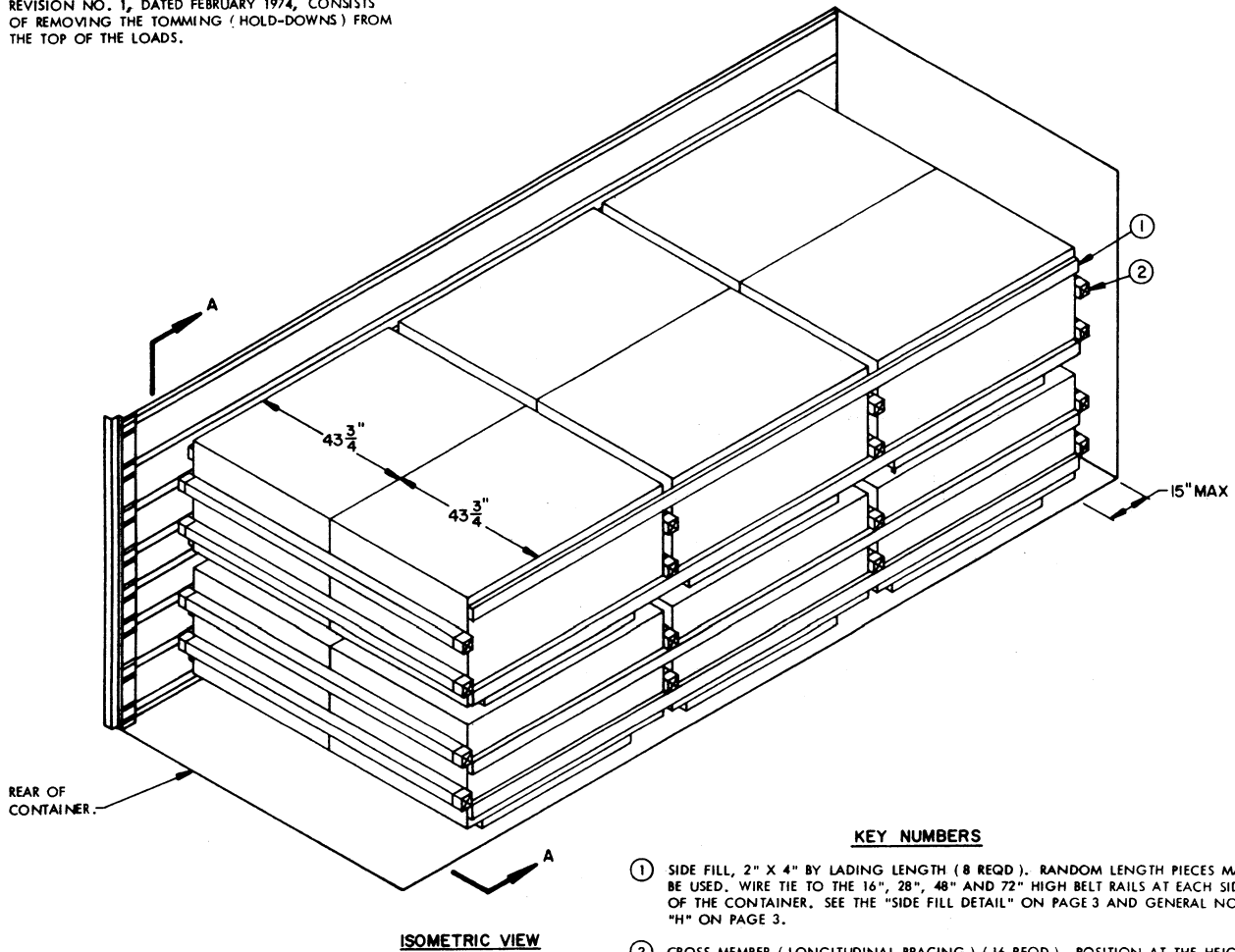


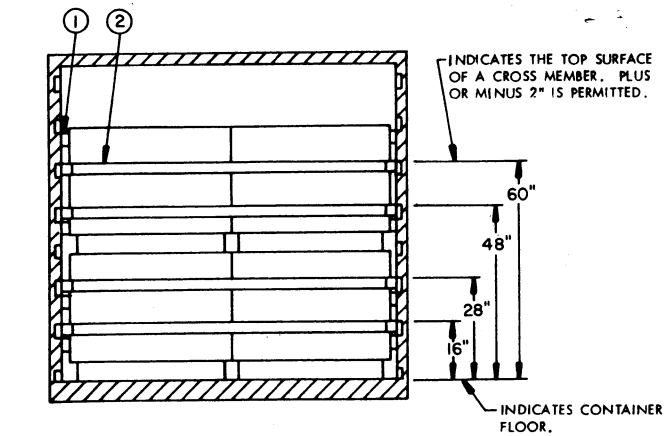
REVISIONS

REVISION NO. 1, DATED FEBRUARY 1974, CONSISTS OF REMOVING THE TOMMING (HOLD-DOWNS) FROM THE TOP OF THE LOADS.



KEY NUMBERS

- ① SIDE FILL, 2" X 4" BY LADING LENGTH (8 REQD). RANDOM LENGTH PIECES MAY BE USED. WIRE TIE TO THE 16", 28", 48" AND 72" HIGH BELT RAILS AT EACH SIDE OF THE CONTAINER. SEE THE "SIDE FILL DETAIL" ON PAGE 3 AND GENERAL NOTE "H" ON PAGE 3.
- ② CROSS MEMBER (LONGITUDINAL BRACING) (16 REQD). POSITION AT THE HEIGHTS AS SPECIFIED IN THE "SECTION A-A" VIEW.

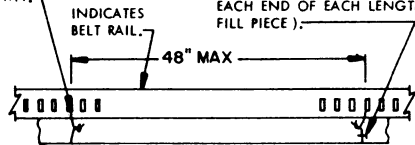


REDUCED-LOAD PROVISIONS

- 1. IF A CONTAINER IS TO BE LOADED WITH LESS UNITS THAN SHOWN, PALLET UNITS SHOULD BE ELIMINATED FROM THE REAR OF THE LOAD. FOR EXAMPLE, IF ONLY ELEVEN UNITS ARE TO BE LOADED, THE METHOD SPECIFIED BY THE "ALTERNATIVE LOADING PATTERN" DETAIL SHOWN ON PAGE 4 MUST BE APPLIED.
- 2. SPECIFICATIONS FOR THE "BASIC LOAD" AND FOR THE "ALTERNATIVE LOADING PATTERN" SHOWN ON PAGE 4 WILL BE APPLIED SEPARATELY OR IN COMBINATION TO BLOCK AND BRACE OTHER THAN 12-UNIT LOADS.

TIE WIRE, NO. 14 GAGE WIRE 18" LONG. WIRE TO FORM A COMPLETE LOOP THRU HOLE IN BELT RAIL AND AROUND FILL PIECE, BRING THE ENDS TOGETHER AND TWIST TAUT. REQUIRED NEAR EACH END OF A FILL PIECE AND EVERY 48" OF FILL PIECE LENGTH AS SHOWN.

RETAINER NAIL, PARTIALLY DRIVEN 10d NAIL BENT OVER THE WIRE TO PREVENT LONGITUDINAL MOVEMENT OF PIECE MARKED ① (1 REQD NEAR EACH END OF EACH LENGTH OF SIDE FILL PIECE).



SIDE FILL DETAIL

FILL PIECE, 2" X 4" MATERIAL (8 REQD). RANDOM LENGTH PIECES MAY BE USED. REQUIRED AT THE 16", 28", 48" AND 72" HIGH BELT RAILS AT EACH SIDE OF THE CONTAINER. SEE GENERAL NOTE "H" AT RIGHT.

(GENERAL NOTES CONTINUED)

- J. A STAGGERED NAILING PATTERN WILL BE USED WHEREVER POSSIBLE WHEN NAILS ARE DRIVEN INTO JOINTS OF DUNNAGE ASSEMBLIES OR WHEN LAMINATING DUNNAGE. ADDITIONALLY, THE NAILING PATTERN FOR AN UPPER PIECE OF LAMINATED DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN THROUGH ONTO OR RIGHT BESIDE A NAIL IN A LOWER PIECE.
- K. PORTIONS OF THE CONTAINER DEPICTED WITHIN THIS DRAWING, SUCH AS ONE OF THE SIDE WALLS, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.

GENERAL NOTES

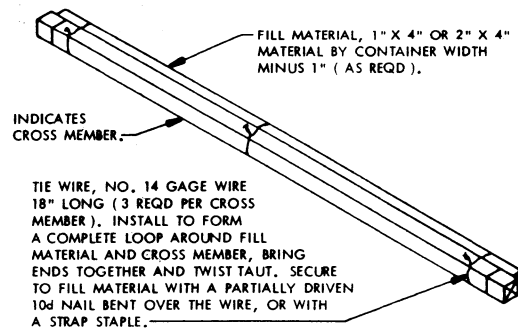
- A. THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AMCR 740-13 AND AUGMENTS TM 743-200-1 (CHAPTER 5).
- B. THE SPECIFIED OUTLOADING PROCEDURES ARE APPLICABLE TO THE 15-BOX PALLET UNIT OF 2.75" ROCKET, WHEN PACKED FOUR ROCKETS PER 63" LONG WOODEN BOX. SUBSEQUENT REFERENCE TO PALLET UNIT MEANS THE PALLET WITH AMMUNITION ITEMS. SEE PAGE 4 FOR DETAIL OF PALLET UNIT. **CAUTION:** REGARDLESS OF THE QUANTITY OF UNITS TO BE SHIPPED, THE "MAXIMUM GROSS WEIGHT" OF 44,800 POUNDS MUST NOT BE EXCEEDED.
- C. THE LOAD AS SHOWN IS BASED ON A 20' LONG BY 8' WIDE BY 8' HIGH MILVAN CONTAINER WITH INSIDE DIMENSIONS OF 19'-4" LONG BY 92" WIDE BY 87" HIGH. THE LOAD IS DESIGNED FOR TRAILER/CONTAINER-ON-FLAT-CAR (T/COFC) SHIPMENT.
- D. THE SPECIFIED OUTLOADING PROCEDURES ARE FOR CONTAINERS EQUIPPED WITH SELF-CONTAINED MECHANICAL BRACING DEVICES AS DESCRIBED WITHIN BUREAU OF EXPLOSIVES PAMPHLET 6C. CROSS MEMBER ATTACHMENT FACILITIES WITHIN THESE CONTAINERS MUST PROVIDE FOR THE INSTALLATION OF LOAD BLOCKING CROSS MEMBERS AT THE HEIGHTS SPECIFIED. THE HEIGHT DIMENSIONS SPECIFIED WITHIN THIS DRAWING FOR THE INSTALLATION OF CROSS MEMBERS CONFORM WITH BUREAU OF EXPLOSIVES PAMPHLET 6C, WITH THE EXCEPTION THAT TWO (2) ADDITIONAL BELT RAILS HAVE BEEN SHOWN; ONE AT 72" AND ONE AT 83" HEIGHT FROM THE CONTAINER FLOOR. VOIDS LENGTHWISE WITHIN THE LOAD MUST BE HELD TO A MINIMUM. CROSS MEMBERS MUST BE PLACED AGAINST THE LADING AS TIGHTLY AS THE HOLE SPACING IN THE CROSS MEMBER ATTACHMENT FACILITY PERMITS. EACH CROSS MEMBER WILL BE INSTALLED WITH THE ENDS ATTACHED AS NEARLY AS POSSIBLE IN "MATED" POSITIONS (AT EQUAL HEIGHTS, AND AT EQUAL DISTANCES FROM THE END OF THE CONTAINER). CROSS MEMBERS IN EMPTY CONTAINERS AND THOSE NOT USED IN LOADED CONTAINERS MUST BE FASTENED INTO BELT RAILS FOR SHIPMENT. COMPONENTS ASSIGNED TO EACH CONTAINER MUST REMAIN THEREWITH EVEN THOUGH UNUSED DURING SOME SHIPMENTS. SEE THE "FILL DETAIL" BELOW FOR THE DUNNAGING METHOD REQUIRED TO ELIMINATE AN EXCESSIVE LENGTHWISE VOID WITHIN A LOAD. THE LOAD BLOCKING COMPONENT DESIGNATED AS "CROSS MEMBER" HEREIN, IS IDENTIFIED AS "BEAM ASSEMBLY" WITHIN TM 55-8115-200-24, DATED SEPTEMBER 1972. THE BEAM ASSEMBLY IS FURTHER IDENTIFIED AS FSN-8115-165-6623.
- E. DUNNAGE LUMBER SPECIFIED IS OF A NOMINAL SIZE. FOR EXAMPLE, 1" X 4" MATERIAL IS ACTUALLY 3/4" THICK BY 3-1/2" OR 3-5/8" WIDE AND 2" X 4" MATERIAL IS ACTUALLY 1-1/2" THICK BY 3-1/2" WIDE OR 1-5/8" THICK BY 3-5/8" WIDE.
- F. **CAUTION:** DO NOT NAIL DUNNAGE MATERIAL TO THE CONTAINER WALLS OR FLOOR. ALL NAILING WILL BE WITHIN THE DUNNAGE.
- G. THE SPACER ASSEMBLY AS DETAILED ON PAGE 4 NEED NOT BE FABRICATED FOR A DRIVE FIT. THE ASSEMBLY SHOULD BE FABRICATED SO THAT IT CAN BE EASILY INSTALLED. HOWEVER, IT MUST FIT TIGHT ENOUGH SO AS TO NOT ALLOW MORE THAN ONE-HALF INCH (1/2") VOID ACROSS THE WIDTH OF A BRACED LOAD.
- H. THE THICKNESS OF SIDE FILL PIECES AS DEPICTED ON EACH SIDE OF THE LOAD MUST BE ADJUSTED, AS REQUIRED TO COMPLY WITH THE DIMENSIONAL VARIANCE OF THE PALLET UNIT, SO AS TO NOT ALLOW MORE THAN ONE AND ONE-HALF INCH (1-1/2") ACROSS THE WIDTH OF A BRACED LOAD. ADJUSTMENTS CAN BE MADE BY USING A DIFFERENT THICKNESS FILL PIECE OR BY LAMINATING ADDITIONAL PIECES TO THE SPECIFIED FILL PIECES ON ONE OR BOTH SIDES OF THE LOAD.

(CONTINUED AT LEFT)

BILL OF MATERIAL		
LUMBER	LINEAR FEET	BOARD FEET
2" X 4"	130	87
NAILS	NO. REQD	POUNDS
10d (3")	16	1/4
WIRE, NO. 14 GAGE	60' REQD	1 LB
CROSS MEMBER		16 REQD

MATERIAL SPECIFICATIONS

- LUMBER** ----- : SEE TM 743-200-1, DUNNAGE LUMBER; FED SPEC MM-L-751.
- NAILS** ----- : COMMON, CEMENT COATED OR CHEMICALLY ETCHED; FED SPEC FF-N-105.
ALT: ANNULAR-RING TYPE NAIL OF THE SAME SIZE.
- WIRE** ----- : FED SPEC QQ-W-461.
- STAPLE, STRAP** : COMMERCIAL GRADE.

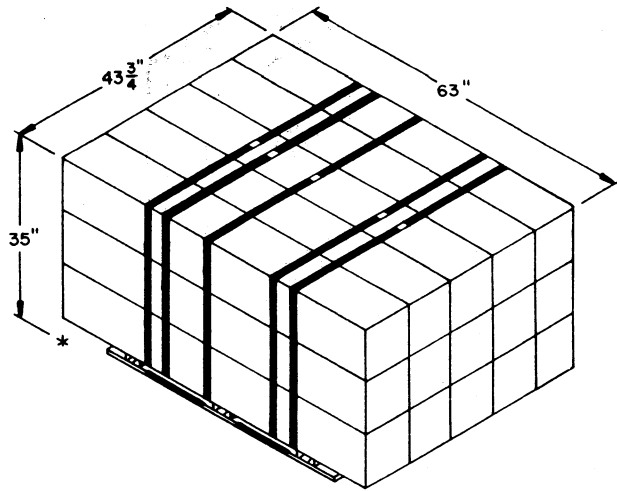


FILL DETAIL

THIS DETAIL DEPICTS METHOD OF POSITIONING FILL MATERIAL BETWEEN CROSS MEMBER AND LADING, WHEN THE VOID BETWEEN THE TWO IS GREATER THAN ONE INCH (1") FOR LONGITUDINAL BRACING.

LOAD AS SHOWN

ITEM	QUANTITY	WEIGHT (APPROX)
PALLET UNIT	12	26,580 LBS
DUNNAGE		219 LBS
CONTAINER		5,700 LBS
TOTAL GROSS WEIGHT		32,499 LBS

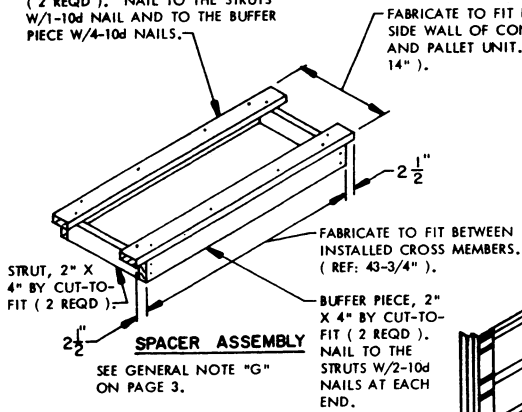


PALLET UNIT

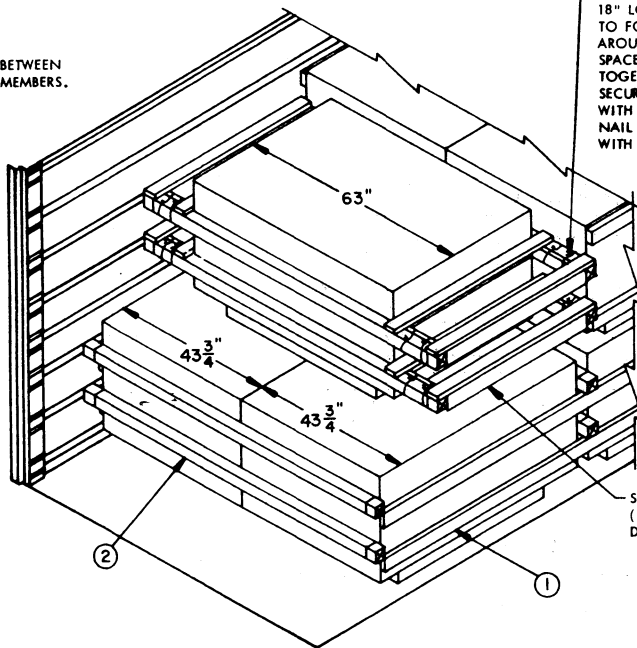
UNIT WEIGHT----- 2,215 POUNDS (APPROX).
 CUBE ----- 55.8 CUBIC FEET.

RETAINER PIECE, 2" X 4" BY CUT-TO-FIT (2 REQD). NAIL TO THE STRUTS W/1-10d NAIL AND TO THE BUFFER PIECE W/4-10d NAILS.

FABRICATE TO FIT BETWEEN SIDE WALL OF CONTAINER AND PALLET UNIT. (REF: 14").



TIE WIRE, NO. 14 GAGE WIRE 18" LONG (16 REQD). INSTALL TO FORM A COMPLETE LOOP AROUND CROSS MEMBER AND SPACER ASSEMBLY, BRING ENDS TOGETHER AND TWIST TAUT. SECURE TO SPACER ASSEMBLY WITH A PARTIALLY DRIVEN 10d NAIL BENT OVER THE WIRE, OR WITH A STRAP STAPLE.



ALTERNATIVE LOADING PATTERN

THE DETAIL ABOVE SPECIFIES A BLOCKING METHOD TO BE USED IN A "REDUCED-LOAD" CONTAINER LOAD.