

GENERAL NOTES

- A. THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AR 740-1, AND AUGMENTS TM 743-200-1 (CHAPTER 5).
- B. THE OUTLOADING PROCEDURES CONTAINED IN THIS DRAWING ARE APPLICABLE TO THE BLU-109/B BOMB IN A CNU-416/E SHIPPING AND STORAGE CONTAINER (PALLET UNIT). SUBSEQUENT REFERENCE TO CONTAINER HEREIN MEANS THE CONTAINER WITH BOMBS.
- C. THE OUTLOADING PROCEDURES DEPICTED WITHIN THIS DOCUMENT ARE APPLICABLE FOR SHIPMENTS IN CONVENTIONAL TYPE VAN TRAILERS HAVING WOOD OR WOOD AND METAL, OR ALL METAL FLOORS. VAN TRAILERS WHICH ARE 45'-0" LONG BY 7'-6" WIDE (INSIDE DIMENSION) AND 40'-0" LONG BY 8'-0" WIDE HAVE BEEN SHOWN, HOWEVER, THE PROCEDURES ARE ALSO APPLICABLE FOR TRAILERS WHICH ARE NINETY-ONE INCHES (91") THRU NINETY-NINE (99") IN WIDTH AND FOR TRAILERS OF OTHER LENGTHS FROM THE SHORTEST TO THE LONGEST AVAILABLE (REF: 24' TO 53'), AND FOR STRAIGHT TRUCK VANS. THE LOADING AND BRACING PROCEDURES SPECIFIED HEREIN ARE ALSO ADEQUATE (CONFIGURATION-WISE AND STRENGTH-WISE) FOR LOADS IN SHORTER OR LONGER VANS THAN SHOWN. THE SPECIFIED BRACING IS ADEQUATE FOR LOADS WEIGHING UP TO AND INCLUDING THE MAXIMUM WEIGHTS PERMITTED BY LAW.
- D. SELECTION OF A VEHICLE TO BE USED TO TRANSPORT THE DESIGNATED ITEM MUST COMPLY WITH AR 55-355, CHAPTER 29, FOR EXPLOSIVES AND OTHER DANGEROUS ARTICLES, IN FULL.
- E. THE GROSS WEIGHT AND AXLE DISTRIBUTION OF WEIGHT FOR A LOAD WILL BE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THE APPLICABLE LOADING REQUIREMENTS, AND THE SHIPPER WILL LOAD ACCORDINGLY. THE TOTAL WEIGHT OF THE LADING, OF THE DUNNAGE, OF THE TRACTOR, AND OF THE SEMI-TRAILER CARRYING THE LADING MUST NOT EXCEED THE MAXIMUM GROSS WEIGHT ALLOWED FOR THE STATE OR STATES THRU WHICH THE LOAD IS TO BE TRANSPORTED BY MOTOR CARRIER. LIKEWISE, THE GROSS WEIGHT ON A SINGLE OR TANDEM AXLE MUST NOT EXCEED THE MAXIMUM ALLOWABLE WEIGHT. IF THERE IS ANY DOUBT AS TO WHETHER THE TOTAL GROSS WEIGHT OR AXLE WEIGHT EXCEEDS THE MAXIMUM ALLOWED, WEIGHTS SHOULD BE VERIFIED BY ACTUALLY WEIGHING THE LOADED VEHICLE.
- F. NOTICE: A SHIPMENT WILL BE POSITIONED IN THE TRAILER CONSISTENT WITH STATE WEIGHT LAWS. THE NUMBER OF LADING UNITS MAY BE ADJUSTED TO FIT THE SIZE OF THE TRAILER TO BE LOADED OR THE QUANTITY TO BE SHIPPED. COMBINATIONS OF THE OUTLOADING PROCEDURES SPECIFIED MAY BE USED; HOWEVER, THE APPROVED METHODS SHOWN MUST BE FOLLOWED AS CLOSELY AS POSSIBLE FOR BLOCKING, BRACING, AND STAYING OF THE DESIGNATED ITEMS.
- G. THE "LOAD AS SHOWN" FOR THE FULL LOAD DEPICTED HEREIN IS BASED ON AN APPROXIMATE LADING WEIGHT OF 43,370 POUNDS. FOR LESS THAN FULL LOADS, REFER TO THE GUIDANCE CONTAINED ON PAGES 8 THRU 11.
- H. OTHER TYPES OF LADING ITEMS MAY BE LOADED INTO TRAILERS WHICH ARE PARTIALLY LOADED WITH CONTAINERS OF BLU-109/B BOMBS, PROVIDING THE TOTAL LOAD IS COMPATIBLE, EXISTING DIRECTIVES ARE NOT VIOLATED AND THE OTHER LADING ITEMS ARE BLOCKED AND BRACED TO EQUAL THE BLOCKING AND BRACING CRITERIA SPECIFIED HEREIN.
- J. ALL LOADS ARE SHOWN IN TRAILERS HAVING SQUARED CORNERS AT THE FORWARD END. IF THE CONVENTIONAL VAN TRAILER BEING USED IS EQUIPPED WITH A ROUNDED OR ANGLED FRONT, SEE THE SPECIAL PROCEDURES ON PAGE 14.
- K. DUNNAGE LUMBER SPECIFIED THROUGHOUT THIS PROCEDURAL DRAWING IS OF NOMINAL SIZE. FOR EXAMPLE, 2" X 4" MATERIAL IS ACTUALLY 1-1/2" THICK BY 3-1/2" WIDE AND 2" X 6" MATERIAL IS ACTUALLY 1-1/2" THICK BY 5-1/2" WIDE.
- L. NOTICE: A STAGGERED NAILING PATTERN WILL BE USED WHENEVER POSSIBLE WHEN NAILS ARE DRIVEN INTO JOINTS OF DUNNAGE ASSEMBLIES. ALSO, A STAGGERED NAILING PATTERN WILL BE USED WHEN DUNNAGE IS NAILED TO THE FLOOR OF THE TRANSPORTING VEHICLE, OR WHEN LAMINATING DUNNAGE. THE NAILING PATTERN WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL DOES NOT PENETRATE INTO OR NEAR A CRACK BETWEEN FLOOR BOARDS. ADDITIONALLY, THE NAILING PATTERN FOR AN UPPER PIECE OF LAMINATED DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN THROUGH, ONTO OR RIGHT BESIDE A NAIL IN A LOWER PIECE.

(CONTINUED AT RIGHT)

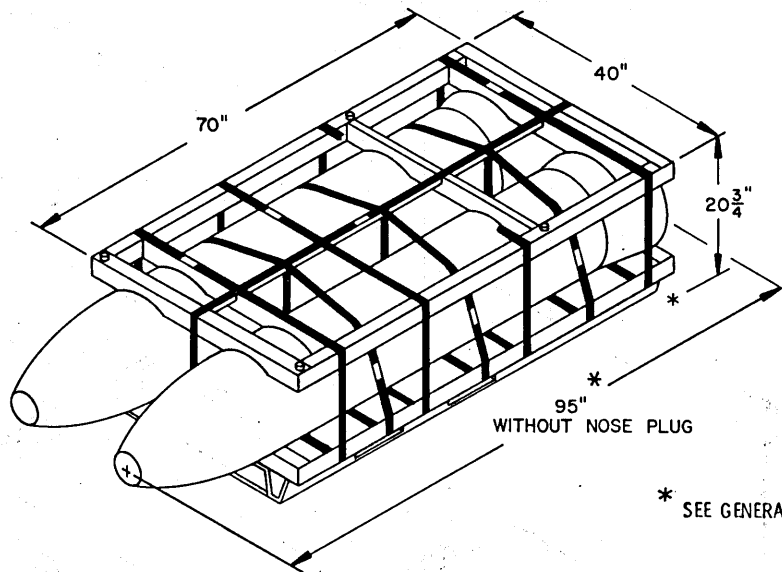
MATERIAL SPECIFICATIONS

LUMBER-----SEE TM 743-200-1, DUNNAGE LUMBER;
FED SPEC FF-N-105.

NAILS-----COMMON; FED SPEC FF-N-105.

(GENERAL NOTES CONTINUED)

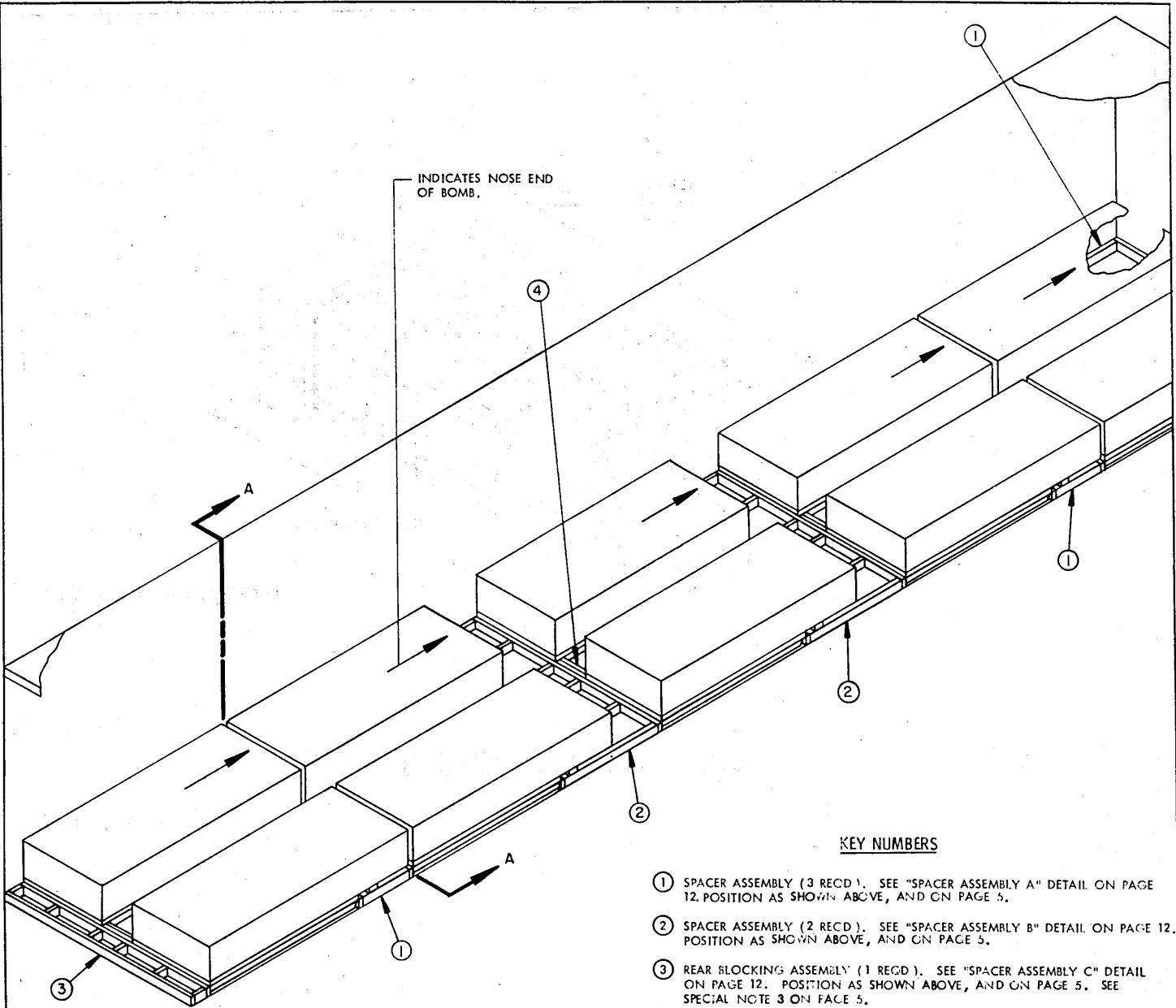
- M. POWER DRIVEN STAPLES MAY BE USED AS ALTERNATIVE FASTENERS FOR NAILS WHEN CONSTRUCTING DUNNAGE ASSEMBLIES WHICH ARE TO BE USED IN THE DELINEATED TRAILER LOADS SHOWN THROUGHOUT THIS DRAWING. THE STAPLES TO BE USED MUST BE EQUAL IN LENGTH TO THE SPECIFIED NAIL SIZE AND MUST BE SUBSTITUTED ON A ONE STAPLE FOR ONE NAIL BASIS. STAPLES WHICH ARE 2-1/2" OR LESS IN LENGTH SHOULD BE IN ACCORDANCE WITH FEDERAL SPECIFICATION FF-N-105 AS NEARLY AS PRACTICABLE. STAPLES WHICH ARE LONGER THAN 2-1/2" WILL BE A COMMERCIAL GRADE, OF A QUALITY EQUIVALENT TO THOSE MANUFACTURED BY SENGO PRODUCTS INCORPORATED. NOTE: STAPLES WILL NOT BE SUBSTITUTED FOR NAILS IN ANY LOAD RESTRAINING FLOOR DUNNAGE APPLICATION.
- N. PORTIONS OF THE TRAILERS, SUCH AS SIDEWALLS, END WALLS, AND ROOFS, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.
- O. FOR ADDITIONAL GUIDANCE, ATTENTION IS DIRECTED TO THE "SPECIAL NOTES" SECTIONS WHICH ARE IMMEDIATELY ADJACENT TO THE DEPICTED OUTLOADING METHODS.
- P. CONVERSION TO METRIC EQUIVALENTS: DIMENSIONS WITHIN THIS DOCUMENT ARE EXPRESSED IN INCHES, AND WEIGHTS ARE EXPRESSED IN POUNDS. WHEN NECESSARY, THE METRIC EQUIVALENTS MAY BE COMPUTED ON THE BASIS OF ONE INCH EQUALS 25.4MM AND ONE POUND EQUALS 0.454KG.
- Q. FOR DETAILS OF THE CNU-416/E SHIPPING AND STORAGE CONTAINER (PALLET UNIT), SEE U.S. AIR FORCE DRAWING NO. 8463196 AND THE "UNIT DETAIL" ON PAGE 3.
- CONTAINER DIMENSIONS ----- 95" LONG BY 40" WIDE BY 20-3/4" HIGH
- GROSS WEIGHT ----- 4,337 POUNDS (APPROX)
- R. NOTE: IF THE BOMBS BEING SHIPPED HAVE CONICAL NOSE PLUGS INSTALLED, THE OVERALL CONTAINER LENGTH WILL BE 99" IN LIEU OF THE 95" SPECIFIED. SPACER ASSEMBLIES WILL BE INCREASED OR DECREASED IN LENGTH AS REQUIRED TO ACCOMMODATE THE LONGER CONTAINER. ADDITIONALLY, WITH THE CONICAL NOSE PLUGS INSTALLED, THE PROCEDURES DELINEATED ON PAGES 6-7 AND 10-11 CANNOT BE USED DUE TO THE ADDITIONAL TRAILER WIDTH REQUIRED AND POSSIBLE TRAILER SIDEWALL DAMAGE RESULTING FROM THE NOSE PLUGS CONTACTING THE SIDEWALLS.
- S. IF POSSIBLE FOR SHIPMENT OF THE DEPICTED CONTAINER, A TRAILER HAVING A MINIMUM INSIDE WIDTH OF 8'-0" SHOULD BE REQUESTED. WHEN A TRAILER IS AT LEAST 8'-0" WIDE, THE CONTAINERS CAN BE SHIPPED IN A CROSSWISE ORIENTATION, AS DELINEATED ON PAGES 6-7 AND 10-11, WHICH WILL AID IN THE HANDLING AND LOADING OF THE CONTAINERS IN THE TRAILER.



* SEE GENERAL NOTE "R" ON PAGE 2

CONTAINER DETAIL

CUBE ----- 45.6 CUBIC FEET (APPROX)
 GROSS WEIGHT ----- 4,337 LBS (APPROX)

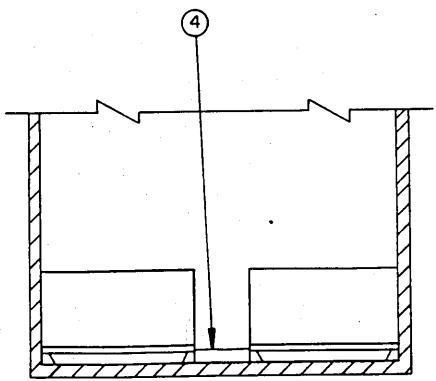


INDICATES NOSE END OF BOMB.

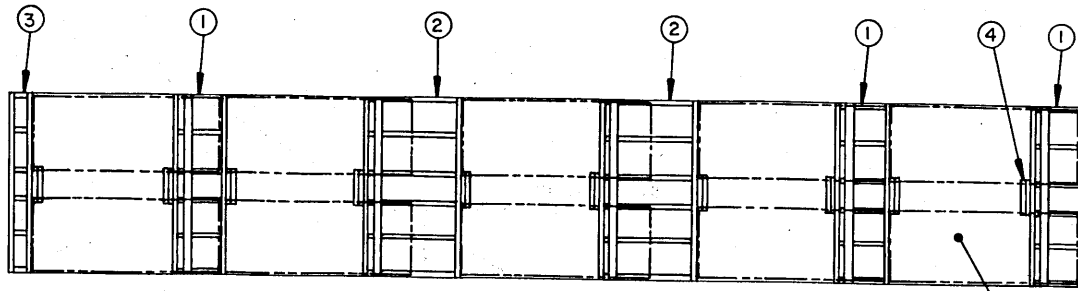
ISOMETRIC VIEW

KEY NUMBERS

- ① SPACER ASSEMBLY (3 RECD.). SEE "SPACER ASSEMBLY A" DETAIL ON PAGE 12. POSITION AS SHOWN ABOVE, AND ON PAGE 5.
- ② SPACER ASSEMBLY (2 RECD.). SEE "SPACER ASSEMBLY B" DETAIL ON PAGE 12. POSITION AS SHOWN ABOVE, AND ON PAGE 5.
- ③ REAR BLOCKING ASSEMBLY (1 RECD.). SEE "SPACER ASSEMBLY C" DETAIL ON PAGE 12. POSITION AS SHOWN ABOVE, AND ON PAGE 5. SEE SPECIAL NOTE 3 ON PAGE 5.
- ④ ANTI-SWAY BRACE, 2" X 4" BY A LENGTH TO SUIT (REF 10") (DOUBLED) (10 RECD.). POSITION OF THE SPACER ASSEMBLIES AS SHOWN IN THE PLAN VIEW ON PAGE 5 AND NAIL THE FIRST PIECE TO THE ASSEMBLY W/3-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE SPECIAL NOTE 4 ON PAGE 5.



SECTION A-A



PLAN VIEW

INDICATES ONE (1) CONTAINER (PALLET UNIT). SEE SPECIAL NOTE 7 BELOW.

SPECIAL NOTES:

1. A 10-UNIT LOAD IS SHOWN IN A 45'-0" LONG BY 7'-6" WIDE (INSIDE DIMENSION) CONVENTIONAL VAN TRAILER. TRAILERS OF OTHER DIMENSIONS MAY BE USED.
2. THE CONTAINER SHOWN IN THE LOAD ON PAGE 4 HAS OVERALL DIMENSIONS OF 7'-11" LONG BY 40" WIDE BY 20-3/4" HIGH AND A WEIGHT OF APPROXIMATELY 4,337 POUNDS.
3. IF THE VOID BETWEEN THE REAR OF THE LOAD AND THE TRAILER DOORS MEASURES 1-1/2" OR LESS, REAR BLOCKING IS NOT REQUIRED. FOR VOIDS UP TO 9", AN APPROPRIATE NUMBER OF 1" X 4" AND/OR 2" X 4" PIECES BY TRAILER WIDTH MINUS 1/2" CAN BE LAID ON EDGE AND NAILED TOGETHER TO FILL THE VOID. NAIL EACH PIECE W/1-APPROPRIATELY SIZED NAIL EVERY 8"; FOR VOIDS GREATER THAN 9" USE "SPACER ASSEMBLY C" AS DETAILED ON PAGE 12. SEE GENERAL NOTE "L" ON PAGE 2.
4. THE ANTI-SWAY BRACES MUST BE CUT LONG ENOUGH TO INSURE A TIGHT FIT BETWEEN LATERALLY ADJACENT CONTAINERS.
5. FOR SHIPMENT OF LESS THAN FULL LOADS, REFER TO THE APPLICABLE GUIDANCE ON PAGES 8 THROUGH 11.
6. TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS MAY BE USED; HOWEVER, SPECIAL REAR BLOCKING MUST BE INSTALLED. SEE THE "PROCEDURES FOR CONVENTIONAL VAN TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS", ON PAGE 15 FOR GUIDANCE. NOTE THAT THE SPECIAL REAR BLOCKING FOR TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS MAY ALSO BE USED IN TRAILERS EQUIPPED WITH HINGED DOORS.
7. THE CONTAINERS IN THE PLAN VIEW ABOVE ARE DEPICTED BY PHANTOM LINES TO SHOW PROPER PLACEMENT OF DUNNAGE ASSEMBLIES.

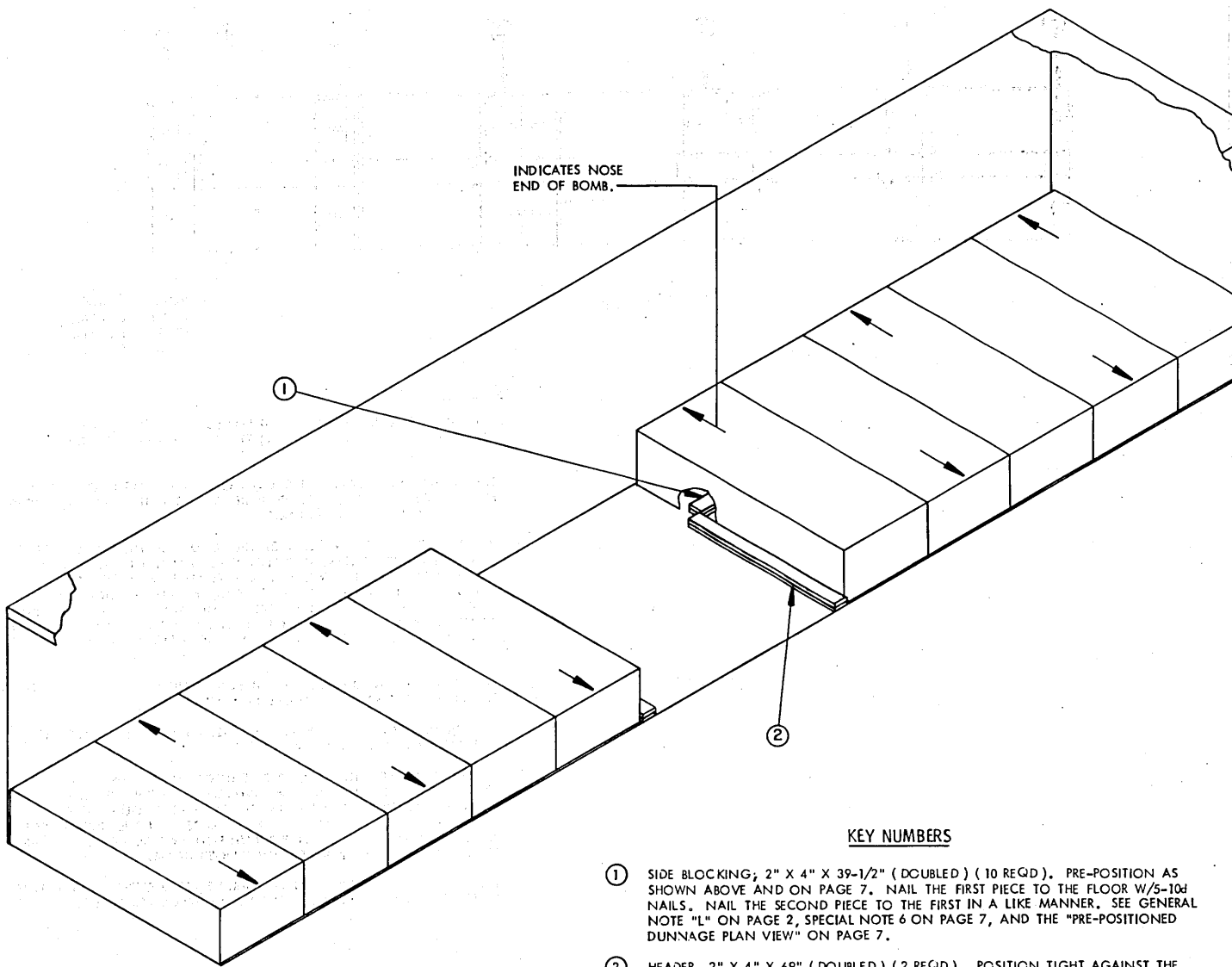
BILL OF MATERIAL

LUMBER	LINEAR FEET	BOARD FEET
2" X 4"	230	153
NAILS	NO. REQ'D	POUNDS
10d (3")	234	3-3/4

LOAD AS SHOWN

ITEM	QUANTITY	WEIGHT (APPROX)
CONTAINER	10	43,370 LBS
DUNNAGE		310 LBS
TOTAL WEIGHT		43,680 LBS

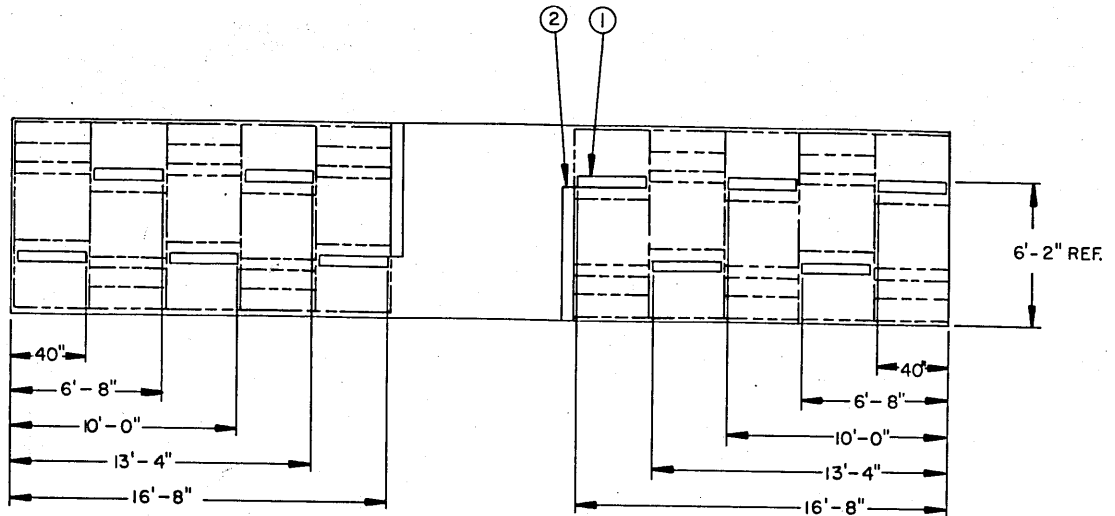
INDICATES NOSE
END OF BOMB.



ISOMETRIC VIEW

KEY NUMBERS

- ① SIDE BLOCKING, 2" X 4" X 39-1/2" (DOUBLED) (10 REQ'D). PRE-POSITION AS SHOWN ABOVE AND ON PAGE 7. NAIL THE FIRST PIECE TO THE FLOOR W/5-10# NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2, SPECIAL NOTE 6 ON PAGE 7, AND THE "PRE-POSITIONED DUNNAGE PLAN VIEW" ON PAGE 7.
- ② HEADER, 2" X 4" X 69" (DOUBLED) (2 REQ'D). POSITION TIGHT AGAINST THE CONTAINER SKIDS AS SHOWN ABOVE AND ON PAGE 7. NAIL THE FIRST PIECE TO THE FLOOR W/11-10# NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2 AND THE "PRE-POSITIONED DUNNAGE PLAN VIEW" ON PAGE 7.



PRE-POSITIONED DUNNAGE PLAN VIEW

SPECIAL NOTES:

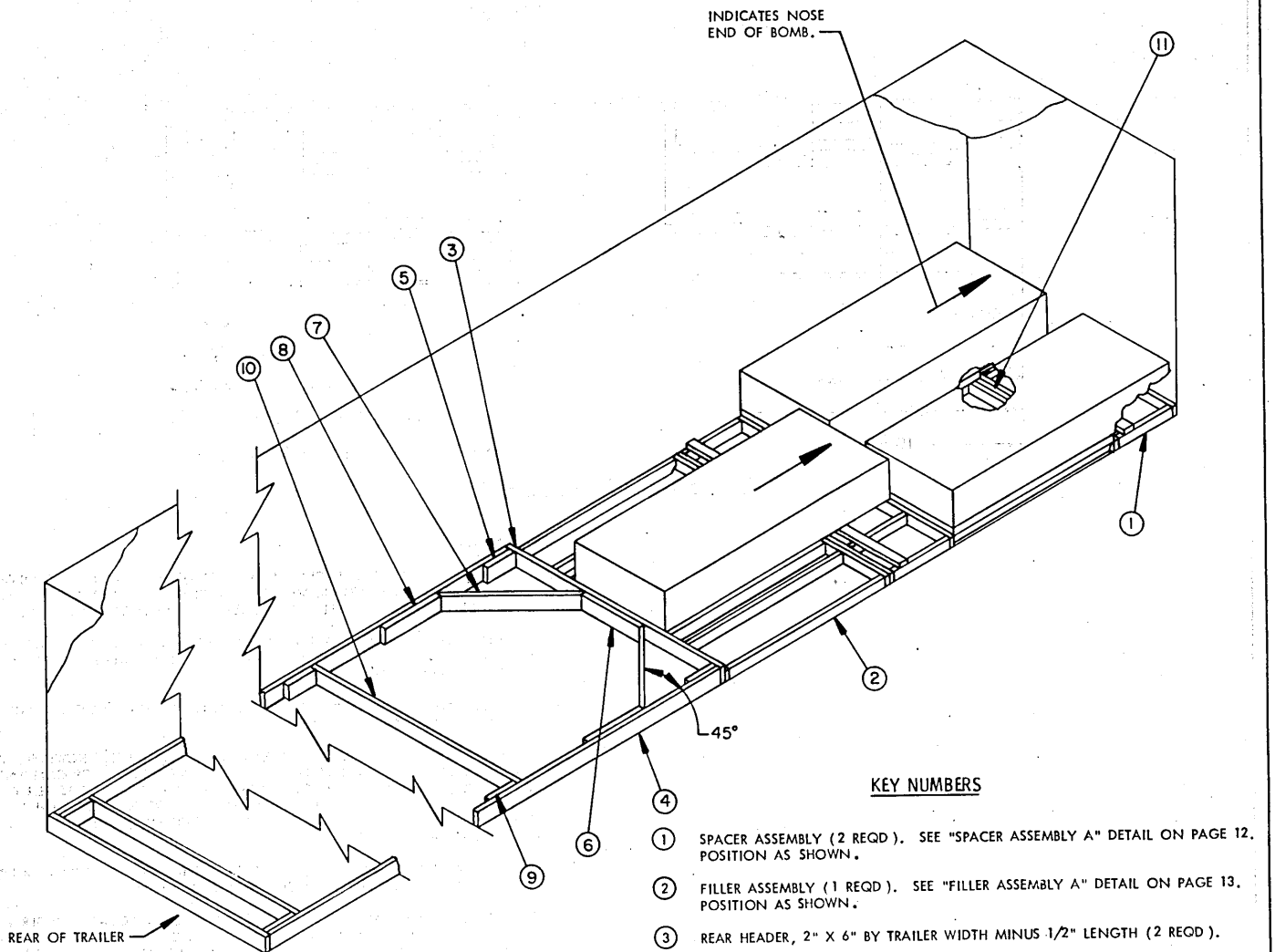
1. A 10-CONTAINER LOAD IS SHOWN IN A 40'-0" LONG BY 8'-0" WIDE (INSIDE DIMENSIONS) CONVENTIONAL VAN TRAILER WITH A NAILABLE FLOOR. TRAILERS 8'-0" WIDE OR WIDER, AS WELL AS SHORTER OR LONGER TRAILERS CAN BE USED.
2. THE CONTAINER SHOWN IN THE LOAD ON PAGE 6 HAS OVERALL DIMENSIONS OF 7'-11" LONG BY 40" WIDE BY 20-3/4" HIGH AND A WEIGHT OF APPROXIMATELY 4,337 POUNDS.
3. FOR SHIPMENT OF LESS THAN FULL LOADS, REFER TO THE APPLICABLE GUIDANCE ON PAGE 9 THROUGH 11.
4. TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS MAY BE USED; HOWEVER, SPECIAL REAR BLOCKING MUST BE INSTALLED. SEE THE "PROCEDURES FOR CONVENTIONAL VAN TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS", ON PAGE 15 FOR GUIDANCE. NOTE THAT THE SPECIAL REAR BLOCKING FOR TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS MAY ALSO BE USED IN TRAILERS EQUIPPED WITH HINGED DOORS. SEE SPECIAL NOTE 7 BELOW.
5. THE CONTAINERS IN THE PLAN VIEW ABOVE ARE DEPICTED BY PHANTOM LINES TO SHOW PROPER PLACEMENT OF FLOOR LINE BLOCKING.
6. THE SIDE BLOCKING, PIECES MARKED ①, SHOULD BE LOCATED UNDER THE NOSE END OF THE BOMBS AND BE POSITIONED TIGHT AGAINST THE CONTAINER SKIDS, WHILE ALLOWING FOR NO MORE THAN ONE HALF INCH (1/2") OF CLEARANCE BETWEEN THE CONTAINER AND THE TRAILER SIDE WALL. DIMENSIONS GIVEN IN THE PLAN VIEW ABOVE ARE FOR REFERENCE ONLY. THE SIDE BLOCKING SHOULD BE SHIFTED AS REQUIRED TO ACHIEVE THE PROPER PLACEMENT OF THE CONTAINERS.
7. THE LOAD MAY BE SHIFTED AS REQUIRED TO COMPLY WITH GENERAL NOTES "E" AND "F" ON PAGE 2. ADDITIONAL HEADERS OR SOLID FILL PIECES ARE REQUIRED IF THE VOID AT THE FRONT OR REAR OF THE TRAILER EXCEEDS 1-1/2". FOR VOIDS BETWEEN 1-1/2" AND 3-1/2" AN APPROPRIATE NUMBER OF 1" X 4" AND/OR 2" X 4" PIECES BY TRAILER WIDTH MINUS 1/2" SHOULD BE LAID ON EDGE AND NAILED TOGETHER TO FILL THE VOID. NAIL EACH PIECE W/1 APPROPRIATELY SIZED NAIL EVERY 8". FOR VOIDS GREATER THAN 3-1/2", HEADER, PIECE MARKED ①, SHOULD BE USED.

BILL OF MATERIAL

LUMBER	LINEAR FEET	BOARD FEET
2" X 4"	89	60
NAILS	NO. REQD	POUNDS
10d (3")	144	2-1/4

LOAD AS SHOWN

ITEM	QUANTITY	WEIGHT (APPROX)
CONTAINER	10	43,370 LBS
DUNNAGE		124 LBS
TOTAL WEIGHT		43,494 LBS



ISOMETRIC VIEW

SPECIAL NOTES:

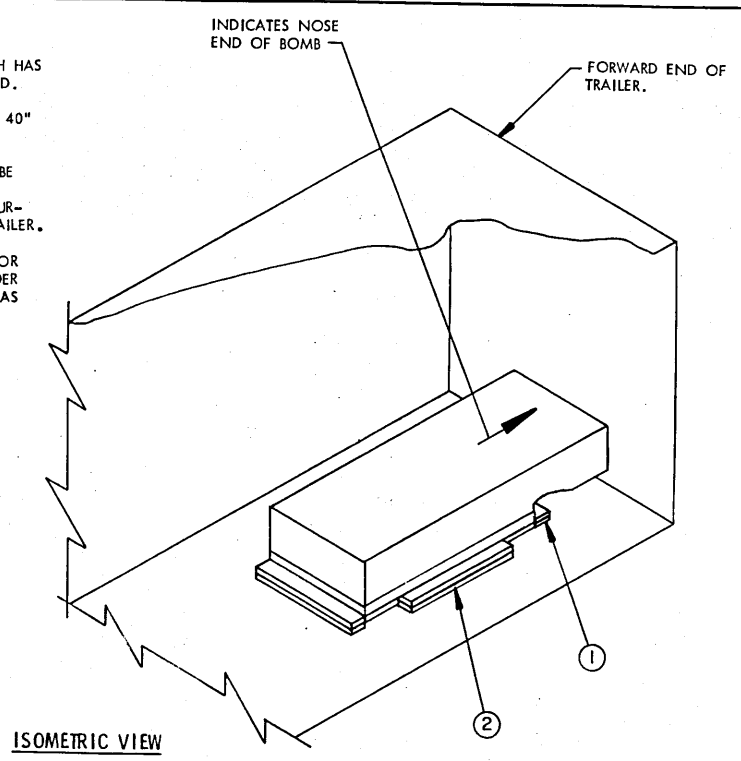
1. A 3-CONTAINER LOAD IS SHOWN IN A 7'-6" WIDE (INSIDE DIMENSION) CONVENTIONAL VAN TRAILER. TRAILERS OF OTHER WIDTHS CAN BE USED.
2. THE CONTAINER SHOWN HAS OVERALL DIMENSIONS OF 7'-11" LONG BY 40" WIDE BY 20-3/4" HIGH AND WEIGHS APPROXIMATELY 4,337 POUNDS.
3. THE TRAILER IS SHOWN HAVING A SQUARE FRONT; HOWEVER, IF THE TRAILER BEING LOADED HAS ROUNDED CORNERS, A FORWARD BLOCKING ASSEMBLY, AS DETAILED ON PAGE 14 MUST BE USED.
4. THE "K-BRACE BLOCKING", SHOWN AS PIECES MARKED ③ THRU ⑩ IS ADEQUATE FOR RETAINING A MAXIMUM LTL LOAD OF 20,000 POUNDS.
5. FOR TRAILERS EQUIPPED WITH ROLL-UP TYPE DOORS THE PROCEDURES DEPICTED ABOVE CANNOT BE USED, REFERENCE PIECES MARKED ③ THRU ⑩. SEE NAILED DOWN HEADER METHOD ON PAGE 15.
6. THE ANTI-SWAY BRACE, PIECE MARKED ⑪ MUST BE CUT LONG ENOUGH TO INSURE A TIGHT FIT BETWEEN LATERALLY ADJACENT CONTAINERS.
7. PROCEDURES DEPICTED CAN BE USED WITH TRAILERS HAVING ALL METAL (NON-NAILABLE) FLOORS.

KEY NUMBERS

- ① SPACER ASSEMBLY (2 REQD). SEE "SPACER ASSEMBLY A" DETAIL ON PAGE 12. POSITION AS SHOWN.
- ② FILLER ASSEMBLY (1 REQD). SEE "FILLER ASSEMBLY A" DETAIL ON PAGE 13. POSITION AS SHOWN.
- ③ REAR HEADER, 2" X 6" BY TRAILER WIDTH MINUS 1/2" LENGTH (2 REQD).
- ④ SIDE STRUT, 2" X 6" BY CUT TO FIT BETWEEN HEADERS, PIECE MARKED ③ (2 REQD).
- ⑤ POCKET CLEAT, 2" X 6" X 12" (2 REQD). NAIL TO THE SIDE STRUT, PIECE MARKED ④, W/3-10d NAILS. TOENAIL TO THE ADJACENT HEADER, PIECE MARKED ③, W/3-12d NAILS.
- ⑥ CENTER CLEAT, 2" X 6" X 24" (1 REQD). NAIL TO THE HEADER, PIECE MARKED ③, W/6-10d NAILS.
- ⑦ DIAGONAL BRACE, 2" X 6" BY CUT TO FIT (2 REQD). DOUBLE BEVEL EACH END WITH 45° CUTS. INSTALL AT A 45° ANGLE AS SHOWN AND TOENAIL TO THE ADJACENT HEADER AND SIDE STRUT, PIECES MARKED ③ AND ④, W/2-16d NAILS AT EACH END.
- ⑧ SIDE CLEAT, 2" X 6" X 24" (2 REQD). NAIL TO THE SIDE STRUT, PIECE MARKED ④, W/8-10d NAILS.
- ⑨ STRUT BRACE RETAINING CLEAT, 2" X 4" X 12" (AS REQD). NAIL TO A SIDE STRUT, PIECE MARKED ④, W/3-10d NAILS.
- ⑩ STRUT BRACE, 2" X 4" BY TRAILER WIDTH MINUS 3" IN LENGTH (MINIMUM OF ONE REQUIRED). INSTALL ONE (1) NEAR THE REAR OF THE TRAILER AS SHOWN. ONE (1) ADDITIONAL PIECE REQUIRED FOR EVERY 7'-0" OF STRUT LENGTH. NAIL TO THE STRUT BRACE RETAINING CLEATS, PIECES MARKED ⑨, W/2-12d NAILS AT EACH END.
- ⑪ ANTI-SWAY BRACE, 2" X 4" BY A LENGTH TO SUIT (REF: 14") (DOUBLED) (2 REQD). NAIL THE FIRST PIECE TO PIECE MARKED ① W/3-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE SPECIAL NOTE 6 AT LEFT.

SPECIAL NOTES:

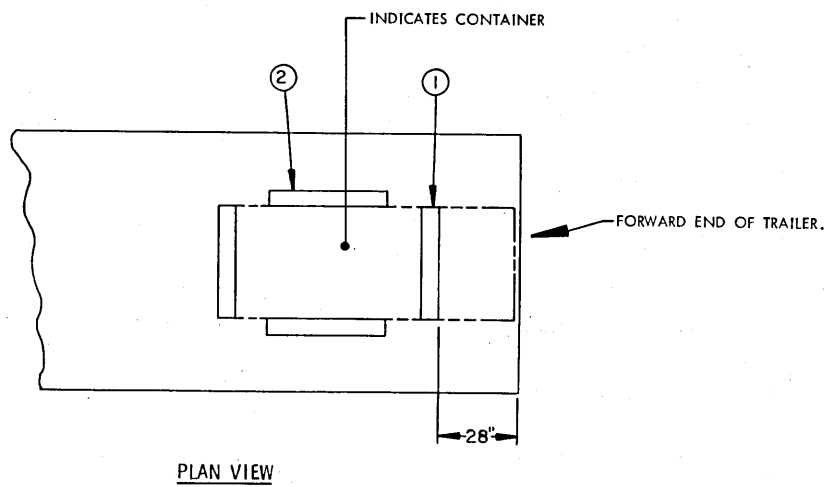
1. A 7'-6" WIDE (INSIDE DIMENSION) CONVENTIONAL VAN TRAILER WHICH HAS A NAILABLE FLOOR IS SHOWN. TRAILERS OF OTHER WIDTHS CAN BE USED.
2. THE CONTAINER SHOWN HAS OVERALL DIMENSIONS OF 7'-11" LONG BY 40" WIDE BY 20-3/4" HIGH AND WEIGHS APPROXIMATELY 4,337 POUNDS.
3. THE CONTAINER, AS SHOWN IN THE "ISOMETRIC VIEW" AT RIGHT, MAY BE POSITIONED ANYWHERE LONGITUDINALLY WITHIN THE TRAILER PROVIDED ADEQUATE NAILING SURFACES ARE AVAILABLE. FOR TRAILER STABILITY PURPOSES, THE CONTAINER SHOULD BE CENTERED LATERALLY WITHIN THE TRAILER.
4. **NOTE:** ONLY THE FORWARD HEADER WILL REQUIRE PRE-POSITIONING PRIOR TO LOADING THE CONTAINER INTO THE TRAILER. THE REMAINING HEADER AND SIDE BLOCKING PIECES MAY BE INSTALLED AFTER THE CONTAINER HAS BEEN LOADED INTO THE TRAILER.



ISOMETRIC VIEW

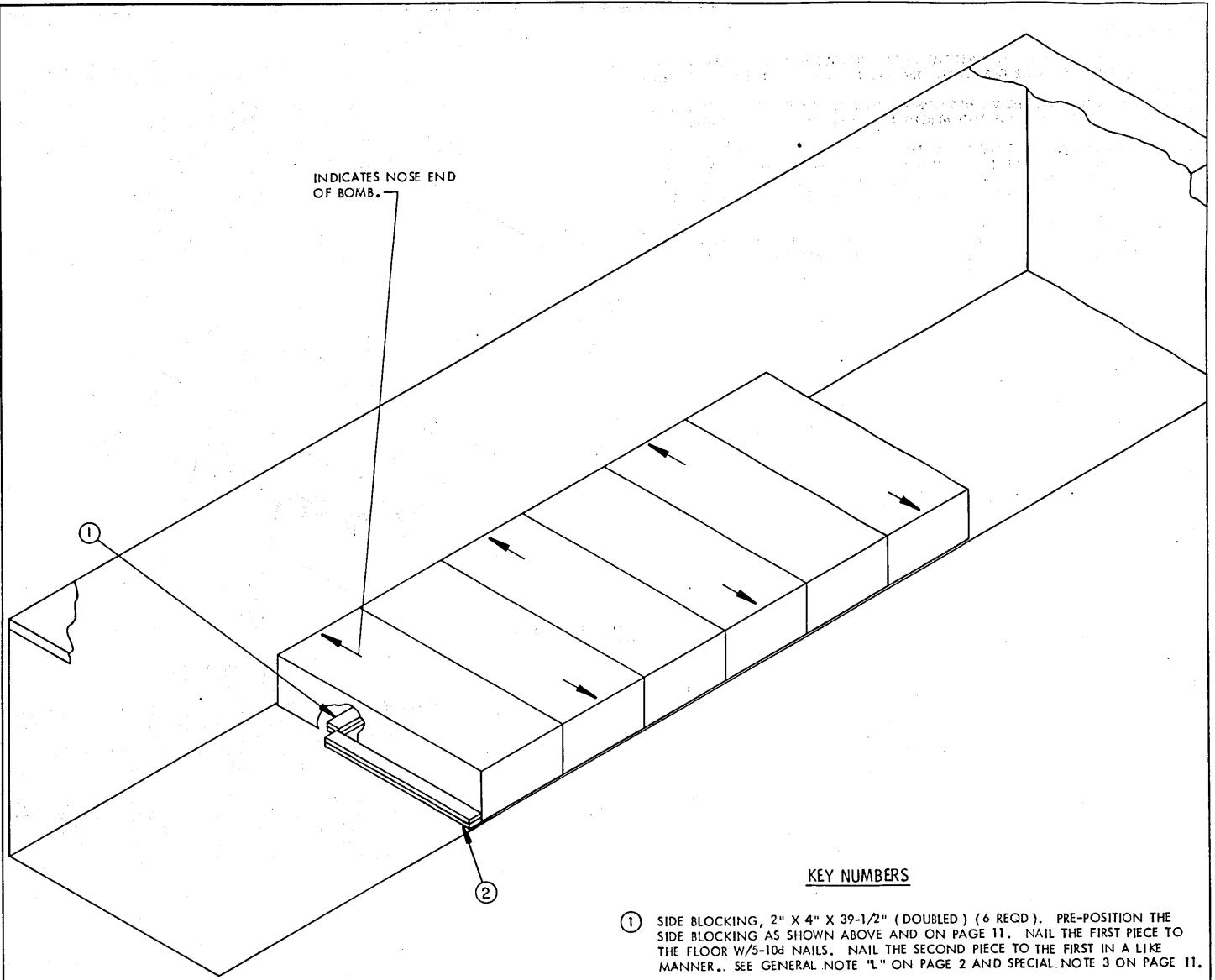
KEY NUMBERS

- ① HEADER, 2" X 4" X 40" (DOUBLED) (2 REQD). POSITION TIGHT AGAINST THE CONTAINER UNIT AS SHOWN ABOVE AND IN THE PLAN VIEW BELOW. NAIL THE FIRST PIECE TO THE FLOOR W/5-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2.
- ② SIDE BLOCKING, 2" X 4" X 40" (DOUBLED) (2 REQD). POSITION TIGHT AGAINST THE CONTAINER SKIDS AS SHOWN ABOVE AND IN THE PLAN VIEW BELOW. NAIL THE FIRST PIECE TO THE FLOOR W/5-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2.



PLAN VIEW

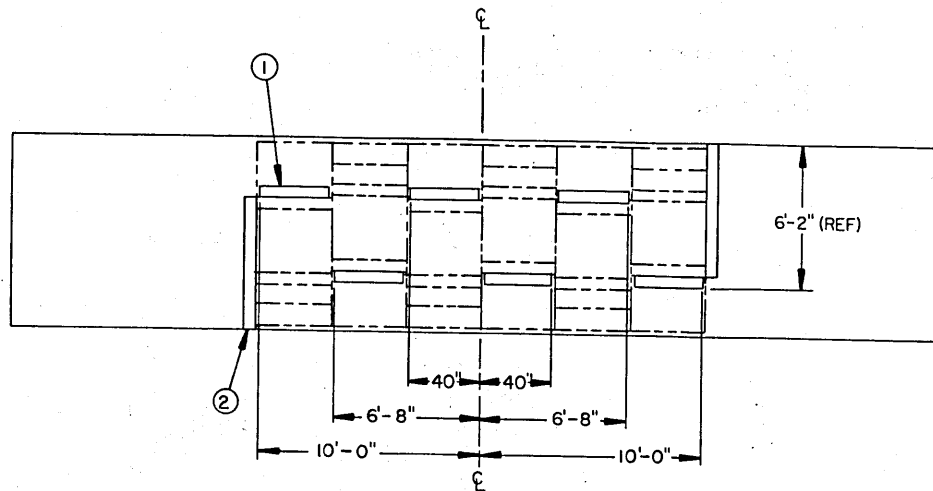
TYPICAL LTL (1-UNIT LOAD) LENGTHWISE IN A CONVENTIONAL VAN TRAILER



ISOMETRIC VIEW

KEY NUMBERS

- ① SIDE BLOCKING, 2" X 4" X 39-1/2" (DOUBLED) (6 REQD). PRE-POSITION THE SIDE BLOCKING AS SHOWN ABOVE AND ON PAGE 11. NAIL THE FIRST PIECE TO THE FLOOR W/5-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2 AND SPECIAL NOTE 3 ON PAGE 11.
- ② HEADER, 2" X 4" X 69" (DOUBLED) (2 REQD). POSITION THE HEADER TIGHT AGAINST THE CONTAINER SKIDS AS SHOWN ABOVE AND ON PAGE 11. NAIL THE FIRST PIECE TO THE FLOOR W/11-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2.

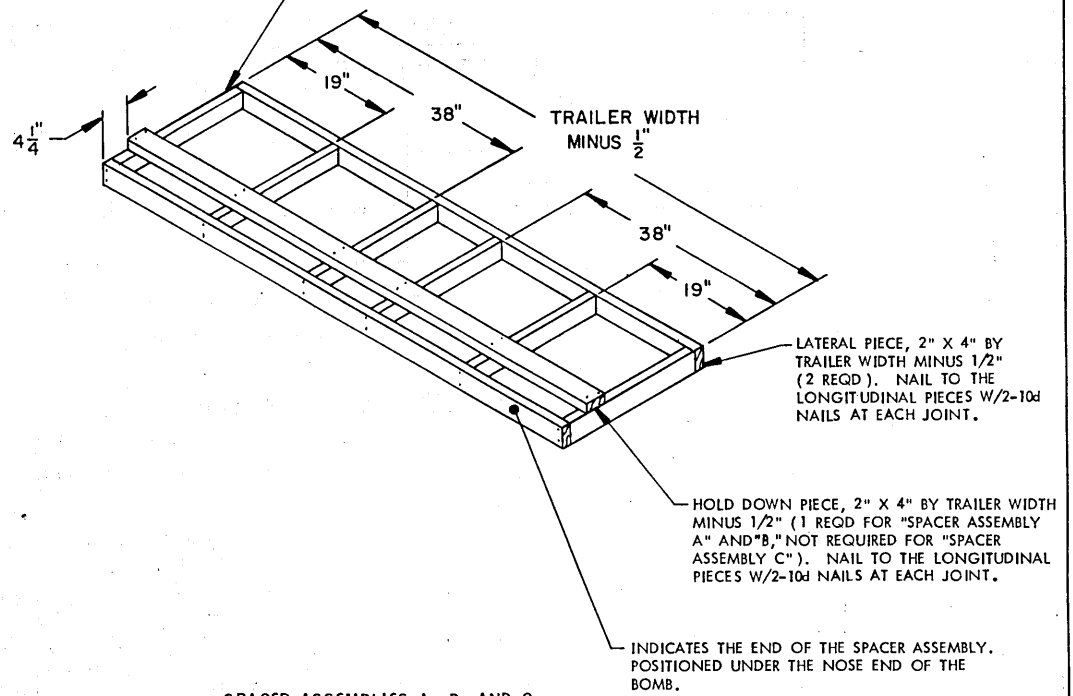


PRE-POSITIONED DUNNAGE PLAN VIEW

SPECIAL NOTES:

1. A 6-CONTAINER LOAD IS SHOWN IN AN 8'-0" WIDE (INSIDE DIMENSION) CONVENTIONAL VAN TRAILER. TRAILERS OF OTHER WIDER WIDTHS, AS WELL AS LONGER OR SHORTER TRAILERS CAN BE USED.
2. THE CONTAINER SHOWN HAS OVERALL DIMENSIONS OF 7'-11" LONG BY 40" WIDE, BY 20-3/4" HIGH AND WEIGHS APPROXIMATELY 4,337 POUNDS.
3. THE SIDE BLOCKING, PIECES MARKED ①, SHOULD BE LOCATED UNDER THE NOSE END OF THE BOMBS AND POSITIONED TIGHT AGAINST THE CONTAINER SKIDS WHILE ALLOWING FOR NO MORE THAN ONE HALF INCH (1/2") OF CLEARANCE BETWEEN THE CONTAINER AND THE TRAILER SIDE WALL. DIMENSIONS GIVEN IN THE PLAN VIEW ABOVE ARE FOR REFERENCE ONLY. THE SIDE BLOCKING SHOULD BE SHIFTED AS REQUIRED TO ACHIEVE THE PROPER PLACEMENT OF CONTAINERS.

LONGITUDINAL PIECE, 2" X 4" BY 24" FOR "SPACER ASSEMBLY A", 46" FOR "SPACER ASSEMBLY B" AND BY A LENGTH TO SUIT FOR "SPACER ASSEMBLY C" (6 REQD). SEE NOTE BELOW.

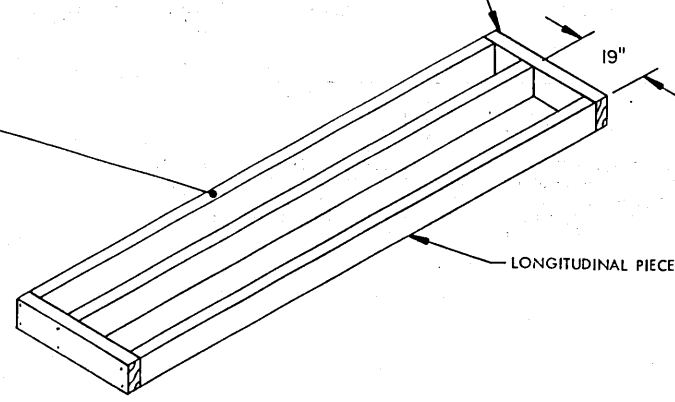


SPACER ASSEMBLIES A, B, AND C

NOTE: IF THE BOMBS ARE BEING SHIPPED WITH THE CONICAL NOSE PLUGS INSTALLED, INCREASE THE LENGTH OF "SPACER ASSEMBLY A" BY 4" AND DECREASE THE LENGTH OF "SPACER ASSEMBLY B" BY 6".

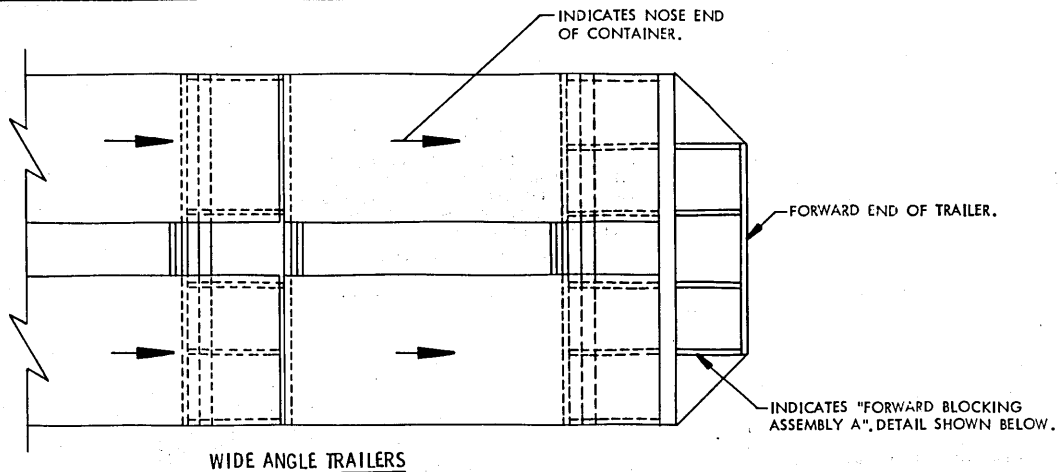
LATERAL PIECE, 2" X 4" BY A LENGTH TO
FILL THE VOID BETWEEN THE CONTAINER
SKIDS AND THE VAN SIDE WALL (REF: 25-3/4")
(2 REQD). NAIL TO THE LONGITUDINAL
PIECES W/2-10d NAILS AT EACH JOINT.

CONTAINER SIDE OF ASSEMBLY.



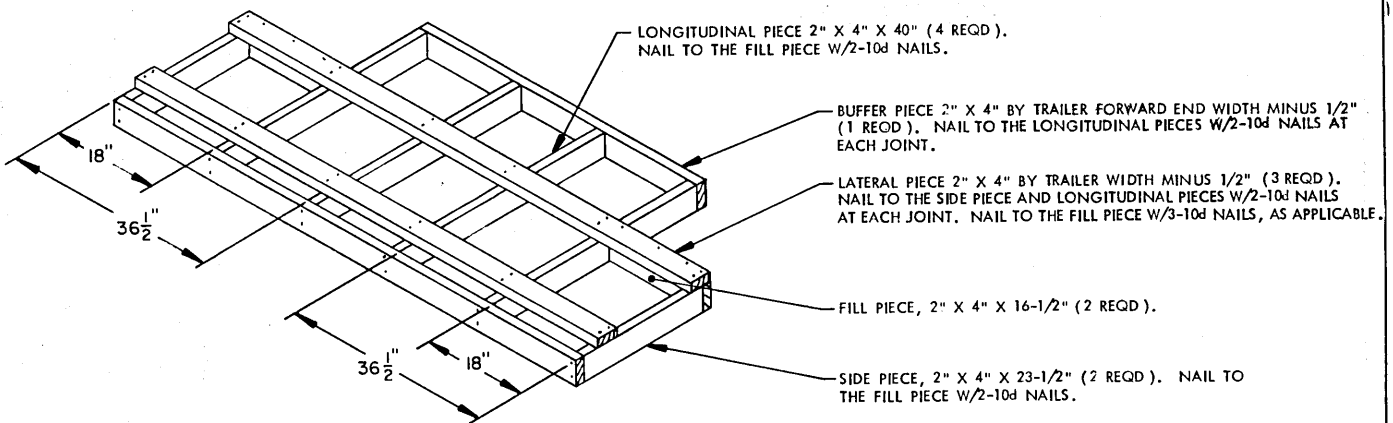
LONGITUDINAL PIECE, 2" X 4" X 70" (3 REQD).

FILLER ASSEMBLY A

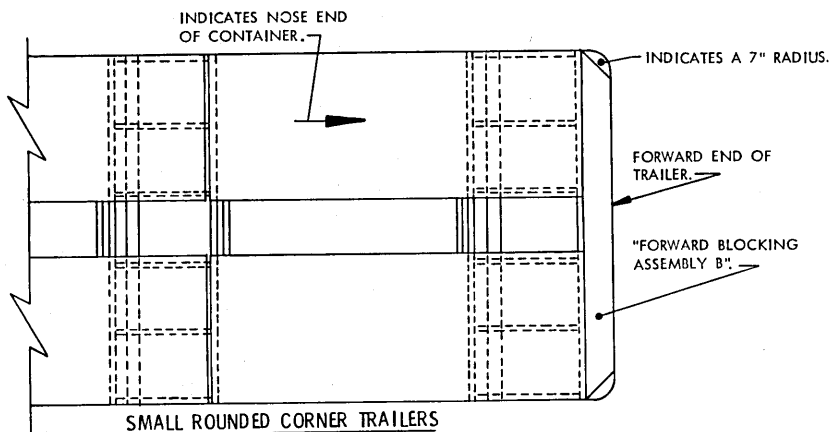


WIDE ANGLE TRAILERS

THIS PROCEDURE IS APPLICABLE TO THE LOADING OF A CONVENTIONAL VAN TRAILER HAVING LARGE ANGLED FRONT CORNERS (REF: 18"). THE PROCEDURES MAY ALSO BE USED IN TRAILERS HAVING LARGE ROUNDED FRONT CORNERS, OR ANGLED CORNERS OF ANOTHER SIZE. SEE "FORWARD BLOCKING ASSEMBLY A" DETAIL BELOW.

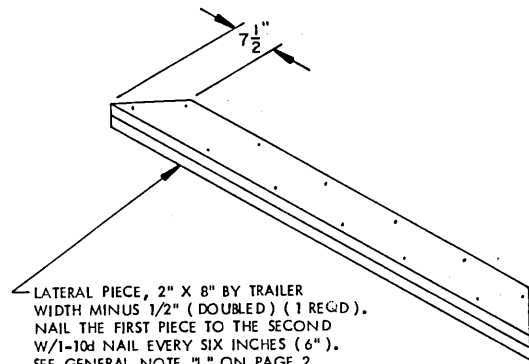


FORWARD BLOCKING ASSEMBLY



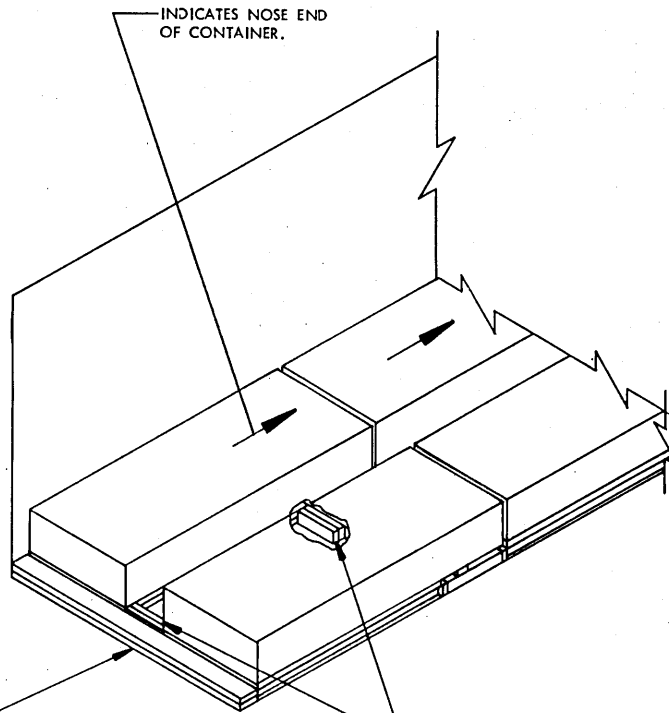
SMALL ROUNDED CORNER TRAILERS

THIS PROCEDURE IS APPLICABLE TO THE LOADING OF A CONVENTIONAL VAN TRAILER HAVING SMALL ROUNDED FRONT CORNERS. SEE "FORWARD BLOCKING ASSEMBLY B" DETAIL AT RIGHT. NOTE: FOR TRAILERS WITH FIVE INCH (5") RADIUS OR LESS USE 2" X 6" MATERIAL, FOR RADIUS BETWEEN 5" AND 7" USE 2" X 8" MATERIAL, FOR RADIUS BETWEEN 7" AND 9" USE 2" X 10" MATERIAL, AND FOR RADIUS BETWEEN 9" AND 11", USE 2" X 12" MATERIAL. SEE GENERAL NOTE "R" ON PAGE 2.



FORWARD BLOCKING ASSEMBLY B

DETAILS



HEADER, 2" X 4" BY TRAILER WIDTH MINUS 1/2" IN LENGTH (DOUBLED) (1 REQD). POSITION TIGHT AGAINST THE CONTAINERS. NAIL THE FIRST PIECE TO THE TRAILER FLOOR W/22-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2.

ISOMETRIC VIEW

ANTI SWAY BRACE 2" X 4" BY THE VOID BETWEEN LATERALLY ADJACENT CONTAINER UNITS MINUS 1/2" (DOUBLED). NAIL THE FIRST PIECE TO THE HEADER OR SPACER ASSEMBLY, AS APPLICABLE, W/3-10d NAILS. NAIL THE SECOND PIECE TO THE FIRST IN A LIKE MANNER. SEE GENERAL NOTE "L" ON PAGE 2.

SPECIAL NOTES:

1. THE NAILED HEADER METHOD OF REAR BLOCKING DEPICTED ABOVE CAN ONLY BE USED IN TRAILERS HAVING A NAILABLE FLOOR AREA BETWEEN THE LADING AND THE METAL THRESHOLD, OR A THRESHOLD PLACE IF THE TRAILER IS SO EQUIPPED, OF AT LEAST FOURTEEN INCHES (14").
2. THE NAILED HEADER METHOD OF REAR BLOCKING IS ADEQUATE FOR THE RETENTION OF THE MAXIMUM WEIGHT LOAD.
3. THE NAILED HEADER METHOD, ALTHOUGH DESIGNED ESPECIALLY FOR TRAILERS HAVING ROLL UP TYPE DOORS, MAY ALSO BE USED IN TRAILERS EQUIPPED WITH HINGED DOORS.



Faint, illegible text or notes located to the right of the main diagram.

Large block of faint, illegible text or notes located below the main diagram.