

# TRUCKLOAD

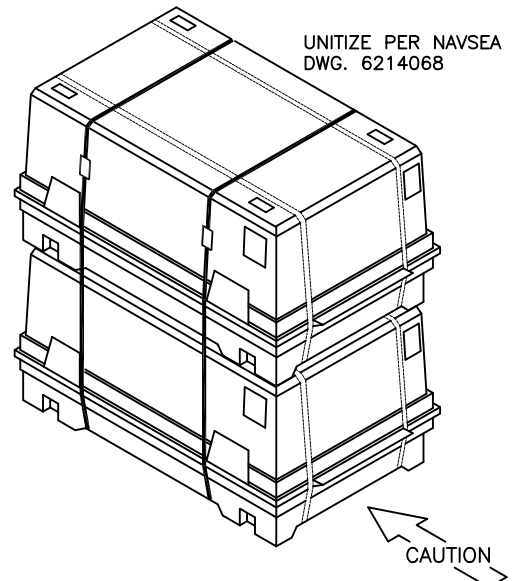
## GUIDANCE SETS (KMU-556, KMU-557, KMU-558, KMU-559 OR KMU-572 SERIES) IN SHIPPING & STORAGE CONTAINERS, CNU-589A/E OR CNU-589/E

<u>CONTAINER DIMENSIONS:</u>	<u>CNU-589/E</u>	<u>CNU-589A/E</u>
OVERALL DIMENSIONS ( L x W x H ) . . . . .	63.4 x 42 x 32.9	65.2 x 42 x 32.4
STACKING HEIGHT . . . . .	32.3	31.8
CUBE . . . . .	50.7 CU-FT	51.4 CU-FT

<u>CONTAINER DATA</u>	<u>KMU-556</u>	<u>KMU-557</u>	<u>KMU-558</u>	<u>KMU-559</u>	<u>KMU-572</u>
GROSS WEIGHT OF CONTAINER (CNU-589/E) . . . . .	459 LBS	558 LBS	566 LBS	357 LBS	N/A
TARE WEIGHT OF CONTAINER (CNU-589/E) . . . . .	167 LBS	167 LBS	167 LBS	167 LBS	167 LBS
GROSS WEIGHT OF CONTAINER (CNU-589A/E) . . . . .	503 LBS	602 LBS	610 LBS	401 LBS	631 LBS
TARE WEIGHT OF CONTAINER (CNU-589A/E) . . . . .	211 LBS	211 LBS	211 LBS	211 LBS	211 LBS

<u>UNIT LOAD DIMENSIONS:</u>	<u>CNU-589/E</u>	<u>CNU-589A/E</u>
OVERALL DIMENSIONS ( L x W x H ) . . . . .	63.4 x 42 x 65.2	65.2 x 42 x 64.2
STACKING HEIGHT . . . . .	64.6	63.6
CUBE . . . . .	100.5 CU-FT	101.7 CU-FT

<u>UNIT LOAD DATA</u>	<u>KMU-556</u>	<u>KMU-557</u>	<u>KMU-558</u>	<u>KMU-559</u>	<u>KMU-572</u>
GROSS WEIGHT OF UNIT LOAD (CNU-589/E) . . . . .	921 LBS	1119 LBS	1135 LBS	717 LBS	N/A
GROSS WEIGHT OF UNIT LOAD (CNU-589A/E) . . . . .	1009 LBS	1207 LBS	1223 LBS	805 LBS	1265 LBS



**NOTES:**

1. GROSS WEIGHT IS ESTIMATED ONLY. DO NOT USE FOR SHIPPING WEIGHT.
2. UNLESS OTHERWISE SPECIFIED, DIMENSIONS ARE IN INCHES.
2. SEE SW020-AC-SAF-010 FOR THE FOLLOWING INFORMATION:
  - A.) CROSS REFERENCE TO ASSOCIATED PALLETIZING, CONTAINER LOADING AND CAR LOADING MILITARY STANDARDS.
  - B.) HAZARD CLASSIFICATION

HANDLING FROM ENDS OF UNIT LOAD REQUIRES USE OF FORKLIFT EQUIPPED WITH 40" MINIMUM TINE LENGTH.

C	SEE NSWC IND DET EARLE ECP I04059	2011-10-17	S/ MB	S/ RAS
B	SEE NSWC IND DET EARLE ECP I02060	2002-8-6	S/ AVS	S/ KHZ
A	SEE NSWC IND DET EARLE ECP I02025	2002-5-3	S/ AVS	S/ KHZ
REV.	REVISION DESCRIPTION	DATE	TDA	SYSCOM

TECH DATA MANAGEMENT SUPERVISOR	S/ A STANTON	1999-7-8
SYSTEMS ENG. SUPERVISOR	S/ G BENDER	1999-7-8
S/ K.H. ZIMMS 1999/7/8		
NAVSEASYSKOM ( BY DIRECTION )		

DISTRIBUTION STATEMENT A  
APPROVED FOR PUBLIC RELEASE: DISTRIBUTION IS UNLIMITED

REQUIREMENTS FOR CONSTRUCTION OF THIS LOAD SHALL CONSIST OF THIS DOCUMENT & THE LATEST ISSUE OF MIL-STD-1320 (NAVY)

THIS LOAD IS AUTHORIZED & RELEASED FOR HIGHWAY SHIPMENT ONLY

DEPARTMENT OF THE NAVY NAVAL SEA SYSTEMS COMMAND ARLINGTON, VA 22242-5160	CAGE CODE 53711	DWG NO. 6214089	REV. C
SIZE A	PAGE 1 OF 11		

## GENERAL NOTES:

1. THIS DOCUMENT PROVIDES INSTRUCTIONS FOR TRUCKLOADING JDAM GUIDANCE SETS (KMU-556/B, KMU-557/B, KMU-558/B, KMU-559/B OR KMU-572/B) PACKAGED IN THE CNU-589A/E OR CNU-589/E SHIPPING AND STORAGE CONTAINER.
2. PROCEDURES FOR LOADING CONTAINERS ONTO BOTH FLATBED AND ENCLOSED VAN TRAILERS ARE PROVIDED.
  - PAGES 4 AND 5: FLATBED TRAILERS 96"-102" WIDE.
  - PAGES 7 AND 8: VAN TRAILERS USING INFLATABLE DUNNAGE AND WOOD
  - PAGES 9 THRU 11: VAN TRAILERS USING ALL WOOD DUNNAGE
3. BECAUSE THIS LOADING PLAN RELIES SIGNIFICANTLY ON WOOD DUNNAGE NAILED TO THE TRAILER FLOOR, THE FLOOR OF THE TRAILER SHALL BE PREDOMINANTLY WOOD.
4. LOADED CONTAINERS ARE PERMITTED TO BE STACKED THREE HIGH INTO VAN TRAILERS ONLY. IN ORDER TO ENSURE CONTINUOUS ENGAGEMENT OF THE STACKING FEATURES, STACKED CONTAINERS SHALL BE BANDED TOGETHER WITH STEEL STRAPPING AS SHOWN IN NAVSEA DRAWING 6214068 AND IN DETAIL A ON PAGE 6.
5. SEE DETAIL A ON PAGE 6, UNITIZING PROCEDURE, FOR VAN TRAILERS ONLY. WHEN SHIPPING EMPTY CONTAINERS, 3/4 X .031-.035 STRAPPING MAY BE USED IN LIEU OF THE SPECIFIED 1-1/4" STRAPPING.
6. LUMBER SHALL CONFORM TO VOLUNTARY PRODUCT STANDARD PS 20-2005 (AMERICAN SOFTWOOD LUMBER STANDARD).
7. UNITED NATIONS (UN), INTERNATIONAL PLANT PROTECTION COMMISSION (IPPC) RESTRICTIONS REGARDING SOLID WOOD PACKAGING MATERIAL (WPM): IN ACCORDANCE WITH THE REQUIREMENTS OF INTERNATIONAL STANDARDS FOR PHYTOSANITARY MEASURES (ISPM) 15 "GUIDELINES FOR REGULATING WOOD PACKAGING MATERIAL IN INTERNATIONAL TRADE," THE FOLLOWING COMMERCIAL HEAT TREATMENT PROCESS HAS BEEN APPROVED BY THE AMERICAN LUMBER STANDARDS COMMITTEE (ALSC) AND IS REQUIRED FOR ALL NON-MANUFACTURED WPM. WPM SHALL BE CONSTRUCTED FROM HEAT TREATED (HT TO 56 DEGREES CENTIGRADE FOR 30 MINUTES) LUMBER AND CERTIFIED BY AN ACCREDITED AGENCY RECOGNIZED BY THE ALSC IN ACCORDANCE WITH WOOD PACKAGING MATERIAL POLICY AND WOOD PACKAGING MATERIAL ENFORCEMENT REGULATIONS ([HTTP://WWW.ALSC.ORG](http://www.alsc.org)). EACH PIECE OF WOOD MUST INCLUDE CERTIFICATION MARKINGS IN ACCORDANCE WITH ALSC STANDARDS AND BE PLACED IN AN UNOBSTRUCTED AREA THAT WILL BE READILY VISIBLE TO INSPECTORS. FAILURE TO COMPLY WITH THE REQUIREMENTS OF THIS RESTRICTION MAY RESULT IN REFUSAL, DESTRUCTION, OR TREATMENT OF MATERIALS AT THE POINT OF ENTRY, POSSIBLY CAUSING UNACCEPTABLE DELAY IN DELIVERY OF NEEDED PARTS.
8. NAILS SHALL CONFORM TO ASTM F1667 DESIGNATION F1667 NL CM S - XX B, WHERE "XX" DESIGNATES THE SIZE (I.E., 10d, 16d, 20d, ETC.).
9. A STAGGERED NAILING PATTERN WILL BE USED WHEN NAILS ARE DRIVEN INTO FLOOR DUNNAGE. THE NAILING PATTERN WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL DOES NOT PENETRATE INTO OR NEAR A CRACK BETWEEN TRAILER FLOOR BOARDS. THE NAILING FOR AN UPPER PIECE OF FLOOR DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN NEAR A NAIL IN A LOWER PIECE.
10. COMBINATION OF STACKS CONTAINING 1, 2, 4 OR 6 CONTAINERS MAY BE USED FOR LESS THAN FULL LOADS. A STACK CONSISTING OF THREE CONTAINERS (I.E. TWO CONTAINERS POSITIONED NEXT TO A SINGLE CONTAINERS) IS NOT PERMITTED ON BOTH FLATBED AND VAN TRAILERS. A ONE-WIDE AND TWO-HIGH STACK OF CONTAINERS BY ITSELF (I.E. 2 CONTAINERS) IS ALSO NOT PERMITTED ON BOTH FLATBED AND VAN TRAILERS.
11. TIEDOWN (FLATBED TRAILERS ONLY):
  - A. THE QUANTITY OF TIEDOWN ASSEMBLIES REQUIRED SHALL BE AS SHOWN IN THE ILLUSTRATIONS. ONLY 4-INCH WEB STRAPS SHALL BE USED FOR TIEDOWN.
  - B. WEB STRAP TIEDOWNS SHALL CONFORM TO AND BE APPLIED AS SPECIFIED IN NAVSEA DRAWING 6214037 AND IN THIS DRAWING.
  - C. DURING PRE-LOADING INSPECTION REQUIRED BY NAVSEA SWO20-AG-SAF-010, WEB STRAP ASSEMBLIES SHALL BE INSPECTED FOR DEFECTS. THE INSPECTION PROCEDURE FOR WEB STRAPS SHALL BE AS SPECIFIED IN NAVSEA DRAWING 6214037. THE RESULTS OF THESE INSPECTIONS SHALL BE RECORDED IN ITEM 12-T OF DD FORM 626. ANY DEFICIENCY SHALL BE CAUSE FOR REJECTION OF THE WEB STRAP ASSEMBLY.
12. THE MAXIMUM GROSS WEIGHT OF THE TRACTOR-TRAILER AND THE ALLOWABLE AXLE WEIGHTS ARE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THESE LIMITATIONS AND THE SHIPPER SHALL LOAD THE TRAILER IN SUCH A MANNER THAT THE TRACTOR-TRAILER WILL NOT EXCEED THESE LIMITATIONS.
13. THE CONTAINERS SHALL BE COMPLETELY COVERED WITH FIRE RESISTANT WATERPROOF TARPULINS. THE TARPULINS MAY BE UNDER THE TIEDOWNS (FLATBEDS TRAILERS ONLY).
14. USE OF INFLATABLE VINYL DUNNAGE BAGS (VAN TRAILERS ONLY):
  - A. VINYL DUNNAGE BAGS ARE AUTHORIZED TO BE USED AS LATERAL RESTRAINT IN LIEU OF THE SWAY BRACES CALLED OUT IN SECTION A-A AND THE 2 X 4 SLEEPERS NAILED TO THE FLOOR AS SHOWN IN SECTION B-B.
  - B. THE VINYL DUNNAGE AIR BAG PART NUMBER HO48X48 (SIZE 48" X 48") MANUFACTURED BY CENTERLOAD SHIPPING TECHNOLOGIES HAS BEEN TESTED AND APPROVED FOR USE BY THE US ARMY DEFENSE AMMUNITION CENTER. SEE THEIR TEST REPORT NO. 05-12 (MAY 2005). OTHER VENDORS' AIR BAGS MAY BE QUALIFIED FOR USE BY SUBMITTING SAMPLES FOR TEST AND EVALUATION TO THE DEFENSE AMMUNITION CENTER, TRANSPORTATION ENGINEERING DIVISION, 1C TREE ROAD, MCALISTER, OK 74501-9053.
  - C. VINYL DUNNAGE BAGS SHALL BE POSITIONED AS SHOWN IN THE APPROPRIATE ILLUSTRATIONS.
  - D. INFLATION/DEFLATION PROCEDURES SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.

## GENERAL NOTES (CONT'D):

15. AFTER THE BLOCKING AND BRACING HAS BEEN INSPECTED, ATTACH THE SHIPPING DOCUMENTS TO AN ACCESSIBLE AREA.
16. APPROPRIATE PLACARDS SHALL BE ATTACHED TO THE BOTH SIDES, FRONT, AND REAR OF THE TRACTOR/TRAILER.
17. UNLESS OTHERWISE SPECIFIED, ALL MATERIAL SHALL BE AS SPECIFIED IN THE GENERAL TRUCKLOADING DOCUMENT, MIL-STD-1320 (NAVY).

## SHIPPING EMPTY CONTAINERS:

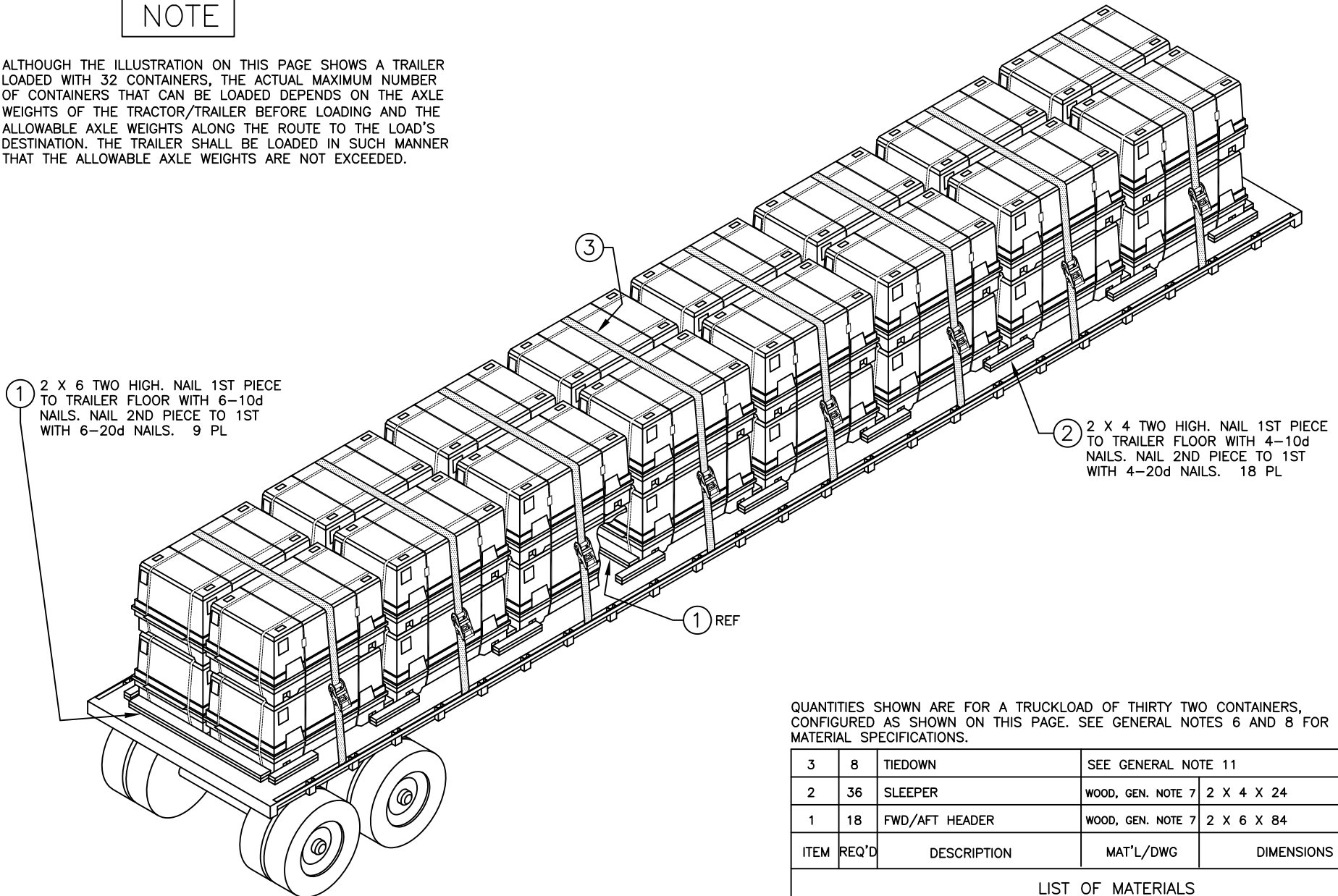
1. EMPTY CONTAINERS MAY BE STACKED UP TO THREE HIGH OR IN ACCORDANCE WITH NAVSEA DRAWING 6214495 FOR BOTH FLATBED AND VAN TRAILERS. WHEN SHIPPING EMPTY CONTAINERS, FOLLOW THE SAME ARRANGEMENTS FOR LOADED CONTAINERS AS SHOWN ON PAGES 4 AND 6 FOR FLATBED TRAILERS AND PAGES 7 THRU 9 FOR VAN TRAILERS.
2. FLOOR BLOCKING SHALL BE THE SAME AS WHEN SHIPPING LOADED CONTAINERS.
3. UNITIZING PROCEDURE SHALL BE AS SHOWN IN NAVSEA DRAWING 6214068 AND IN DETAIL A ON PAGE 6.

# TYPICAL FULL TRUCKLOAD OF LOADED CONTAINERS - FLATBED TRAILER

(SEE NOTE ON THIS PAGE REGARDING MAX QUANTITY)

## NOTE

ALTHOUGH THE ILLUSTRATION ON THIS PAGE SHOWS A TRAILER LOADED WITH 32 CONTAINERS, THE ACTUAL MAXIMUM NUMBER OF CONTAINERS THAT CAN BE LOADED DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE AXLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE ALLOWABLE AXLE WEIGHTS ARE NOT EXCEEDED.



① 2 X 6 TWO HIGH. NAIL 1ST PIECE TO TRAILER FLOOR WITH 6-10d NAILS. NAIL 2ND PIECE TO 1ST WITH 6-20d NAILS. 9 PL

② 2 X 4 TWO HIGH. NAIL 1ST PIECE TO TRAILER FLOOR WITH 4-10d NAILS. NAIL 2ND PIECE TO 1ST WITH 4-20d NAILS. 18 PL

① REF

QUANTITIES SHOWN ARE FOR A TRUCKLOAD OF THIRTY TWO CONTAINERS, CONFIGURED AS SHOWN ON THIS PAGE. SEE GENERAL NOTES 6 AND 8 FOR MATERIAL SPECIFICATIONS.

3	8	TIEDOWN	SEE GENERAL NOTE 11
2	36	SLEEPER	WOOD, GEN. NOTE 7 2 X 4 X 24
1	18	FWD/AFT HEADER	WOOD, GEN. NOTE 7 2 X 6 X 84
ITEM	REQ'D	DESCRIPTION	MAT'L/DWG DIMENSIONS

### LIST OF MATERIALS

NAVSEA  
DWG NO.

6214089

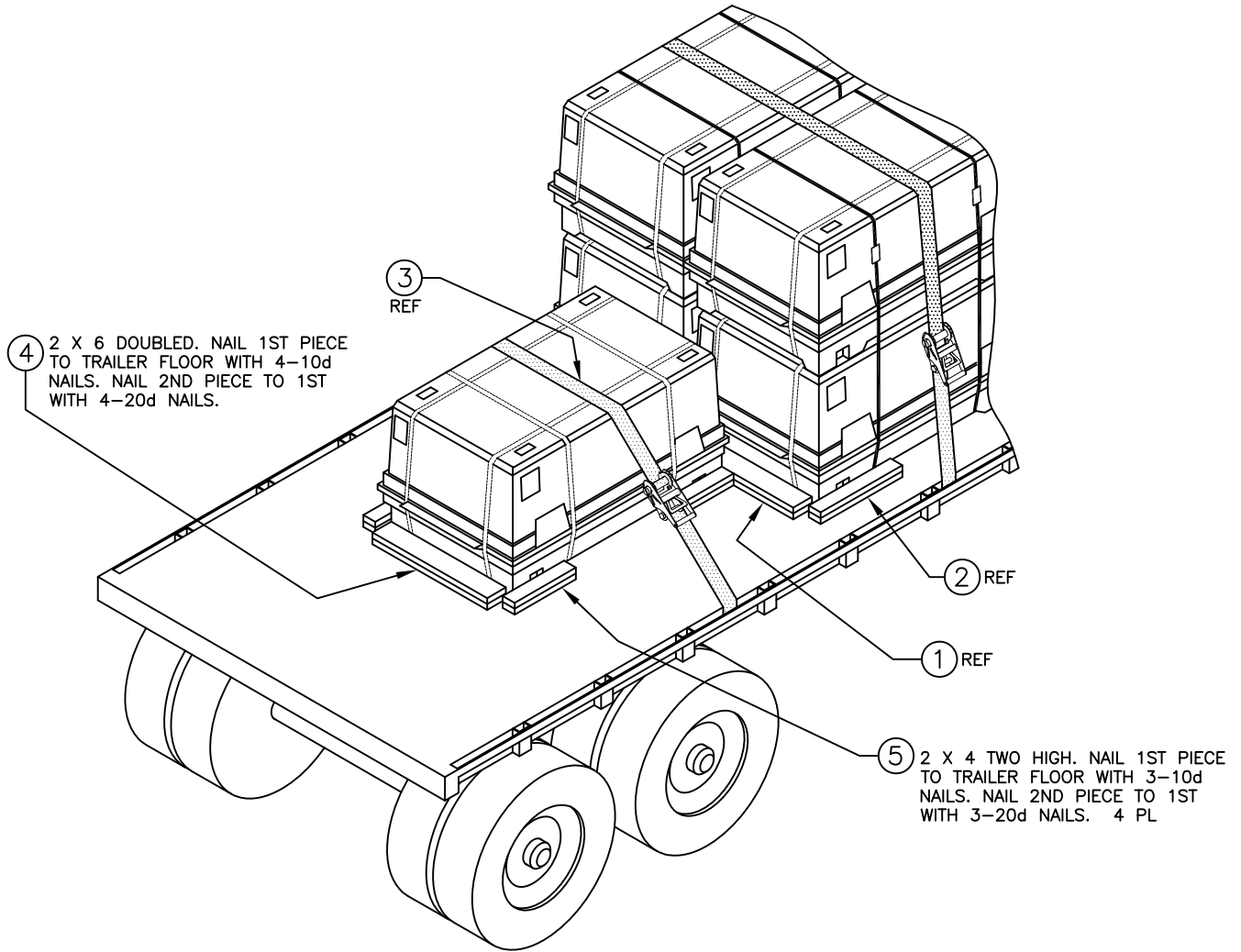
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# BLOCKING & BRACING - SINGLE WIDE LOAD



5	AR	SLEEPER (SINGLE WIDE LOAD)	WOOD, GEN. NOTE 7	2 X 4 X 18
4	AR	FWD/AFT HEADER (SINGLE WIDE LOAD)	WOOD, GEN. NOTE 7	2 X 6 X 36
ITEM	REQ'D	DESCRIPTION	MAT'L/DWG	DIMENSIONS

## LIST OF MATERIALS

NAVSEA  
DWG NO.

6214089

REV. C

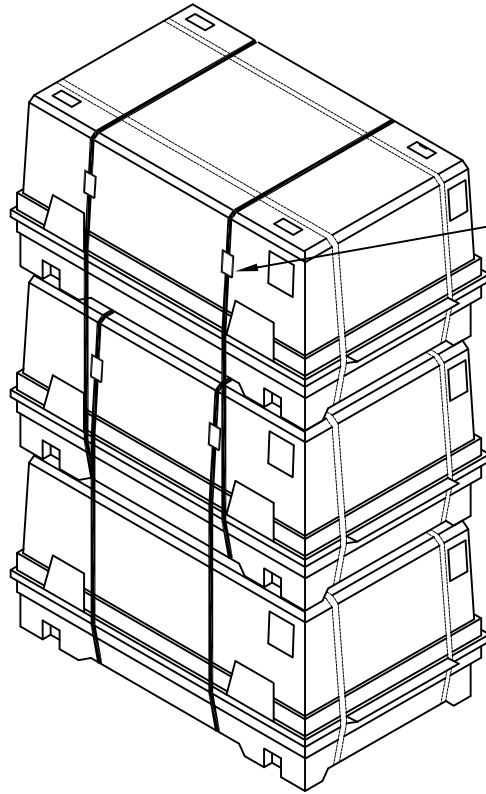
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CAGE CODE  
53711

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# DETAIL A

## UNITIZING PROCEDURE



- 6 7 1-1/4 UNITIZING STRAP & SEAL,  
2 PL. LENGTH AS REQUIRED.
- 8 9 3/4 UNITIZING STRAP & SEAL,  
2 PL. LENGTH AS REQUIRED  
FOR EMPTY CONTAINERS.

**UNITIZING PROCEDURE:**

1. THREE-HIGH STACKING OF CONTAINERS IS PERMITTED WHEN LOADING INSIDE VAN TRAILERS ONLY.
2. STACK 2ND CONTAINER ON TOP OF LOWER CONTAINER AND SECURE WITH STEEL STRAPPING, ITEM 1, TENSION AND SECURE EACH STRAP WITH ONE DOUBLE NOTCHED SEAL, ITEM 2, AS SHOWN IN DRAWING 53711-6214068. MAKE SURE THAT THE STACKING FEATURES ARE ALIGNED BEFORE APPLYING THE STRAPPING.
3. SECURE 3RD CONTAINER TO 2ND IN LIKE MANNER.
4. THE UNITIZING OF CONTAINERS IS AUTHORIZED AS PART OF THE TRUCKLOADING PROCEDURE ONLY. MOVEMENT OF UNITIZED CONTAINERS SHALL BE LIMITED TO THAT NECESSARY TO LOAD AND UNLOAD THE VEHICLE. CONTAINERS SHALL BE DE-UNITIZED (I.E., THE STRAPS REMOVED) AFTER UNLOADING FROM THE VEHICLE.
5. UNITIZED CONTAINERS SHALL BE LIFTED USING THE FORKLIFT POCKETS OF THE LOWER CONTAINER ONLY.

QUANTITIES SHOWN FOR ITEMS 6 AND 7 ARE FOR UNITIZING THREE LOADED CONTAINERS. QUANTITIES SHOWN FOR ITEMS 8 AND 9 ARE FOR UNITIZING THREE EMPTY CONTAINERS.

9	4	SEAL (FOR EMPTY CONTAINERS)	STEEL, GEN. NOTE 5	FOR 3/4" STRAPPING
8	4	UNITIZING STRAP (FOR EMPTY CONTAINERS)	STEEL, GEN. NOTE 5	3/4 X .031-.035
7	4	SEAL	STEEL, GEN. NOTE 5	FOR 1-1/4" STRAPPING
6	4	UNITIZING STRAP	STEEL, GEN. NOTE 5	1-1/4 X .031-.035
ITEM	REQ'D	DESCRIPTION	MAT'L/DWG	DIMENSIONS

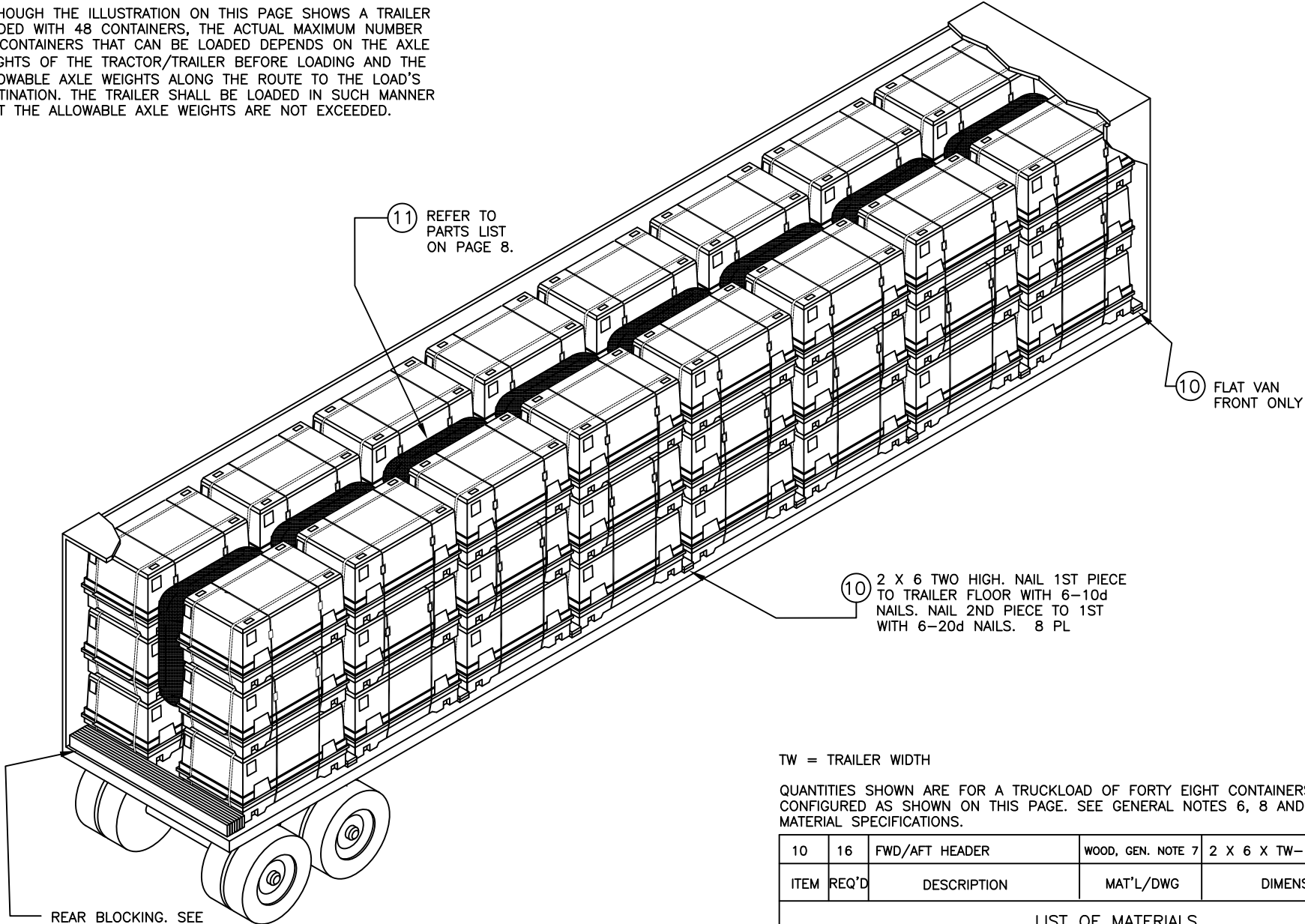
LIST OF MATERIALS

**NOTE**

**FULL TRUCKLOAD - VAN TRAILER**

(SEE NOTE ON THIS PAGE REGARDING MAX QUANTITY)

ALTHOUGH THE ILLUSTRATION ON THIS PAGE SHOWS A TRAILER LOADED WITH 48 CONTAINERS, THE ACTUAL MAXIMUM NUMBER OF CONTAINERS THAT CAN BE LOADED DEPENDS ON THE AXLE WEIGHTS OF THE TRACTOR/TRAILER BEFORE LOADING AND THE ALLOWABLE AXLE WEIGHTS ALONG THE ROUTE TO THE LOAD'S DESTINATION. THE TRAILER SHALL BE LOADED IN SUCH MANNER THAT THE ALLOWABLE AXLE WEIGHTS ARE NOT EXCEEDED.



TW = TRAILER WIDTH

QUANTITIES SHOWN ARE FOR A TRUCKLOAD OF FORTY EIGHT CONTAINERS, CONFIGURED AS SHOWN ON THIS PAGE. SEE GENERAL NOTES 6, 8 AND 12 FOR MATERIAL SPECIFICATIONS.

10	16	FWD/AFT HEADER	WOOD, GEN. NOTE 7	2 X 6 X TW-1
ITEM	REQ'D	DESCRIPTION	MAT'L/DWG	DIMENSIONS

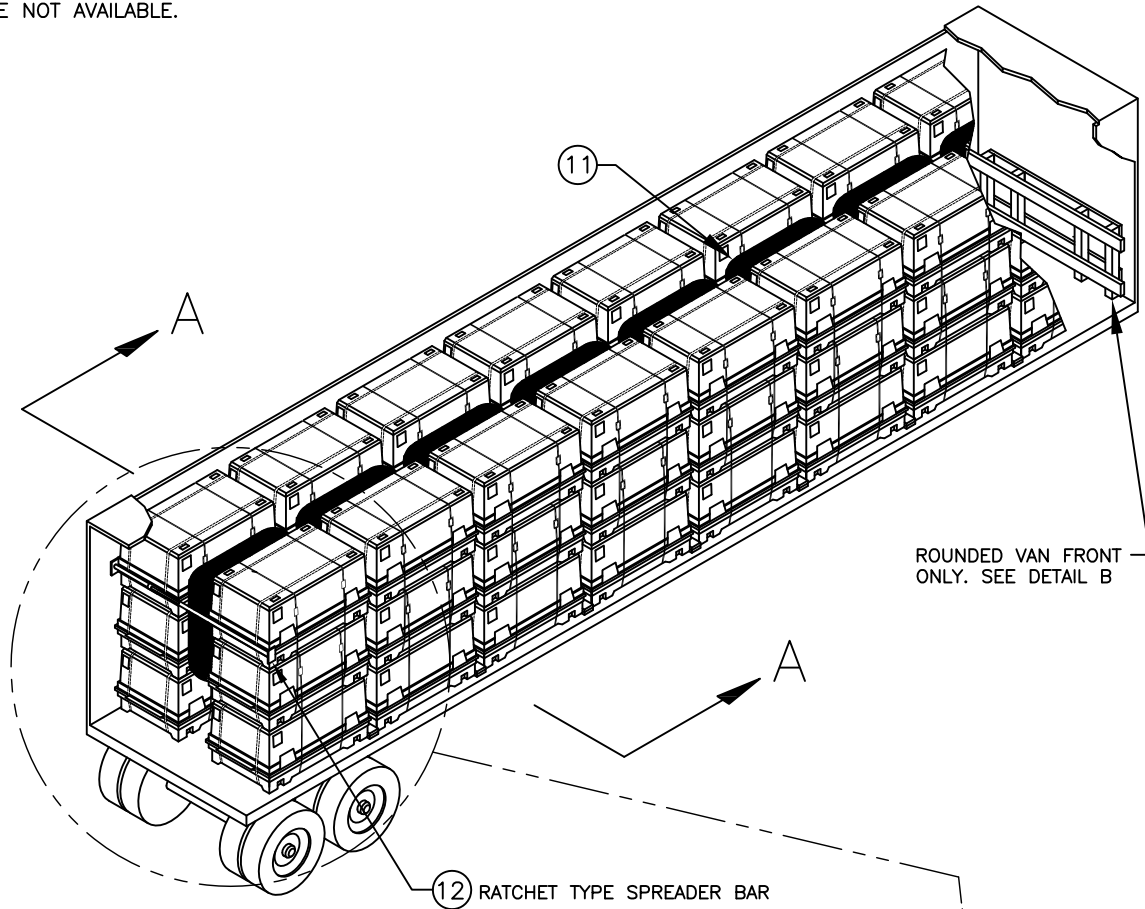
LIST OF MATERIALS

NAVSEA DWG NO.	6214089	REV. C	SIZE A	CAGE CODE 53711	PAGE 7 OF 11
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# FULL TRUCKLOAD - VAN TRAILER

## NOTE

THIS DRAWING ILLUSTRATES THE PREFERRED METHOD OF BLOCKING AND BRACING USING INFLATABLE VINYL BAGS. USE ANTI-SWAY BRACES, SHOWN ON PAGES 9 AND 10, ONLY WHEN THESE BAGS ARE NOT AVAILABLE.



SECTION A-A  
TYPICAL PLACEMENT OF INFLATABLE BAG



FULL LOADED VAN TRAILER WITH SPREADER BAR

12	AR	SPREADER BAR, RATCHET		ADJUSTABLE
11	AR	INFLATABLE BAG	VINYL, GEN. NOTE 14	48 X 48
ITEM	REQ	DESCRIPTION	MAT'L/DWG	DIMENSIONS
LIST OF MATERIALS				

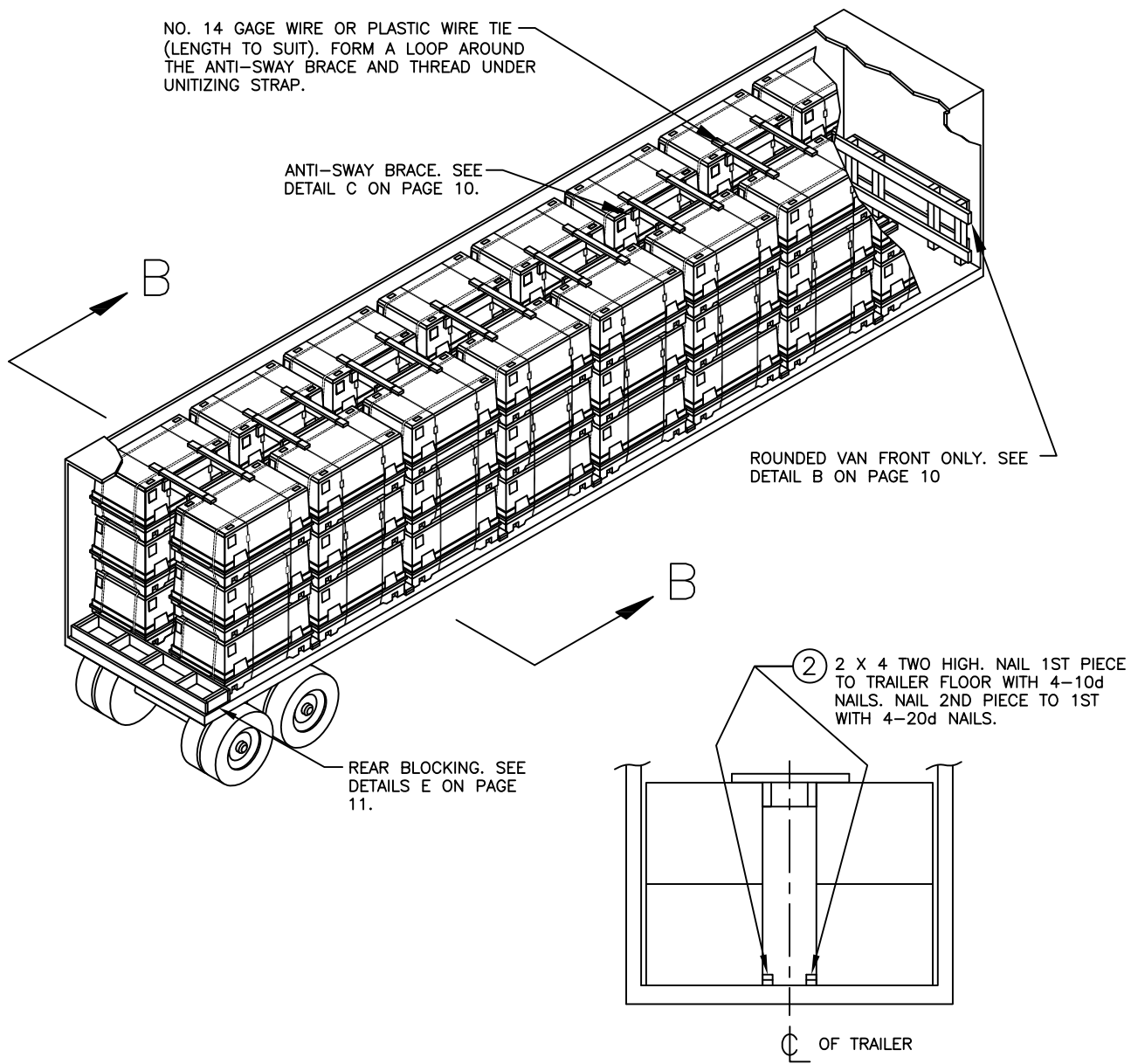


# FULL TRUCKLOAD - VAN TRAILER - ALTERNATE METHOD

## NOTES

1. THIS DRAWING ILLUSTRATES THE ALTERNATE METHOD OF BLOCKING AND BRACING USING ANTI-SWAY BRACES. ANTI-SWAY BRACES SHOULD ONLY BE USED WHEN INFLATABLE BAGS ARE NOT AVAILABLE.

2. THIS DRAWING ILLUSTRATES A LOAD WHEN THE DISTANCE BETWEEN THE LANDING AND THE TRAILER DOORS IS BETWEEN 12 AND 36 INCHES. SEE DETAILS ON PAGE 11 FOR THIS AND OTHER CONFIGURATIONS.

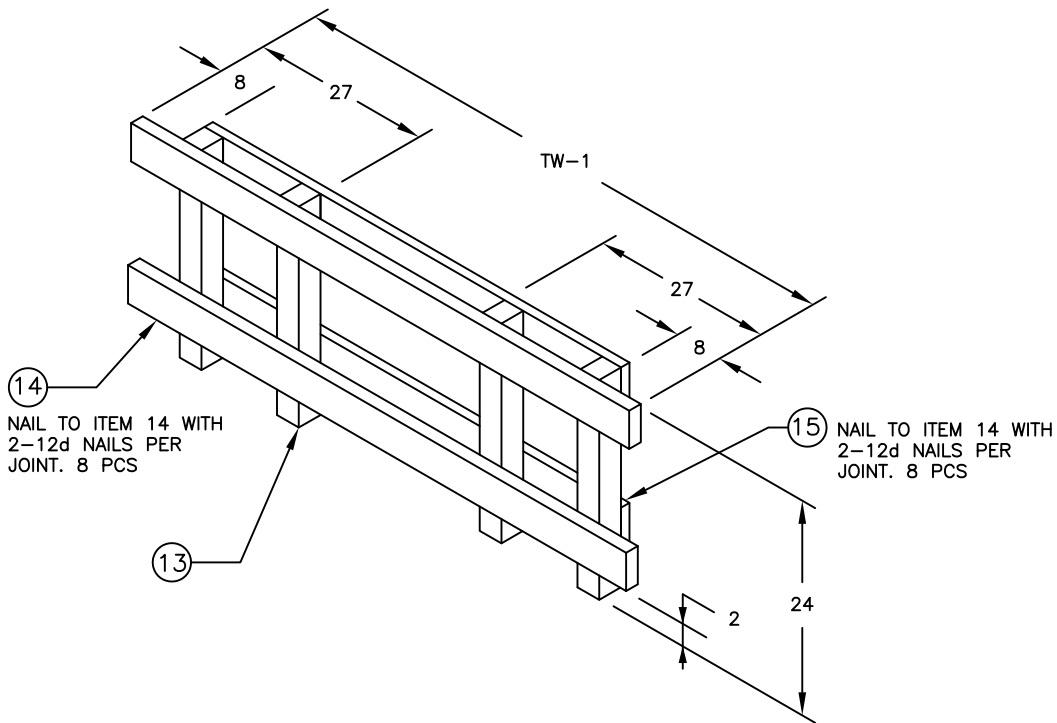


### SECTION B-B

REFER TO PARTS LIST ON PAGE 4.

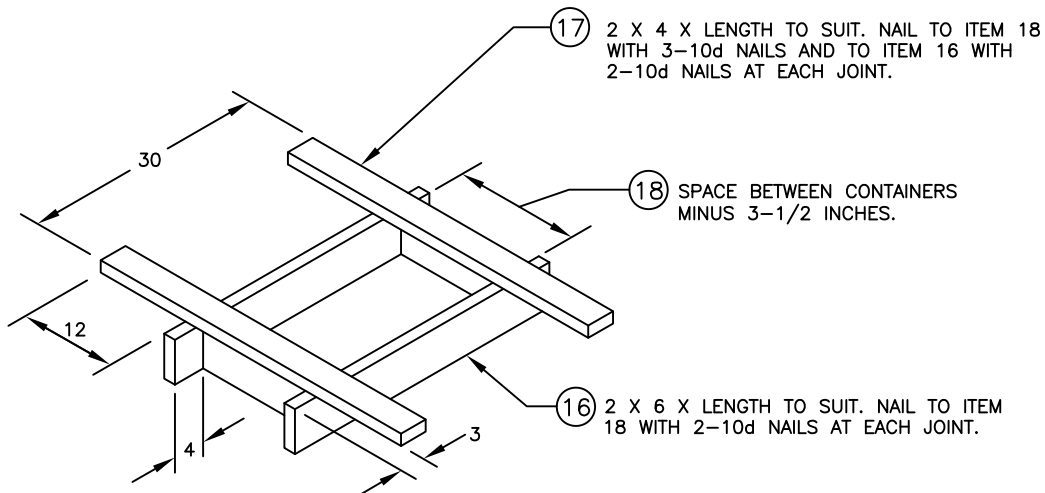
# DETAIL B

FORWARD BULKHEAD ASSEMBLY FOR VANS WITH ROUNDED FRONTS



# DETAIL C

ANTI-SWAY BRACE



TW = TRAILER WIDTH

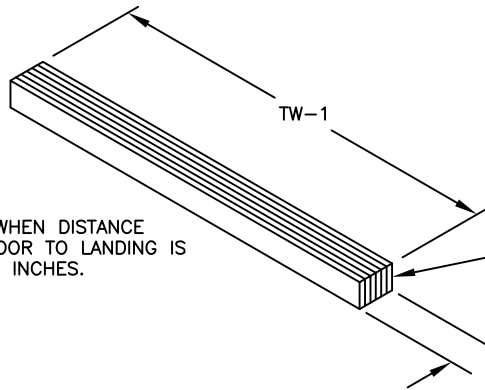
18	AR	SPACER PIECE	WOOD, GEN. NOTE 7	2 X 6 X AR
17	AR	SUPPORT PIECE	WOOD, GEN. NOTE 7	2 X 4 X AR
16	AR	BUFFER PIECE	WOOD, GEN. NOTE 7	2 X 6 X 38
15	2	SUPPORT, HORIZONTAL	WOOD, GEN. NOTE 7	2 X 6 X TW-16
14	2	SUPPORT, HORIZONTAL	WOOD, GEN. NOTE 7	2 X 6 X TW-1
13	4	POST, VERTICAL	WOOD, GEN. NOTE 7	4 X 4 X 24
ITEM	REQ	DESCRIPTION	MAT'L/DWG	DIMENSIONS

LIST OF MATERIALS

# DETAIL D

REAR BLOCKING

TABLE 1	
DISTANCE (IN.)	QUANTITY
1-1/2 TO 2-7/8	1
3 TO 4-3/8	2
4-1/2 TO 5-7/8	3
6 TO 7-3/8	4
7-1/2 TO 8-7/8	5
9 TO 10-3/8	6
10-1/2 TO 11-7/8	7



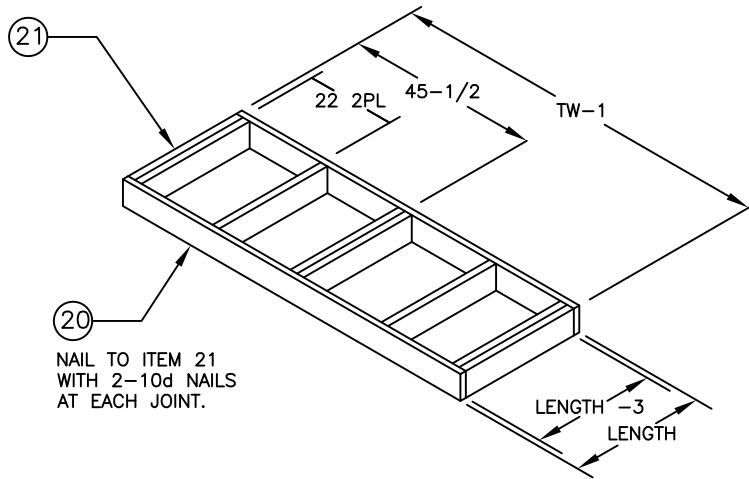
TO BE USED WHEN DISTANCE FROM REAR DOOR TO LANDING IS LESS THAN 12 INCHES.

(19) NAIL TOGETHER WITH 8-10d NAILS.

SEE TABLE 1 FOR QUANTITY OF 2 X 6'S

# DETAIL E

REAR BLOCKING



NAIL TO ITEM 21 WITH 2-10d NAILS AT EACH JOINT.

TO BE USED WHEN LADING IS BETWEEN 12 AND 36 INCHES FROM REAR DOOR.

TW = TRAILER WIDTH

21	8	BACKUP CLEAT	WOOD, GEN. NOTE 7	2 X 6 X AR
20	2	CROSSMEMBER	WOOD, GEN. NOTE 7	2 X 6 X TW-1
19	AR	AFT HEADER	WOOD, GEN. NOTE 7	2 X 6 X TW-1
ITEM	REQ	DESCRIPTION	MAT'L/DWG	DIMENSIONS
LIST OF MATERIALS				