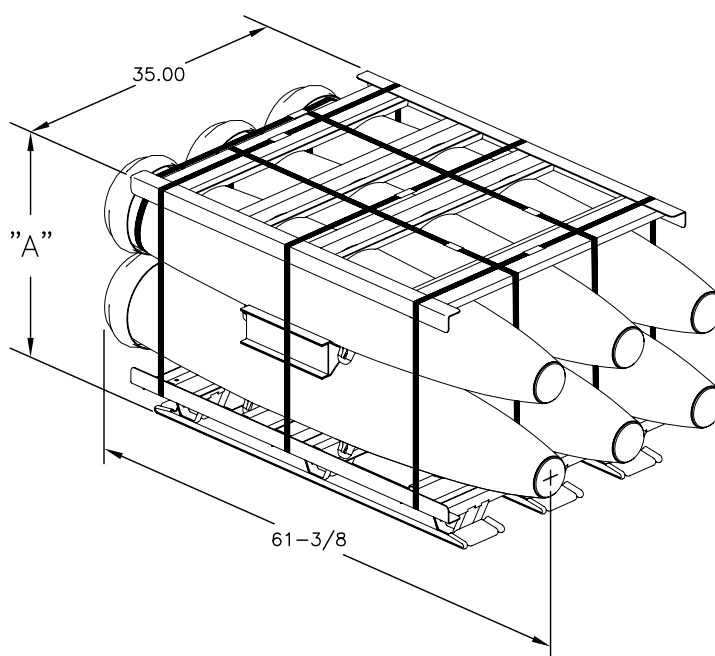


TRUCKLOAD

BOMB, GENERAL PURPOSE: 500 LB. MK 82 SERIES, BLU-III SERIES, BLU-I26 SERIES, BDU-45 SERIES, AND BDU-50 SERIES ON MHU-I22 SERIES PALLET (ADL 623ASI00)

| UNIT LOAD DATA | COATED | UNCOATED |
|-------------------------|-------------------------|--------------------|
| UNIT LOAD DOCUMENT..... | NAVSEA DWG 6214275..... | NAVSEA DWG 6214275 |
| DIMENSIONS..... | SEE ILLUSTRATION..... | SEE ILLUSTRATION |
| GROSS WEIGHT..... | 3,149 LBS..... | 3,149 LBS |
| CUBE..... | 36.1 CU-FT..... | 35.4 CU-FT |



| | | "A" HEIGHT |
|------------------|--|------------|
| 4, 5, OR 6 BOMBS | THERMALLY PROTECTED BOMB OR THERMAL PROTECTION REMOVED | 29 |
| | NON-THERMALLY PROTECTED BOMB | 28-1/2 |
| 1, 2, OR 3 BOMBS | THERMALLY PROTECTED BOMB OR THERMAL PROTECTION REMOVED | 17-1/2 |
| | NON-THERMALLY PROTECTED BOMB | 17 |

NOTES:

- GROSS WEIGHT IS ESTIMATED ONLY. DO NOT USE FOR SHIPPING WEIGHT.
- UNLESS OTHERWISE SPECIFIED, DIMENSIONS ARE IN INCHES.
- SEE SW020-AC-SAF-010 FOR THE FOLLOWING INFORMATION:
 - CROSS REFERENCE TO ASSOCIATED RAILCAR LOADING, PALLETIZING, AND CONTAINER LOADING MILITARY STANDARDS
 - HAZARD CLASSIFICATION

| REV. | REVISION DESCRIPTION | DATE | TDA | SYSCOM |
|------|---|--------------------|-----------------|---|
| - | ORIGINAL ISSUE, SUPERSEDES MIL-STD-1320-112B | 2008-4-18 | S/ R SMITH | S/ R SMITH |
| | TECH DATA MANAGEMENT SUPERVISOR | 2008-4-17 | S/ R. SMITH | DISTRIBUTION STATEMENT A APPROVED FOR PUBLIC RELEASE: DISTRIBUTION IS UNLIMITED REQUIREMENTS FOR CONSTRUCTION OF THIS LOAD SHALL CONSIST OF THIS DOCUMENT & THE LATEST ISSUE OF MIL-STD-1320 (NAVY) THIS LOAD IS AUTHORIZED & RELEASED FOR HIGHWAY SHIPMENT ONLY |
| | SYSTEMS ENG. SUPERVISOR | 2008-4-18 | S/ VAN SCHAAK | |
| | S/ ROY A. SMITH 2008-4-18 | | | |
| | NAVSEASYSKOM (BY DIRECTION) | | | |
| | DEPARTMENT OF THE NAVY NAVAL SEA SYSTEMS COMMAND ARLINGTON, VA 22242-5160 | CAGE CODE 53711 | DWG NO. 7517082 | REV. — |
| | SIZE A | | PAGE 1 OF 13 | |

GENERAL NOTES:

1. THIS DOCUMENT PROVIDES DETAILED INSTRUCTIONS FOR TRUCKLOADING 500 LB BOMS (MK 82 SERIES, BLU-111 SERIES, BLU-126 SERIES, BDU-45 SERIES, AND BDU-50 SERIES) UNITIZED ON THE MHU-122 SERIES PALLET.
2. THE PROCEDURES AND PRACTICES CONTAINED HEREIN ARE INTENDED FOR 40 FT AND LONGER TRAILERS (BOTH FLATBED AND ENCLOSED VAN) 96 TO 102 INCHES WIDE.
3. BECAUSE THIS LOADING PLAN RELIES SIGNIFICANTLY ON WOOD DUNNAGE NAILED TO THE TRAILER FLOOR, THE FLOOR OF THE TRAILER (EITHER FLATBED OR VAN) SHALL BE PREDOMINANTLY WOOD.
4. STACKING IS NOT PERMITTED EITHER ON FLATBEDS OR IN VAN TRAILERS.
5. A PARTIAL UNIT LOAD WITH ONLY ONE BOMB SHALL BE TRANSPORTED IN VAN TRAILERS ONLY.
6. TIEDOWN (FLATBED TRAILERS ONLY):

A. THE QUANTITY OF TIEDOWN ASSEMBLIES REQUIRED SHALL BE AS SHOWN IN THE ILLUSTRATIONS. EITHER CHAIN, 4-INCH WEB STRAPS, OR STEEL STRAPPING MAY BE USED FOR TIEDOWN.

B. WEB STRAPS:

- 1) WEB STRAPS ARE THE PREFERRED METHOD OF TIEDOWN, BEING THAT THEY ARE THE EASIEST TO APPLY AND DON'T REQUIRE THE USE OF PROTECTOR BOARDS.
- 2) WEB STRAP TIEDOWNS SHALL CONFORM TO AND BE APPLIED AS SPECIFIED IN NAVSEA DRAWING 6214037 AND IN THIS DRAWING.
- 3) ONLY ASSEMBLIES WITH 4-INCH STRAPPING SHALL BE USED.
- 4) AVOID LOCATING WEB TIEDOWNS ON TOP OF THE LATERAL STEEL STRAPPING OF THE UNIT LOAD, AS TEARING OR FRAYING COULD RESULT.

C. CHAINS:

- 1) CHAIN SHALL CONFORM TO THE NATIONAL ASSOCIATION OF CHAIN MANUFACTURERS WELDED STEEL CHAIN SPECIFICATION. CHAIN SHALL BE GRADE 70 OR BETTER, SIZE 3/8 OR 5/16. GRABHOOKS, LOADBINDERS, AND OTHER LOAD BEARING HARDWARE SHALL HAVE A WORKING LOAD LIMIT AT LEAST THAT OF THE CHAIN AND SHALL BE COMPATIBLE WITH THE SIZE CHAIN BEING USED.
- 2) CHAINS SHALL BE ATTACHED TO THE TRAILER'S STAKE POCKETS, NOT AROUND THE RUBRAIL.
- 3) RATCHET TYPE LOADBINDERS ARE PREFERRED. HOWEVER, IF USING OVER-THE-CENTER TYPE LOADBINDERS, THE HANDLES SHALL BE SECURED IN THE CLOSED POSITION USING .08 DIA OR THICKER STEEL WIRE (ASTM A853; ANNEALED AT FINISH, BLACK OXIDE FINISH, GRADE 1006 OR BETTER).
- 4) TO PREVENT DAMAGE TO THE PALLET FRAMES, DOUBLED 2 X 6 PROTECTOR BOARDS SHALL BE PLACED UNDER THE CHAINS AS SHOWN IN DETAIL K. NAILS SHALL BE USED TO KEEP THE CHAINS IN PLACE ON TOP OF THE PROTECTOR BOARDS. THIS IS ACCOMPLISHED BY DRIVING A NAIL THROUGH ONE CHAIN LINK AND PARTIALLY INTO THE PROTECTOR BOARD ASSEMBLY AND THEN CLINCHING THE NAIL OVER THE CHAIN. USE ONE NAIL NEAR EACH END OF THE PROTECTOR BOARD ASSEMBLIES. DO NOT APPLY NAILS UNTIL AFTER THE CHAINS ARE TENSIONED.

D. STEEL STRAPPING:

- 1) STRAPPING SHALL BE 2 X .044 OR 2 X .050 AND SHALL CONFORM TO ASTM D3953, TYPE 1, HEAVY DUTY, FINISH A, B (ANY GRADE), C, OR D. SEALS SHALL CONFORM TO ASTM D3953, CLASS H, FINISH A, B (ANY GRADE), OR C, STYLE I, II, III, OR IV.
- 2) STEEL STRAP TIEDOWNS SHALL BE ATTACHED TO THE SIDES OF THE TRAILER BY LOOPING THE STRAP AROUND THE RUB RAIL OR STAKE POCKET AND BACK ONTO ITSELF. A MINIMUM OF ONE SEAL WITH TWO PAIR OF NOTCHES WILL BE USED TO SEAL THE JOINT WHEN A NOTCH-TYPE SEALER IS USED. A MINIMUM OF ONE SEAL WITH TWO PAIR OF CRIMPS WILL BE USED WHEN CRIMP-TYPE SEALERS ARE USED.
- 3) WHERE STRAPPING IS JOINED IN AN END-OVER-END LAP JOINT, A MINIMUM OF ONE SEAL WITH TWO PAIR OF NOTCHES WHEN A NOTCH-TYPE SEALER IS USED. WHEN USING A CRIMP-TYPE SEAL, TWO SEALS WITH TWO PAIR OF CRIMPS SHALL BE USED.
- 4) TO PROTECT THE STRAP FROM POSSIBLE SHARP EDGES OF THE RUB RAIL OR STAKE POCKET, AN ADDITIONAL PIECE OF STRAPPING (APPROXIMATELY 18 INCHES) SHALL BE PLACED UNDERNEATH THE STRAP AT THIS LOCATION. IT SHALL BE SECURED TO THE LOAD BEARING STRAP USING ONE SEAL WITH EITHER A SINGLE NOTCH OR A SINGLE CRIMP.
- 5) PROTECTOR BOARDS ARE NOT REQUIRED FOR STEEL STRAP TIEDOWNS.

E. DURING PRE-LOADING INSPECTION REQUIRED BY NAVSEA SW020-AG-SAF-010, ALL CHAIN AND WEB STRAP TIEDOWNS SHALL BE INSPECTED FOR DEFECTS. THE INSPECTION PROCEDURE FOR WEB STRAPS SHALL BE AS SPECIFIED IN DRAWING 6214037. CHAINS, FITTINGS AND LOAD BINDERS SHALL BE INSPECTED FOR STRETCH, GOUGING, BENT LINKS, WEAR, AND ANY OTHER NOTICEABLE DEFECTS THAT WOULD AFFECT THE STRENGTH OF THE ASSEMBLY. RESULTS OF THESE INSPECTIONS SHALL BE RECORDED IN ITEM 12-T OF DD FORM 626. ANY DEFICIENCY SHALL BE CAUSE FOR REJECTION OF THE CHAINS, FITTINGS, BINDERS, OR WEB STRAPPING ASSEMBLIES.

GENERAL NOTES (CONT'D):

7. A STAGGERED NAILING PATTERN WILL BE USED WHEN NAILS ARE DRIVEN INTO FLOOR DUNNAGE. THE NAILING PATTERN WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL DOES NOT PENETRATE INTO OR NEAR A CRACK BETWEEN TRAILER FLOOR BOARDS. THE NAILING FOR AN UPPER PIECE OF FLOOR DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN NEAR A NAIL IN A LOWER PIECE.
8. THE MAXIMUM NUMBER OF UNIT LOADS THAT CAN BE LOADED INTO/ONTO A TRAILER DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE MAXIMUM ALLOWABLE GROSS WEIGHT OF THE TRACTOR-TRAILER AND THE ALLOWABLE AXLE WEIGHTS ARE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THESE LIMITATIONS AND THE SHIPPER SHALL LOAD THE TRAILER IN SUCH A MANNER THAT THE TRACTOR-TRAILER WILL NOT EXCEED THESE LIMITATIONS.
9. WHEN USING FLATBED TRAILERS, THE UNIT LOADS SHALL BE COMPLETELY COVERED WITH FIRE RESISTANT AND WATERPROOF TARPULINS. THE TARPULINS MAY BE UNDER THE TIEDOWNS.
10. IF USING A VAN TRAILER WITH A ROLL-UP REAR DOOR, THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE LOAD CANNOT SHIFT AND BEAR AGAINST THE REAR DOOR. THUS, USE OF DETAIL G WOULD BE FORBIDDEN IN THIS TYPE OF TRAILER.
11. LUMBER SHALL CONFORM TO VOLUNTARY PRODUCT STANDARD PS 20-2005 (AMERICAN SOFTWOOD LUMBER STANDARD).
12. NAILS SHALL CONFORM TO ASTM F1667 DESIGNATION F1667 NL CM S - XX B, WHERE "XX" DESIGNATES THE SIZE (I.E., 10d, 16d, 20d, ETC.).
13. AFTER THE BLOCKING AND BRACING HAS BEEN INSPECTED, ATTACH THE SHIPPING DOCUMENTS TO AN ACCESSIBLE AREA INSIDE THE TRAILER, OR IF USING A FLATBED, ON THE REAR DECK OF THE TRAILER.
14. APPROPRIATE EXPLOSIVES PLACARDS SHALL BE ATTACHED TO THE BOTH SIDES, AND REAR OF THE TRAILER.
15. FOR GENERAL TRUCKLOADING PROCEDURES REFER TO THE GENERAL TRUCKLOADING DOCUMENT, MIL-STD-1320 (NAVY).

NOTES

FULL TRUCKLOAD - VAN TRAILER

(SEE NOTE ON THIS PAGE REGARDING MAX QUANTITY)

1. ALTHOUGH THE ILLUSTRATION ON THIS PAGE SHOWS A TRAILER LOADED WITH 13 UNIT LOADS, THE ACTUAL MAXIMUM NUMBER OF UNIT LOADS THAT CAN BE LOADED DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE AXLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE ALLOWABLE AXLE WEIGHTS ARE NOT EXCEEDED.

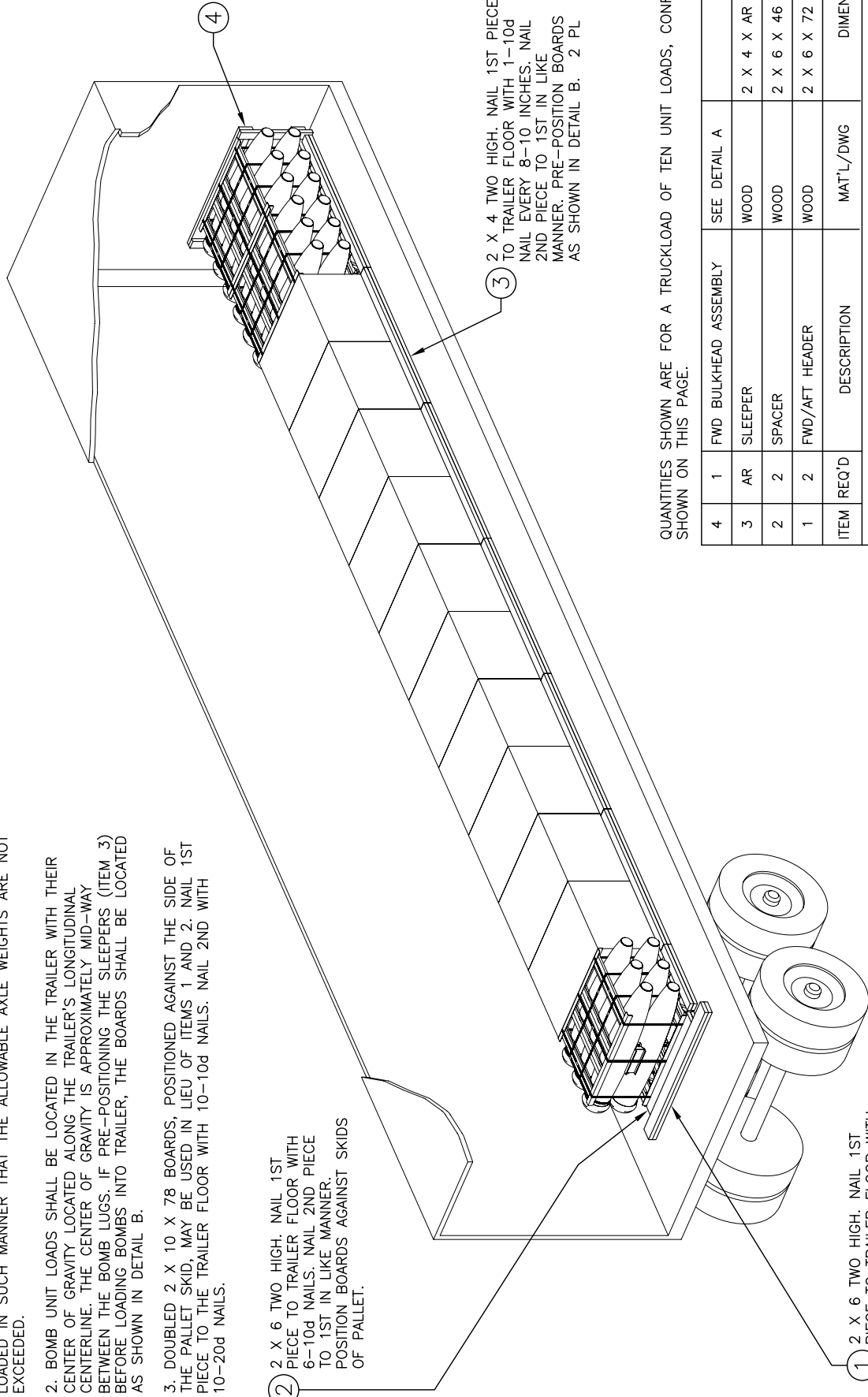
2. BOMB UNIT LOADS SHALL BE LOCATED IN THE TRAILER WITH THEIR CENTER OF GRAVITY LOCATED ALONG THE TRAILER'S LONGITUDINAL CENTERLINE. THE CENTER OF GRAVITY IS APPROXIMATELY MID-WAY BETWEEN THE BOMB LUGS. IF PRE-POSITIONING THE SLEEPERS (ITEM 3) BEFORE LOADING BOMBS INTO TRAILER, THE BOARDS SHALL BE LOCATED AS SHOWN IN DETAIL B.

3. DOUBLED 2 X 10 X 78 BOARDS, POSITIONED AGAINST THE SIDE OF THE PALLET SKID, MAY BE USED IN LIEU OF ITEMS 1 AND 2. NAIL 1ST PIECE TO THE TRAILER FLOOR WITH 10-10d NAILS. NAIL 2ND WITH 10-20d NAILS.

2 2 X 6 TWO HIGH. NAIL 1ST PIECE TO TRAILER FLOOR WITH 6-10d NAILS. NAIL 2ND PIECE TO 1ST IN LIKE MANNER. POSITION BOARDS AGAINST SKIDS OF PALLET.

3 2 X 4 TWO HIGH. NAIL 1ST PIECE TO TRAILER FLOOR WITH 1-10d NAIL EVERY 8-10 INCHES. NAIL 2ND PIECE TO 1ST IN LIKE MANNER. PRE-POSITION BOARDS AS SHOWN IN DETAIL B. 2 PL

1 2 X 6 TWO HIGH. NAIL 1ST PIECE TO TRAILER FLOOR WITH 9-10d NAILS. NAIL 2ND PIECE TO 1ST WITH 9-20d NAILS.



QUANTITIES SHOWN ARE FOR A TRUCKLOAD OF TEN UNIT LOADS, CONFIGURED AS SHOWN ON THIS PAGE.

| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |
|------|-------|-----------------------|--------------|------------|
| 4 | 1 | FWD BULKHEAD ASSEMBLY | SEE DETAIL A | |
| 3 | AR | SLEEPER | WOOD | 2 X 4 X AR |
| 2 | 2 | SPACER | WOOD | 2 X 6 X 46 |
| 1 | 2 | FWD/AFT HEADER | WOOD | 2 X 6 X 72 |

LIST OF MATERIALS

NAVSEA
DWG NO.

7517082

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SIZE A

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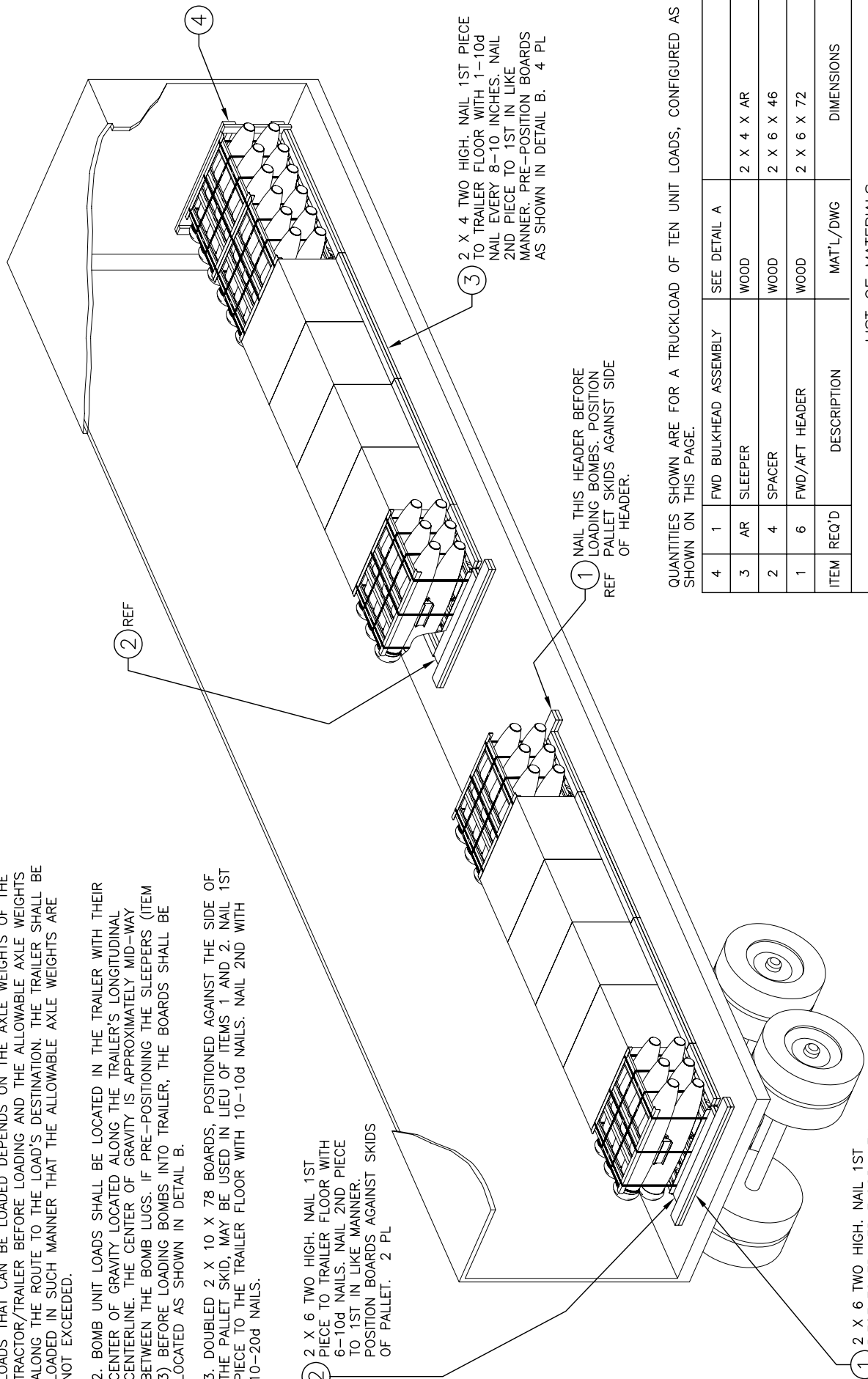
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FULL TRUCKLOAD - VAN TRAILER W/ SPLIT LOAD

(SEE NOTE 1 ON THIS PAGE REGARDING MAX QUANTITY)

NOTES

- ALTHOUGH THE ILLUSTRATION ON THIS PAGE SHOWS A TRAILER LOADED WITH 13 UNIT LOADS, THE ACTUAL MAXIMUM NUMBER OF UNIT LOADS THAT CAN BE LOADED DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE AXLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE ALLOWABLE AXLE WEIGHTS ARE NOT EXCEEDED.
- BOMB UNIT LOADS SHALL BE LOCATED IN THE TRAILER WITH THEIR CENTER OF GRAVITY LOCATED ALONG THE TRAILER'S LONGITUDINAL CENTERLINE. THE CENTER OF GRAVITY IS APPROXIMATELY MID-WAY BETWEEN THE BOMB LUGS. IF PRE-POSITIONING THE SLEEPERS (ITEM 3) BEFORE LOADING BOMBS INTO TRAILER, THE BOARDS SHALL BE LOCATED AS SHOWN IN DETAIL B.
- DOUBLED 2 X 10 X 78 BOARDS, POSITIONED AGAINST THE SIDE OF THE PALLET SKID, MAY BE USED IN LIEU OF ITEMS 1 AND 2. NAIL 1ST PIECE TO THE TRAILER FLOOR WITH 10-10d NAILS. NAIL 2ND WITH 10-20d NAILS.



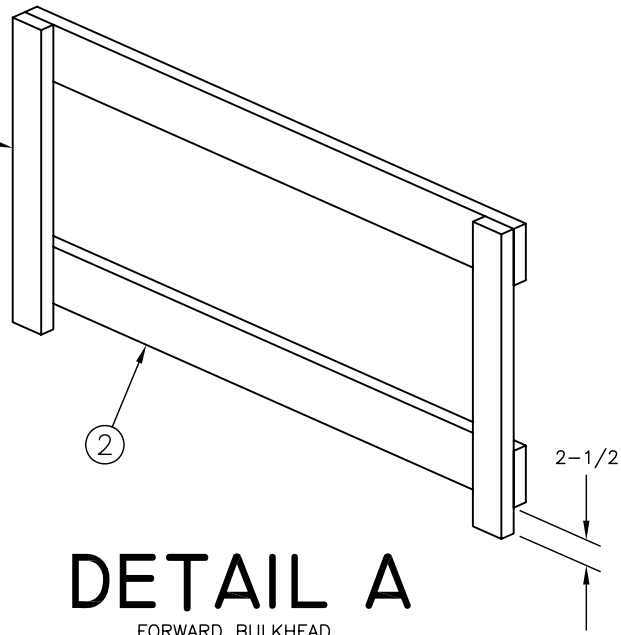
QUANTITIES SHOWN ARE FOR A TRUCKLOAD OF TEN UNIT LOADS, CONFIGURED AS SHOWN ON THIS PAGE.

| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |
|------|-------|-----------------------|--------------|------------|
| 4 | 1 | FWD BULKHEAD ASSEMBLY | SEE DETAIL A | |
| 3 | AR | SLEEPER | WOOD | 2 X 4 X AR |
| 2 | 4 | SPACER | WOOD | 2 X 6 X 46 |
| 1 | 6 | FWD/AFT HEADER | WOOD | 2 X 6 X 72 |

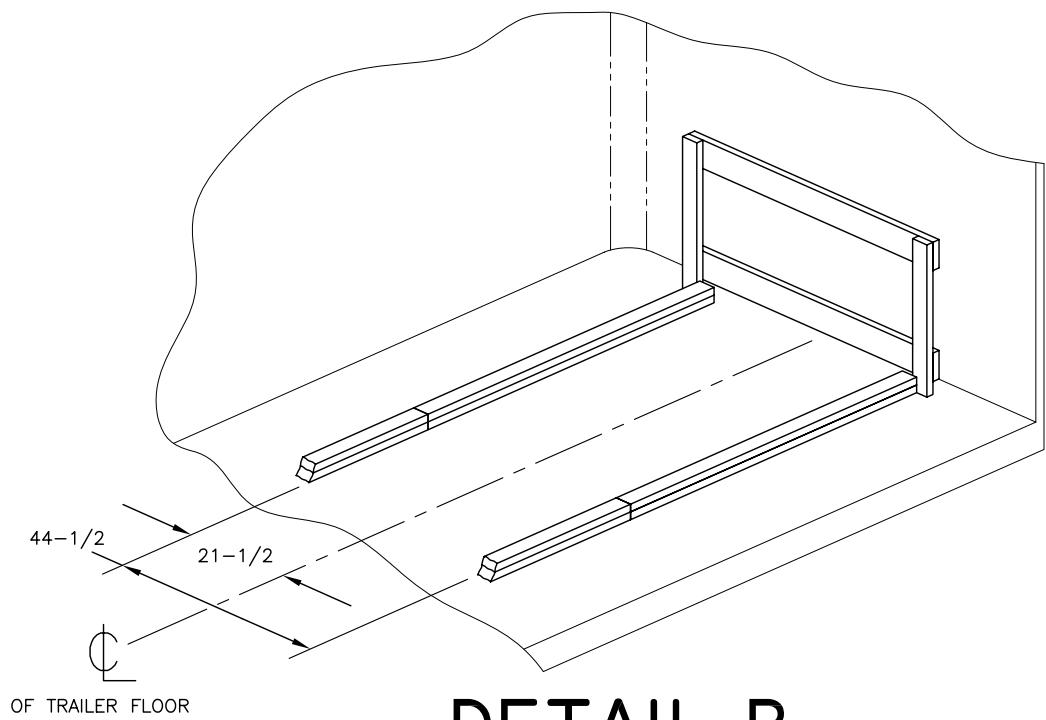
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| NAVSEA DWG NO. | 7517082 | REV. | — | SIZE | A | PAGE | 5 | OF | 13 |
| | | CAGE CODE | 53711 | | | | | | |

NAIL TO ITEM 2 WITH
THREE 8d OR 9d NAILS
PER JOINT. 2 PL



DETAIL A
FORWARD BULKHEAD



DETAIL B
PRE-POSITIONED SIDE BLOCKING
(FOR SINGLE UNIT LOAD WITH NOSE
END FACING SIDEWAYS)

QUANTITIES SHOWN ARE THAT REQUIRED TO CONSTRUCT DETAIL A.

| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |
|------|-------|---------------|-----------|----------------|
| 2 | 2 | CROSSMEMBER | WOOD | 2 X 6 X 60 |
| 1 | 2 | VERTICAL POST | WOOD | 2 X 4 X 29-1/2 |

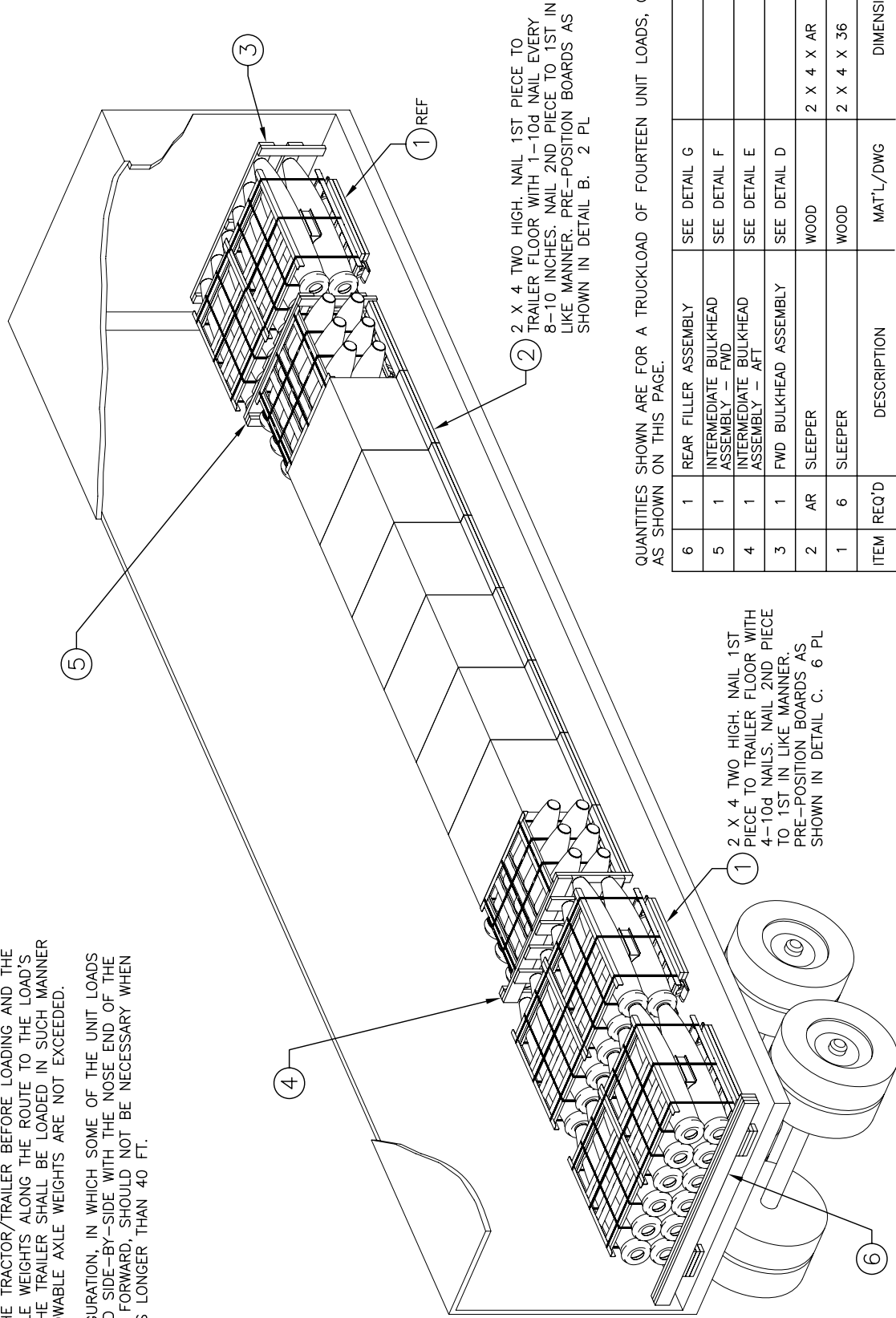
LIST OF MATERIALS

FULL TRUCKLOAD - 40 FT VAN TRAILER

(SEE NOTE 1 ON THIS PAGE REGARDING MAX QUANTITY)

NOTES

- ALTHOUGH THE ILLUSTRATION ON THIS PAGE SHOWS A TRAILER LOADED WITH 14 UNIT LOADS, THE ACTUAL MAXIMUM NUMBER OF UNIT LOADS THAT CAN BE LOADED DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE AXLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE ALLOWABLE AXLE WEIGHTS ARE NOT EXCEEDED.
- THIS CONFIGURATION, IN WHICH SOME OF THE UNIT LOADS ARE POSITIONED SIDE-BY-SIDE WITH THE NOSE END OF THE BOMBS FACING FORWARD, SHOULD NOT BE NECESSARY WHEN USING TRAILERS LONGER THAN 40 FT.



QUANTITIES SHOWN ARE FOR A TRUCKLOAD OF FOURTEEN UNIT LOADS, CONFIGURED AS SHOWN ON THIS PAGE.

| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |
|------|-------|--------------------------------------|--------------|------------|
| 6 | 1 | REAR FILLER ASSEMBLY | SEE DETAIL G | |
| 5 | 1 | INTERMEDIATE BULKHEAD ASSEMBLY - FWD | SEE DETAIL F | |
| 4 | 1 | INTERMEDIATE BULKHEAD ASSEMBLY - AFT | SEE DETAIL E | |
| 3 | 1 | FWD BULKHEAD ASSEMBLY | SEE DETAIL D | |
| 2 | AR | SLEEPER | WOOD | 2 X 4 X AR |
| 1 | 6 | SLEEPER | WOOD | 2 X 4 X 36 |

LIST OF MATERIALS

NAVSEA
DWG NO.

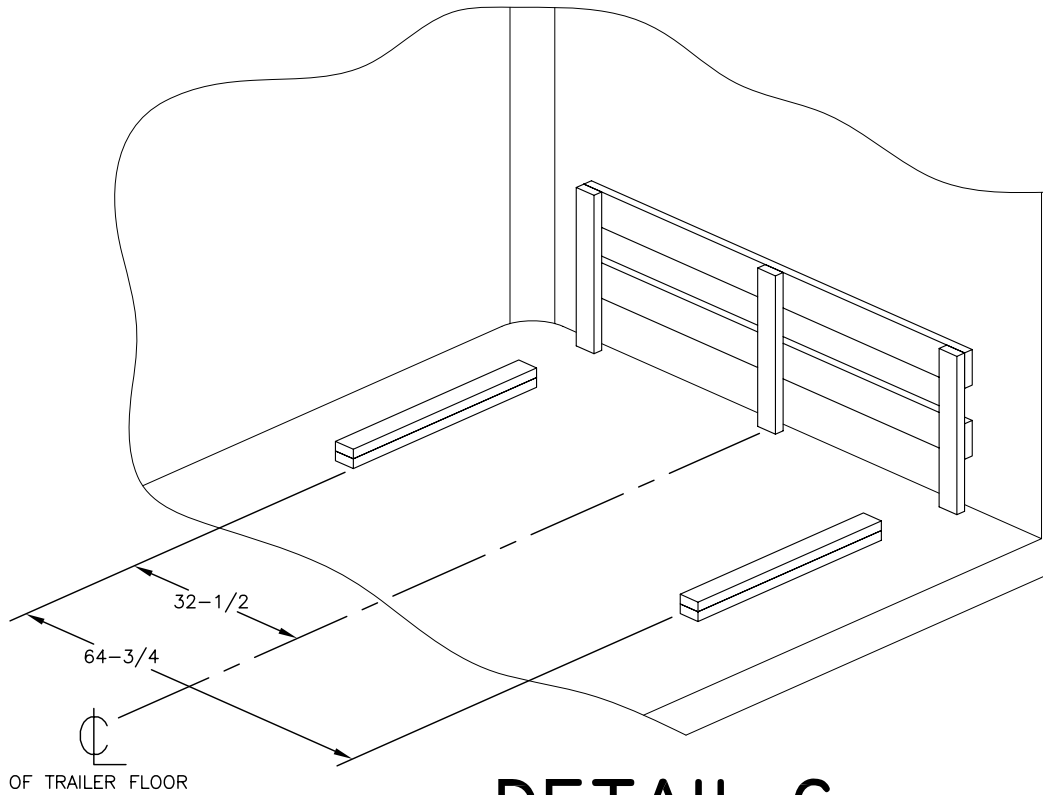
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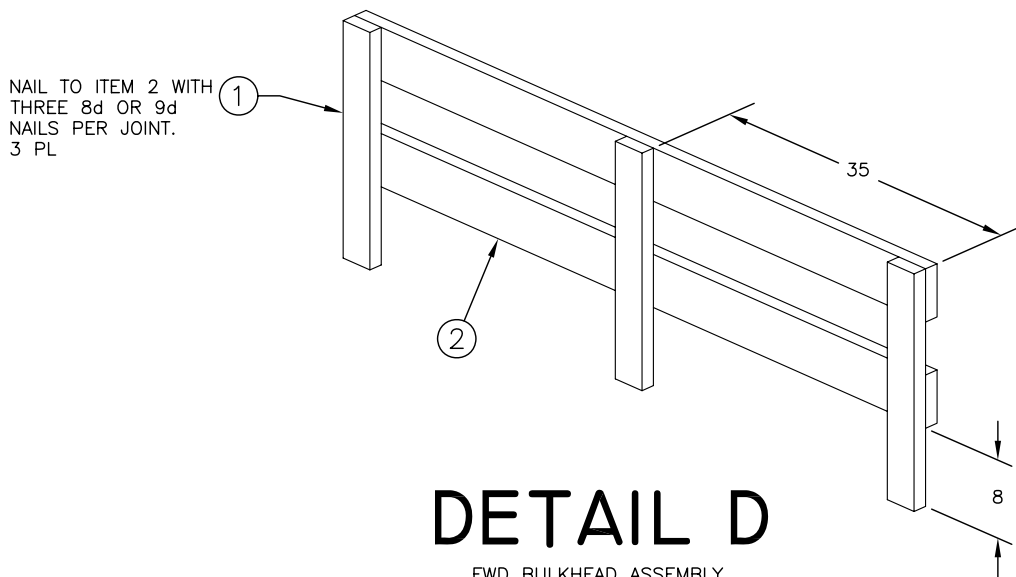
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DETAIL C

PRE-POSITIONED SLEEPERS FOR
TWO-WIDE CONFIGURATION.



DETAIL D

FWD BULKHEAD ASSEMBLY
(FOR TWO SIDE-BY-SIDE UNIT LOADS)

QUANTITIES SHOWN ARE THAT REQUIRED TO CONSTRUCT DETAIL D.

| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |
|------|-------|---------------|-----------|----------------|
| 2 | 2 | CROSSMEMBER | WOOD | 2 X 6 X 75 |
| 1 | 3 | VERTICAL POST | WOOD | 2 X 4 X 24-1/2 |

LIST OF MATERIALS

NAVSEA
DWG NO.

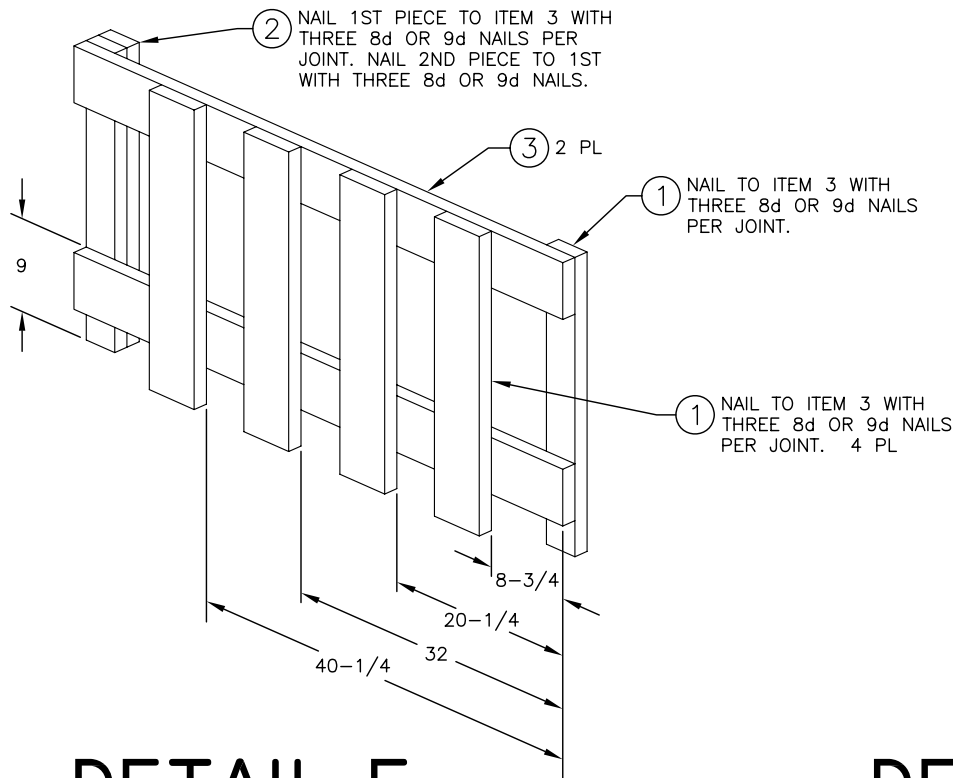
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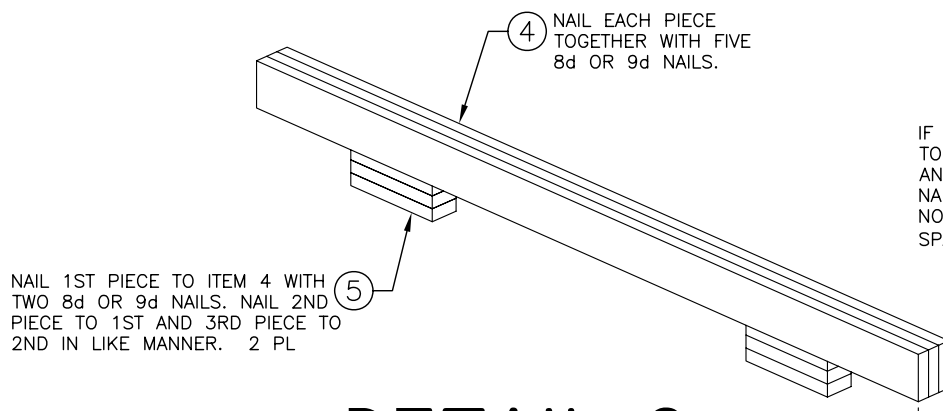


DETAIL E

INTERMEDIATE SPACER, AFT
(CONSTRUCT AS SHOWN)

DETAIL F

INTERMEDIATE SPACER, FWD
(OPPOSITE HAND OF DETAIL E)



DETAIL G

REAR FILLER ASSEMBLY

NOTE

IF LESS THAN THREE BOARDS ARE NEEDED TO FILL SPACE BETWEEN REAR OF LOAD AND TRAILER DOORS, USE 2 X 12 BOARDS NAILED TOGETHER WITH 8d OR 9d NAILS. NO FILLER ASSEMBLY IS REQUIRED IF THE SPACE IS LESS THAN 1-1/2 INCHES.

QTY AS REQ'D TO FILL SPACE BETWEEN REAR OF LOAD AND TRAILER DOORS (MINIMUM 3 BOARDS, SEE NOTE ABOVE)

QUANTITIES SHOWN ARE THAT REQUIRED TO CONSTRUCT ONE OF EACH DETAILS E, F, AND G.

* TW = TRAILER WIDTH

| 5 | 6 | RISER | WOOD | 2 X 4 X 12 |
|------|-------|------------------|-----------|----------------|
| 4 | AR | CROSSMEMBER | WOOD | 2 X 6 X TW*-1 |
| 3 | 4 | CROSSMEMBER | WOOD | 2 X 6 X 60 |
| 2 | 6 | VERTICAL SPACER | WOOD | 2 X 4 X 29-1/2 |
| 1 | 8 | VERTICAL SUPPORT | WOOD | 2 X 6 X 29-1/2 |
| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |

LIST OF MATERIALS

NAVSEA
DWG NO.

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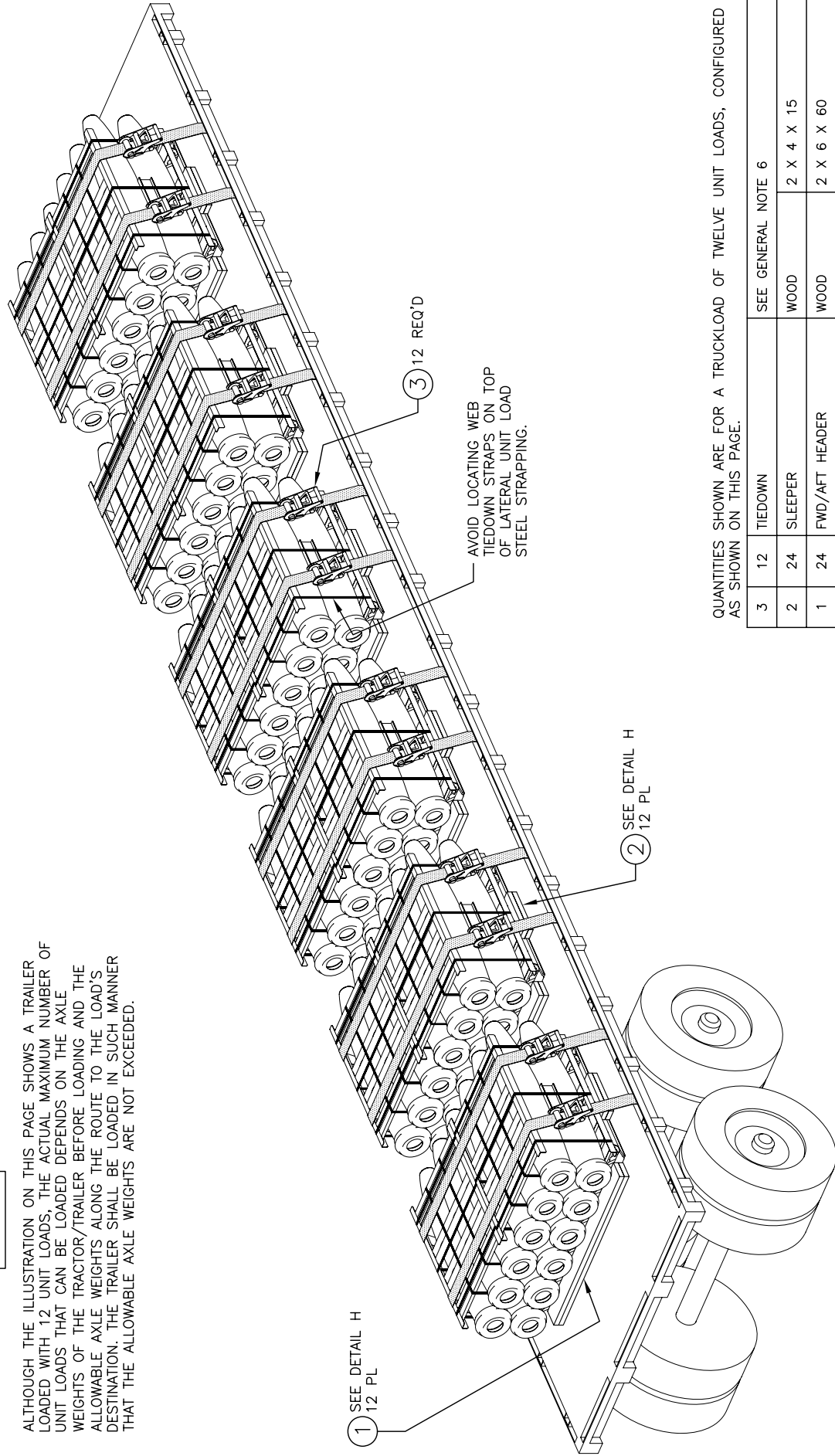
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TYPICAL FULL TRUCKLOAD - FLATBED TRAILER

(SEE NOTE ON THIS PAGE REGARDING MAX QUANTITY)

NOTE

ALTHOUGH THE ILLUSTRATION ON THIS PAGE SHOWS A TRAILER LOADED WITH 12 UNIT LOADS, THE ACTUAL MAXIMUM NUMBER OF UNIT LOADS THAT CAN BE LOADED DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE AXLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE ALLOWABLE AXLE WEIGHTS ARE NOT EXCEEDED.



QUANTITIES SHOWN ARE FOR A TRUCKLOAD OF TWELVE UNIT LOADS, CONFIGURED AS SHOWN ON THIS PAGE.

| | | | | |
|------|-------|----------------|--------------------|------------|
| 3 | 12 | TIEDOWN | SEE GENERAL NOTE 6 | |
| 2 | 24 | SLEEPER | WOOD 2 X 4 X 15 | |
| 1 | 24 | FWD/AFT HEADER | WOOD 2 X 6 X 60 | |
| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |

LIST OF MATERIALS

NAVSEA
DWG NO.

7517082

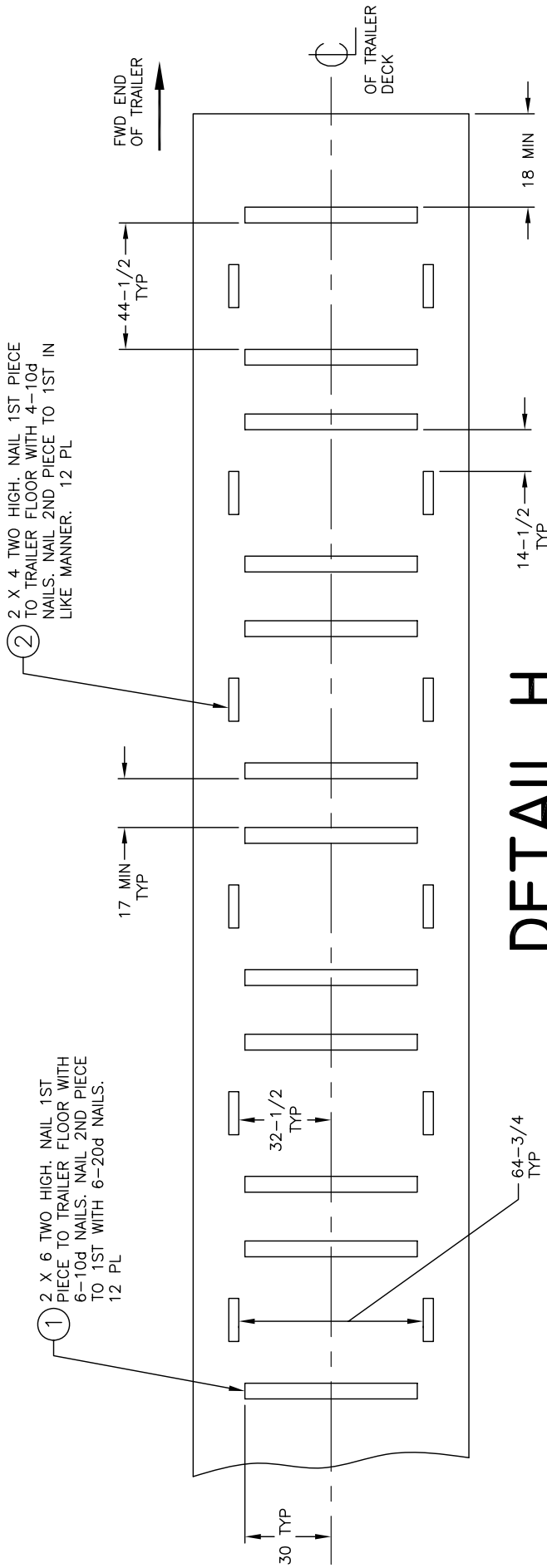
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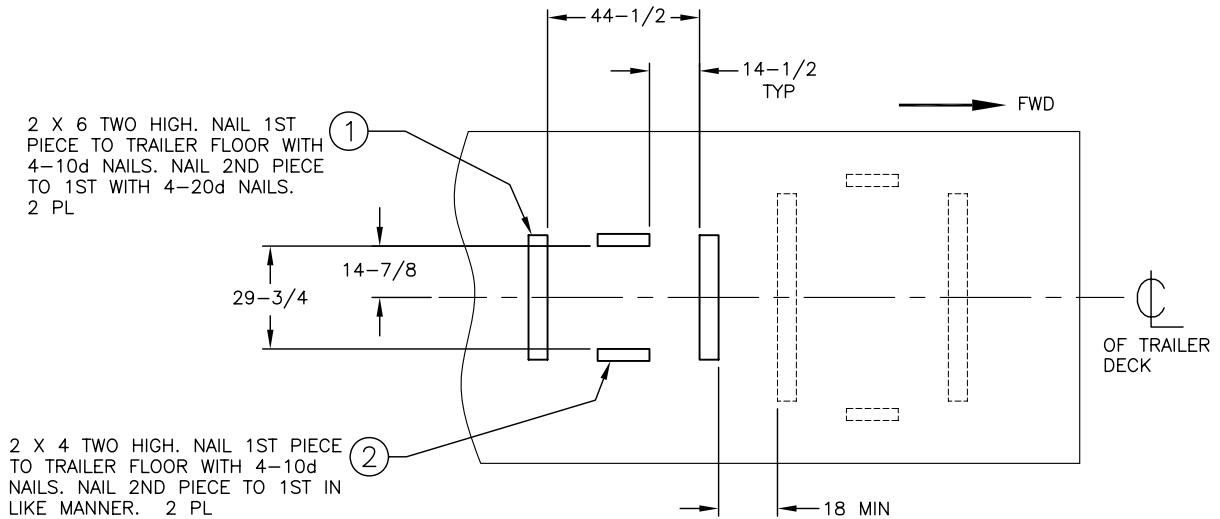
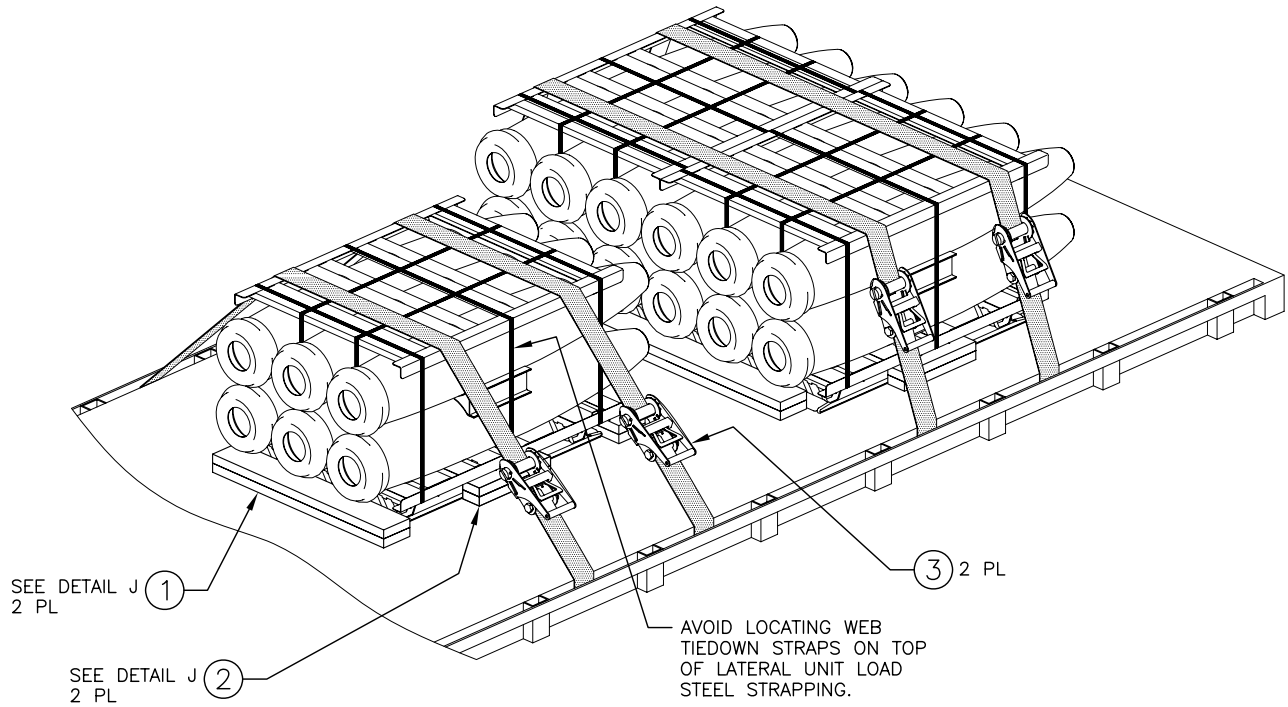


DETAIL H

PRE-POSITIONED FLOOR BLOCKING
(FOR FLATBED TRAILER)

LESS THAN FULL TRUCKLOAD - FLATBED TRAILER

(SINGLE WIDE UNIT LOAD)



DETAIL J

PRE-POSITIONED FLOOR BLOCKING
(FOR SINGLE UNIT LOAD ON FLATBED TRAILER)

QUANTITIES SHOWN ARE FOR BLOCKING AND BRACING A SINGLE UNIT LOAD AS SHOWN ON THIS PAGE.

| 3 | 2 | TIEDOWN | SEE GENERAL NOTE 6 | |
|------|-------|----------------|--------------------|------------|
| 2 | 4 | SLEEPER | WOOD | 2 X 4 X 15 |
| 1 | 4 | FWD/AFT HEADER | WOOD | 2 X 6 X 36 |
| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |

LIST OF MATERIALS

NAVSEA
DWG NO.

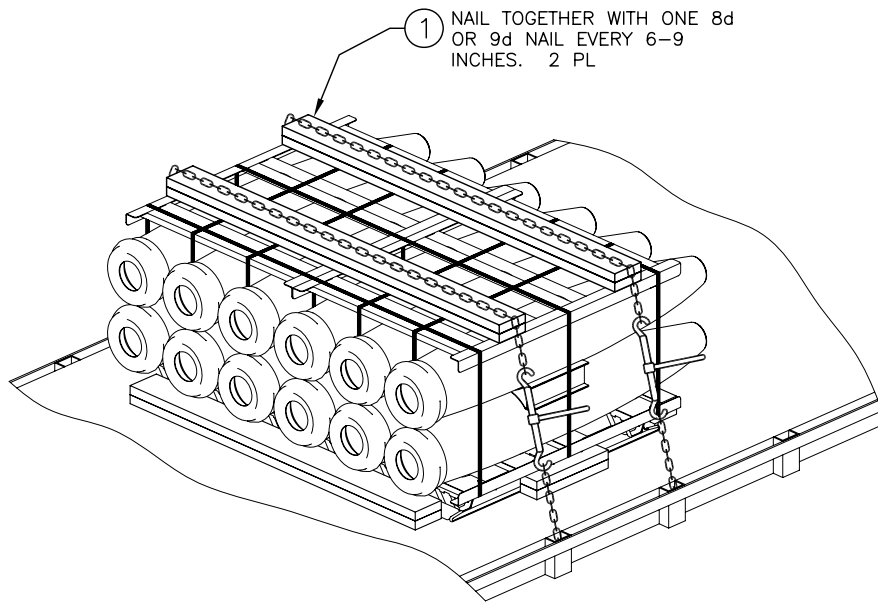
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DETAIL K

USE OF PROTECTOR BOARDS
UNDERNEATH CHAIN TIEDOWNS

NOTE: FOR A SINGLE UNIT LOAD, THE LENGTH OF THE PROTECTOR BOARDS SHALL BE 35".

| 1 | 4 | PROTECTOR BOARD | WOOD | 2 X 6 X 70 |
|------|-------|-----------------|-----------|------------|
| ITEM | REQ'D | DESCRIPTION | MAT'L/DWG | DIMENSIONS |

LIST OF MATERIALS

| | | | | | | | | | | | |
|-------------------|---------|------|---|------|---|-----------|-------|------|----|----|----|
| NAVSEA DWG NO. | 7517082 | REV. | — | SIZE | A | CAGE CODE | 53711 | PAGE | 13 | OF | 13 |
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