

REV NO 1 APPROVED BY
MTMC

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DATE 13 Aug 79

CHAPARRAL

MINIMUM REQUIREMENTS FOR THE HANDLING, STOWAGE, AND BRACING ABOARD SHIPS OF CARRIER, SELF- PROPELLED, M730, W/O FIRE UNIT

THIS DRAWING, INCLUDING REVISION 1, SUPERSEDES
DRAWING 19-48-7258-GSE18CH3, DATED AUGUST 1972.

DO NOT SCALE

| REVISIONS | | | | DRAFTSMAN | PROJ ENG | DRSMI-SP |
|-----------|--|--|--|---|--------------------|--------------------|
| | | | | <i>P. B. MWD</i> | <i>W. F. Ennis</i> | <i>W. F. Ennis</i> |
| | | | | CHECKER | LOC ENGINE OFFICE | |
| | | | | <i>W. F. Ennis</i> | | |
| | | | | APPROVED, U.S. ARMY MISSILE COMMAND | | |
| | | | | <i>W. F. Ennis</i> | | |
| | | | | APPROVED BY ORDER OF COMMANDING GENERAL, U.S. ARMY MATERIEL DEVELOPMENT AND READINESS COMMAND (DARCOM) | | |
| | | | | U. S. ARMY DEFENSE AMMUNITION CENTER AND SCHOOL | | |
| | | | | U. S. ARMY DARCOM DRAWING | | |
| | | | | JULY 1979 | | |
| | | | | CLASS | DIVISION | DRAWING |
| | | | | 19 | 48 | 7258 |
| | | | | | | FILE |
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NOTES

GENERAL:

- A. THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AR 740-1.
- B. THIS DRAWING DEPICTS MINIMUM PROCEDURES APPLICABLE TO THE HANDLING, STOWAGE, AND BRACING ABOARD SHIPS OF THE CARRIER, SELF-PROPELLED, M730, W/O FIRE UNIT, FOR THE CHAPARRAL MISSILE SYSTEM.
- C. THE CARRIER SHOWN HEREIN MAY BE STOWED IN THE SAME HOLD OR TWEEN DECK WITH OTHER TYPES OF CARGO. HOWEVER, IF THIS ITEM IS TO BE STOWED ADJACENT TO DANGEROUS ARTICLES, PERTINENT REQUIREMENTS OF COAST GUARD REGULATION NO. CG 108 MUST BE APPLIED.
- D. LADING DATA:

DIMENSIONS ----- 19'-3-1/4" LONG X 8'-9-3/4" WIDE X 6'-4" HIGH.
GROSS WEIGHT ---- 16,233 LBS (APPROX).
CUBE ----- 1,076 CU. FT.

HANDLING:

- A. PERTINENT PROVISIONS OF TITLE 46 CODE OF FEDERAL REGULATIONS APPLY.
- B. HANDLING OF THE CARRIER SHOULD BE ACCOMPLISHED BY USING THE "SPECIAL LIFTING DEVICE" (APN5952332) AND THE LIFT POINTS DESIGNATED HEREIN. HOWEVER, IF THE CARRIER IS EQUIPPED WITH TOP REAR LIFTING LOOPS, AS INDICATED IN THE ELEVATION VIEW ON PAGE 3, THE HANDLING SLING MAY BE ATTACHED DIRECTLY TO THE TOP REAR LOOPS, IN LIEU OF UTILIZING THE "SPECIAL LIFTING DEVICE".
- C. THE HANDLING SLING SHALL BE OF A DESIGN AND CONFIGURATION TO LIFT THE ITEM IN SUCH A MANNER THAT THE CABLE LEGS DO NOT COME IN CONTACT WITH OR APPLY PRESSURE AGAINST THE FRAMING, SUPERSTRUCTURE OR OTHER MEMBERS OF THE ITEM WHEN BEING LIFTED.
- D. EACH LEG OF THE HANDLING SLING MUST BE SECURELY ATTACHED TO A LIFT POINT PRIOR TO LIFTING.
- E. ALTHOUGH DESIRABLE, A LEVEL LIFT IS NOT MANDATORY. THE CENTER OF GRAVITY OF THIS ITEM IS SHOWN TO ASSIST IN DETERMINING CABLE LENGTHS TO ASSURE A SAFE LIFT.

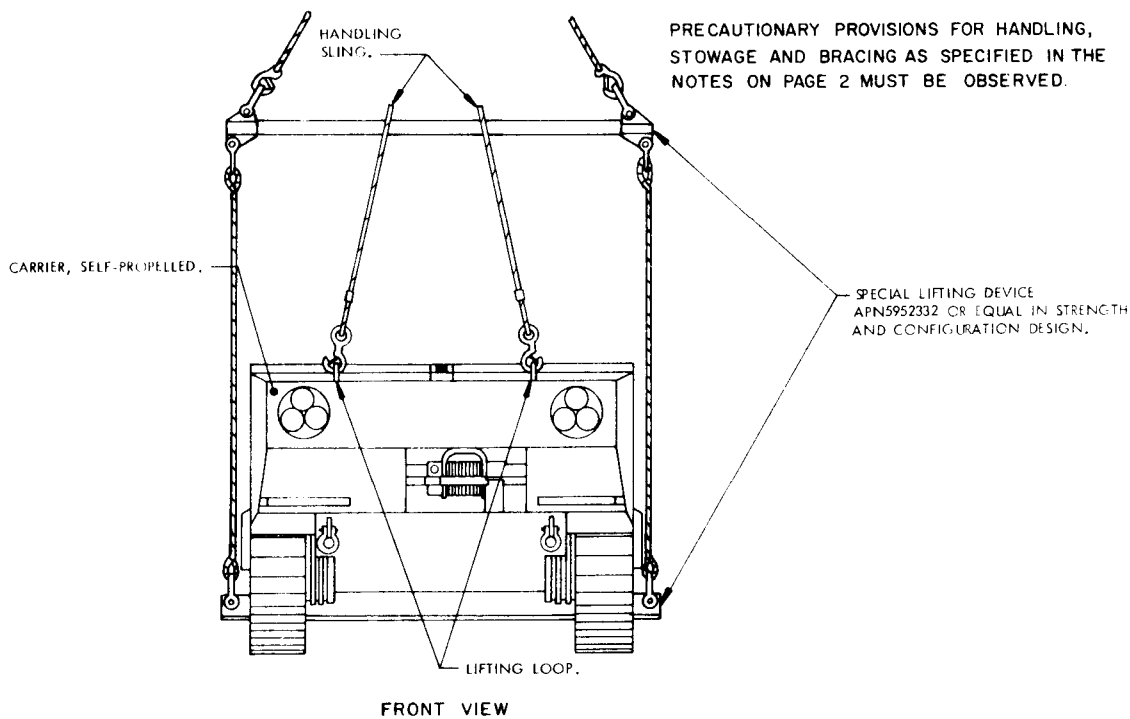
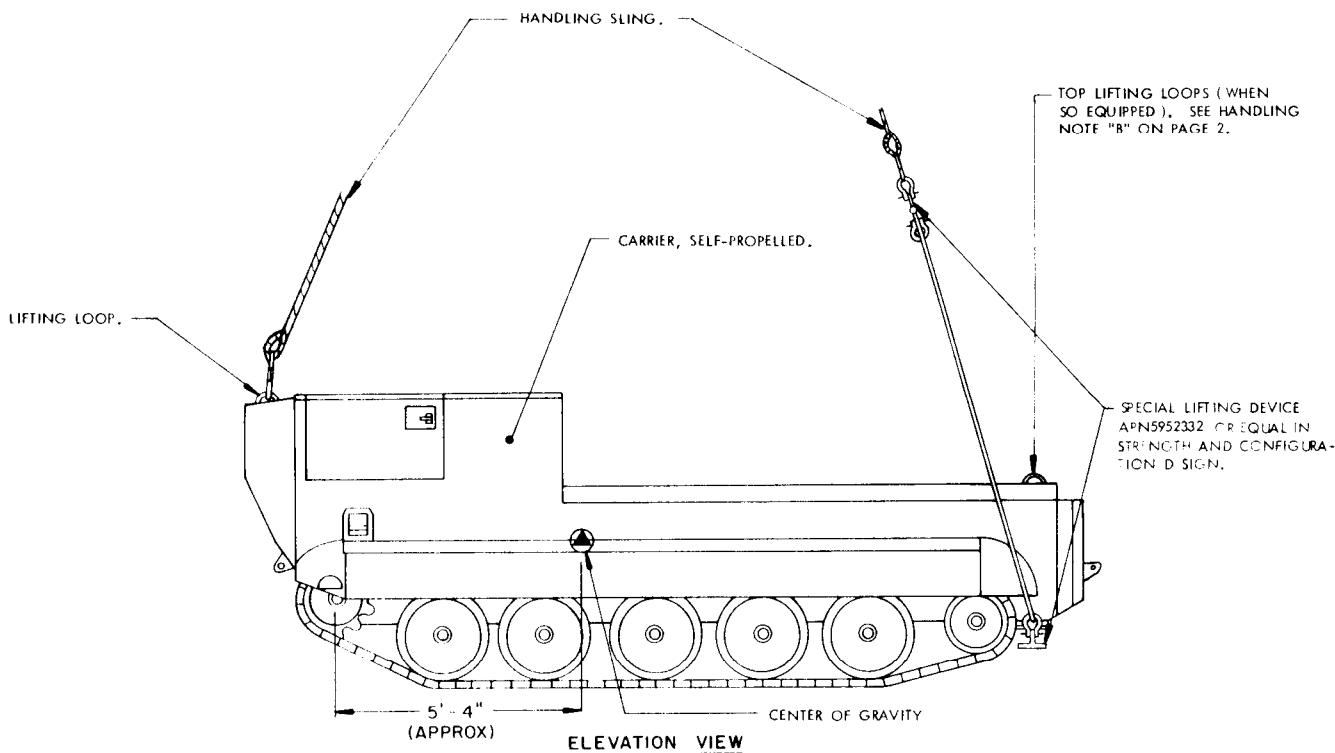
STOWAGE AND BRACING:

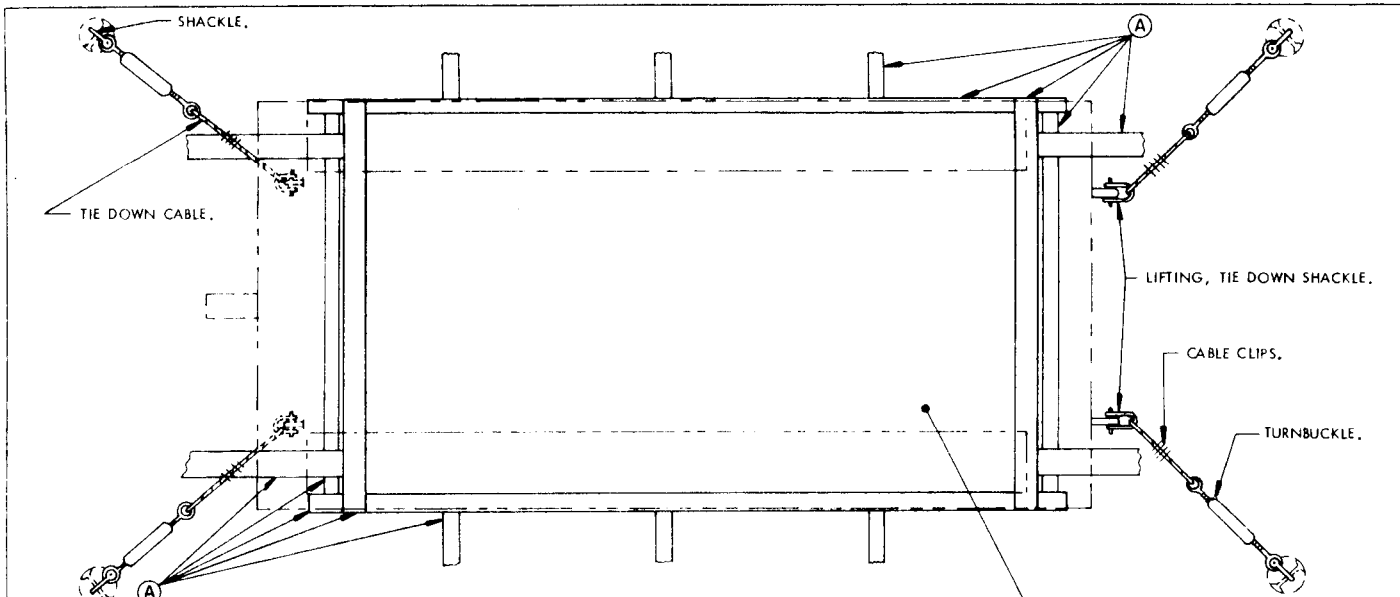
- A. WHEN STOWING THE CARRIER WITHIN THE HOLD OR TWEEN DECK, BRACING BETWEEN CARRIERS OR BETWEEN A CARRIER AND OTHER CARGO SHALL ONLY BE APPLIED AGAINST THE TRACK WHEELS AND/OR TRACKS OF THE VEHICLE. ALSO, OTHER CARGO ITEMS MUST NOT BE STACKED ON TOP OF OR BE BRACED AGAINST ANY OTHER PART OF THE CARRIER THAN AS IDENTIFIED IMMEDIATELY ABOVE.
- B. SPECIES, GRADE AND SIZE OF LUMBER TO BE USED WILL COMPLY WITH REQUIREMENTS OF CURRENT SHIPWRIGHT-CARPENTRY AND RELATED SERVICES CONTRACTS. BRACING METHODS AND LUMBER SIZES DEPICTED IN THIS DRAWING ARE CONSIDERED MINIMUM AND ARE NOT INTENDED TO CONFLICT WITH CONTRACT REQUIREMENTS.
- C. THE CARRIER IS EQUIPPED WITH TIE DOWN DEVICES AS SHOWN HEREIN WHICH ARE SYMMETRICAL ABOUT THE LONGITUDINAL LINE OF CENTER. THE LAUNCHER SHOULD BE SECURED FORE AND AFT TO SHIP TIE DOWN POINTS USING A MINIMUM OF TWO SYMMETRICAL LOCATED TIE DOWN DEVICES IN EACH DIRECTION AS TYPICALLY SHOWN WITHIN THIS DOCUMENT.
- D. A TYPICAL TIE DOWN PROCEDURE IS SHOWN AND DEPICTS THE PREFERRED METHOD OF USING CABLES AND TURNBUCKLES. IF USED, A TURNBUCKLE MUST BE OF A SIZE EQUAL IN STRENGTH TO THE MINIMUM SIZE CABLE REQUIRED TO SECURE THE ITEM. IN LIEU OF CABLES AND TURNBUCKLES, SECUREMENT MAY BE ACCOMPLISHED BY OTHER ACCEPTED METHODS.

REVISIONS

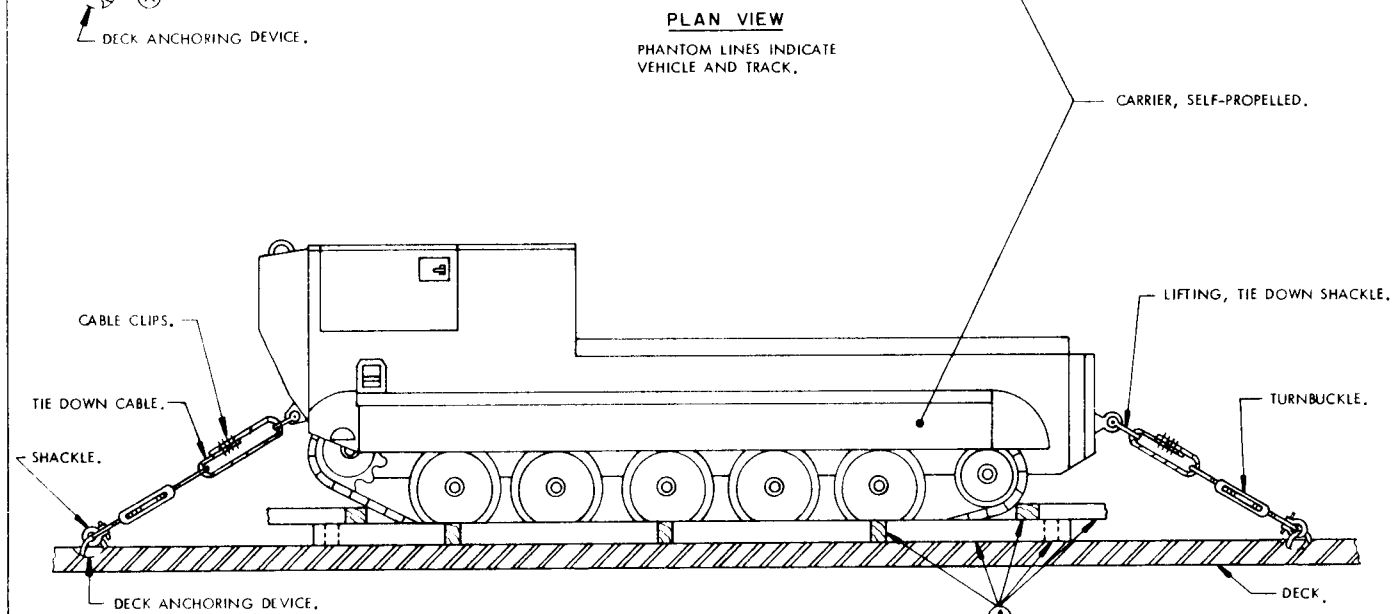
REVISION NO. 1, DATED JULY 1979, CONSISTS OF:

- 1. DELETING BOW MOUNTING BRACKETS.
- 2. SHOWING LIFT PROCEDURES USING SPECIAL LIFTING DEVICE.
- 3. CHANGING LADING DATA.
- 4. DELETING X FROM XM730.
- 5. CHANGING PROCEDURES TO CONFORM TO CURRENT STANDARDS.



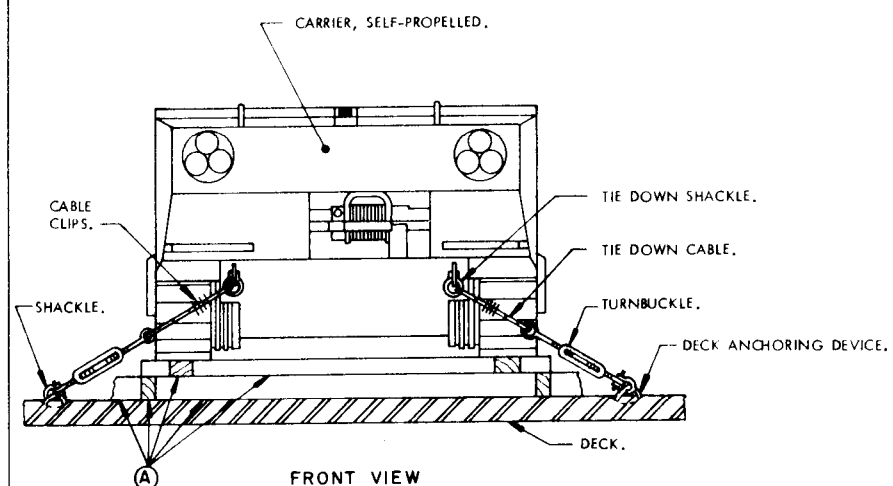


PLAN VIEW
 PHANTOM LINES INDICATE
 VEHICLE AND TRACK.



ELEVATION VIEW

| | |
|-------------------------|---------|
| MINIMUM LUMBER SIZES | |
| (A) | 4" X 6" |



FRONT VIEW

PRECAUTIONARY PROVISIONS FOR HANDLING,
 STOWAGE AND BRACING AS SPECIFIED IN THE
 NOTES ON PAGE 2 MUST BE OBSERVED.