION: REGARDLESS OF THE QUANTITY OF UNITS TO BE SHIPPED, THE "MAXIMUM GROSS WEIGHT" OF 44.800 POUNDS MUST NOT BE EXCEEDED.
- THE LOAD AS SHOWN'IS BASED ON A 20' LONG BY 8' WIDE BY 8' HIGH MILVAN CONTAINER WITH INSIDE DIMENSIONS OF 19'-4" LONG BY 92" WIDE BY 87" HIGH. THE LOAD IS DESIGNED FOR TRAILER/CONTAINER-ON-FLAT-CAR (T/COFC) SHIP-MENT.
- THE SPECIFIED OUTLOADING PROCEDURES ARE FOR CONTAINERS EQUIPPED WITH THE SPECIFIED OUTLOADING PROCEDURES ARE FOR CONTAINERS EQUIPPED WITH SELF-CONTAINED MECHANICAL BRACING DEVICES AS DESCRIBED WITHIN BUREAU OF EXPLOSIVES PAMPHLET &C. CROSS MEMBER ATTACHMENT FACILITIES WITHIN THESE CONTAINERS MUST PROVIDE FOR THE INSTALLATION OF LOAD BLOCKING CROSS MEMBERS AT THE HEIGHTS SPECIFIED. THE HEIGHT DIMENSIONS SPECIFIED WITHIN THIS DRAWING FOR THE INSTALLATION OF CROSS MEMBERS CONFORM WITH BUREAU OF EXPLOSIVES PAMPHLET &C, WITH THE EXCEPTION THAT TWO (2) ADDITIONAL BELT RAILS HAVE BEEN SHOWN; ONE AT 72" AND ONE AT 83" HEIGHT FROM THE CONTAINER FLOOR. VOIDS LENGTHWISE WITHIN THE LOAD MUST BE HELD TO A MINIMUM. CROSS MEMBERS MUST BE PLACED AGAINST THE LADING AS TIGHTLY AS THE HOLE SPACING IN THE CROSS MEMBER ATTACHMENT FACILITY PREMITS FACH CROSS MEMBER WILL BE INSTALLED WITH THE ENDS LADING AS TIGHTLY AS THE HOLE SPACING IN THE CROSS MEMBER ATTACHMENT FACILITY PERMITS. EACH CROSS MEMBER WILL BE INSTALLED WITH THE ENDS ATTACHED AS NEARLY AS POSSIBLE IN "MATEO" POSITIONS (AT EQUAL HEIGHTS, AND AT EQUAL DISTANCES FROM THE END OF THE CONTAINER). CROSS MEMBERS IN EMPTY CONTAINERS AND THOSE NOT USED IN LOADED CONTAINERS MUST BE FASTENED INTO BELT RAILS FOR SHIPMENT. COMPONENTS ASSIGNED TO EACH CONTAINER MUST REMAIN THEREWITH EVEN THOUGH UNUSED DURING SOME SHIPMENTS. SEE THE "FILL DETAIL" BELOW FOR THE DUNNAGING METHOD REQUIRED TO ELIMINATE AN EXCESSIVE LENGTHWISE VOID WITHIN A LOAD. THE LOAD BLOCKING COMPONENT DESIGNATED AS "CROSS MEMBER" HEREIN, IS IDENTIFIED AS "BEAM ASSEMBLY" WITHIN TM 55-8115-200-24, DATED SEPTEMBER 1972. THE BEAM ASSEMBLY" WITHIN TENTH THE PROPERTY OF THE DENTIFIED AS "BEAM ASSEMBLY" WITHIN THE DENTIFIED AS FSN-8115-165-6623.
- DUNNAGE LUMBER SPECIFIED IS OF A NOMINAL SIZE. FOR EXAMPLE, 1" X 4" MATERIAL IS ACTUALLY 3/4" THICK BY 3-1/2" OR 3-5/8" WIDE AND 2" X 4" MATERIAL IS ACTUALLY 1-1/2" THICK BY 3-1/2" WIDE OR 1-5/8" THICK BY 3-5/8"
- $\underline{\text{CAUTION}}\colon$ DO NOT NAIL DUNNAGE MATERIAL TO THE CONTAINER WALLS OR FLOOR. ALL NAILING WILL BE WITHIN THE DUNNAGE.
- THE SPACER ASSEMBLY AS DETAILED ON PAGE 4 NEED NOT BE FABRICATED FOR A DRIVE FIT. THE ASSEMBLY SHOULD BE FARRICATED SO THAT IT CAN BE EASILY INSTALLED. HOWEVER, IT MUST FIT TIGHT ENOUGH SO AS TO NOT ALLOW MORE THAN ONE-HALF INCH (1/2") VOID ACROSS THE WIDTH OF A BRACED LOAD.
- THE THICKNESS OF SIDE FILL PIECES AS DEPICTED ON EACH SIDE OF THE LOAD MUST BE ADJUSTED, AS REQUIRED TO COMPLY WITH THE DIMENSIONAL VARIANCE OF THE PALLET UNIT, SO AS TO NOT ALLOW MORE THAN ONE AND ONE-HALF INCH (1-1/2") ACROSS THE WIDTH OF A BRACED LOAD. ADJUSTMENTS CAN BE MADE BY USING A DIFFERENT THICKNESS FILL PIECE OR BY LAMINATING ADDI-TIONAL PIECES TO THE SPECIFIED FILL PIECES ON ONE OR BOTH SIDES OF THE

(CONTINUED AT LEFT)



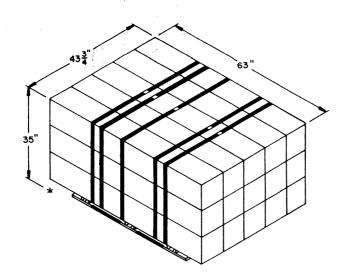
FILL DETAIL

THIS DETAIL DEPICTS METHOD OF POSITIONING FILL MATERIAL BETWEEN CROSS MEMBER AND LADING, WHEN THE VOID BETWEEN THE TWO IS GREATER THAN ONE INCH (1") FOR LONGHTUDINAL BRACING.

LOAD AS SHOWN

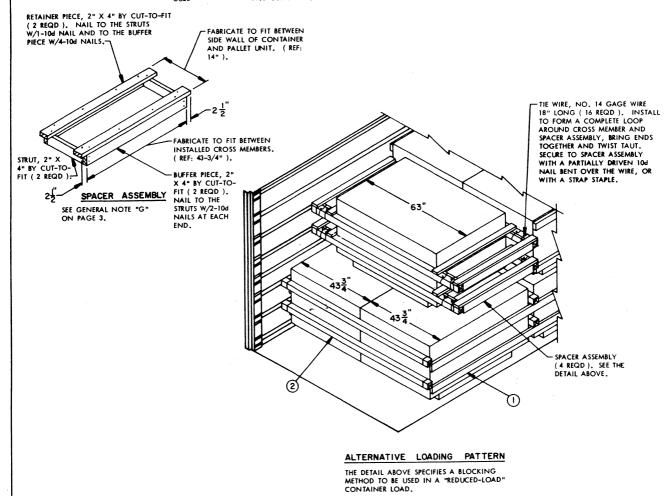
ITEM	QUANTITY	WE	IGHT	(APPROX)
PALLET	UNIT 12	-26,580	LBS	
DUNN	\GE	219	LBS	
CONTA	INER	5,700	LBS	
	TOTAL GROSS WEIGHT	32,499	LBS	

PAGE 3



PALLET UNIT

UNIT WEIGHT ----- 2,215 POUNDS (APPROX). CUBE ----- 55.8 CUBIC FEET.



PAGE 4