

APPROVED BY  
 BUREAU OF EXPLOSIVES  
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 DATE 7/12/68

# LOADING AND BRACING (TL & LTL) IN CLOSED OR OPEN TOP VAN TRAILERS • OF CBU ITEMS PACKED IN THE CNU - IO4/E CONTAINER

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● THIS DOCUMENT INCLUDES PROCEDURES FOR CONVENTIONAL TYPE TRAILERS AND FOR TRAILERS EQUIPPED WITH MECHANICAL BRACING DEVICES AS APPROVED BY THE BUREAU OF EXPLOSIVES, ASSOCIATION OF AMERICAN RAILROADS.

**CAUTION:** PROCEDURES SHOWN HEREIN FOR MECHANICAL BRACING DEVICE EQUIPPED TRAILERS ARE ONLY APPLICABLE FOR HIGHWAY MOVEMENT; NOT FOR TOFC OR COFC MOVEMENTS.

DO NOT SCALE

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## GENERAL NOTES

(GENERAL NOTES CONTINUED)

- A. THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AMCR 740-13, AND AUGMENTS TM 743-220-1 (CHAPTER 5).
- B. THE OUTLOADING PROCEDURES SPECIFIED HEREIN ARE APPLICABLE TO CBU ITEMS WHEN PACKAGED IN THE CNU-104/E CONTAINER. SUBSEQUENT REFERENCE TO CONTAINER HEREIN MEANS THE CNU-104/E CONTAINER WITH CONTENTS. CAUTION: CONTAINERS MUST NOT BE STACKED MORE THAN THREE (3) CONTAINERS HIGH. STACKS OF MORE THAN ONE CONTAINER HIGH MUST BE SKIDDED AND UNITIZED AS SHOWN ON PAGE 3 TO FACILITATE LOADING OF TRAILER.
- C. FOR DETAILS OF THE CONTAINER SEE DRAWING NO. 67E10255.  
CONTAINER DIMENSIONS ----- 154-1/2" LONG X 24-1/4" WIDE X 30-1/2" HIGH.  
GROSS WEIGHT (MAX) ----- 1,155 POUNDS (APPROX).  
TARE WEIGHT ----- 185 POUNDS (APPROX).
- D. THE 27-UNIT LOAD SHOWN ON PAGES 4 AND 5 IS BASED ON A 40'-0" LONG BY 7'-8" WIDE (INSIDE DIMENSION) HIGH VOLUME CLOSED OR OPEN TOP VAN TRAILER WHICH HAS A MINIMUM DOOR OPENING HEIGHT OF 93", AND HAS A WOOD, OR A WOOD AND METAL, OR A METAL FLOOR. THE DELINEATED OUTLOADING PROCEDURES ARE ALSO APPLICABLE TO TRAILERS OF ANY OTHER WIDTH BY ADJUSTING THE THICKNESS OF THE "SIDE FILL BLOCKING" AS DETAILED ON PAGE 8.
- E. IF A TRAILER IS BEING USED TO SHIP A 3-LAYER LOAD AND IT CONTAINS A MECHANICAL LOAD-BLOCKING SYSTEM WHICH CONFORMS TO SPECIFICATIONS SET FORTH WITHIN THE BUREAU OF EXPLOSIVES PAMPHLET 6C AND THE APPENDICES THERETO, THE MECHANICAL SYSTEM MAY BE USED IN ACCORDANCE WITH PROCEDURES DELINEATED ON PAGE 5 OF THIS DOCUMENT, IN LIEU OF REAR-OF-LOAD BLOCKING SPECIFIED FOR THE LOADS DEPICTED HEREIN.
- F. VOIDS LENGTHWISE BETWEEN LOADS MUST BE MINIMUM. FOR CONVENTIONAL TRAILERS, REAR BLOCKING MUST CONTACT REAR DOORS OF THE TRAILER WHEN THEY ARE CLOSED. FOR TRAILERS EQUIPPED WITH MECHANICAL BRACING DEVICES, THE CROSS MEMBERS MUST BE PLACED AGAINST THE LADING AS TIGHTLY AS THE WALL MEMBER LOCKING HOLE SPACING PERMITS. CROSS MEMBERS WILL BE INSTALLED WITH EACH END ATTACHED AS NEARLY AS POSSIBLE IN "MATED" POSITIONS, AT EQUAL HEIGHTS AND AT EQUAL DISTANCES FROM END OF TRAILER.
- G. MECHANICAL CROSS MEMBERS IN EMPTY TRAILERS AND THOSE UNUSED IN LOADED TRAILERS MUST BE "SECURED" FOR SHIPMENT. COMPONENTS ASSIGNED TO EACH TRAILER MUST REMAIN THERewith EVEN THOUGH UNUSED DURING SOME SHIPMENTS.
- H. THE GROSS WEIGHT AND AXLE DISTRIBUTION OF WEIGHT FOR A LOAD WILL BE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THE APPLICABLE LOADING REQUIREMENTS, AND THE SHIPPER WILL LOAD ACCORDINGLY.
- J. THE NUMBER OF LADING UNITS MAY BE ADJUSTED TO FIT THE SIZE OF THE TRAILER TO BE LOADED OR THE QUANTITY TO BE SHIPPED AND COMBINATIONS OF THE OUTLOADING PROCEDURES SPECIFIED HEREIN MAY BE USED. HOWEVER, THE APPROVED METHODS SHOWN HEREIN MUST BE FOLLOWED AS CLOSELY AS POSSIBLE FOR BLOCKING, BRACING, AND STAYING OF THE DESIGNATED ITEMS.
- K. THE OUTLOADING PROCEDURES SPECIFIED HEREIN CAN ALSO BE UTILIZED FOR THE SHIPMENT OF THE DEPICTED CONTAINERS WHEN THEY ARE LOADED WITH EMPTY DISPENSERS OR LOADED WITH AN ITEM WHICH IS IDENTIFIED DIFFERENTLY BY NOMENCLATURE THAN THE ITEMS DESIGNATED WITHIN THE DRAWING TITLE.
- L. OTHER TYPES OF LADING ITEMS MAY BE LOADED INTO TRAILERS WHICH ARE PARTIALLY LOADED WITH ITEMS PACKED IN THE CNU-104/E CONTAINERS, PROVIDING THE TOTAL LOAD IS COMPATIBLE, EXISTING DIRECTIVES ARE NOT VIOLATED, AND THE OTHER LADING ITEMS ARE BLOCKED AND BRACED TO EQUAL THE BLOCKING AND BRACING CRITERIA SPECIFIED HEREIN.
- M. ONE AND ONE-QUARTER INCH (1-1/4") STEEL STRAPPING WILL BE USED TO UNITIZE STACKED CONTAINERS AS SHOWN ON PAGE 3, AND AS SPECIFIED BY THE OUTLOADING METHODS ON PAGES 4, 5, AND 6. IF THE CAPACITY OF THE MATERIALS HANDLING EQUIPMENT USED TO LOAD THE CONTAINERS ABOARD A TRAILER PERMITS, IT IS RECOMMENDED THAT THE CONTAINERS BE UNITIZED PRIOR TO PLACEMENT WITHIN THE TRAILER. NOTICE: IN SOME INSTANCES CONTAINERS WILL ALREADY BE UNITIZED WHEN OFFERED FOR LOADING. THESE UNITIZED STACKS SHOULD BE INSPECTED, AND AS REQUIRED, LOOSE UNITIZING STEEL STRAPPING MUST BE REPLACED.
- N. WHEN ANY STRAP IS SEALED AT AN END-OVER-END LAP JOINT, A MINIMUM OF TWO (2) SEALS, BUTTED TOGETHER, WITH TWO (2) PAIRS OF CRIMPS PER SEAL MUST BE USED. CAUTION: EXERCISE CARE DURING STRAP TENSIONING TO PREVENT DAMAGE TO CONTAINERS.
- O. DUNNAGE LUMBER SPECIFIED THROUGHOUT THIS PROCEDURAL DRAWING IS OF NOMINAL SIZE. FOR EXAMPLE, 1" X 4" MATERIAL IS ACTUALLY 3/4" THICK BY 3-5/8" WIDE AND 2" X 6" MATERIAL IS ACTUALLY 1-5/8" THICK BY 5-5/8" WIDE.
- P. NOTICE: A STAGGERED NAILING PATTERN WILL BE USED WHEREVER POSSIBLE WHEN NAILS ARE DRIVEN INTO JOINTS OF DUNNAGE ASSEMBLIES. ALSO, A STAGGERED NAILING PATTERN WILL BE USED WHEN DUNNAGE IS NAILED TO THE FLOOR OF THE TRANSPORTING VEHICLE, OR WHEN LAMINATING DUNNAGE. ADDITIONALLY, THE NAILING PATTERN FOR AN UPPER PIECE OF LAMINATED DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN THROUGH ONTO OR RIGHT BESIDE A NAIL IN A LOWER PIECE.
- Q. PORTIONS OF THE TRAILER BODIES DEPICTED WITHIN THIS PROCEDURAL DRAWING, SUCH AS ONE OF THE SIDE WALLS, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.
- R. FOR ADDITIONAL GUIDANCE, ATTENTION IS DIRECTED TO THE "UNITIZATION AND HANDLING PROCEDURES" ON PAGE 3 AND TO THE "SPECIAL NOTES" SECTIONS WHICH ARE IMMEDIATELY ADJACENT TO DEPICTED OUTLOADING METHODS.

(GENERAL NOTES CONTINUED AT RIGHT ABOVE)

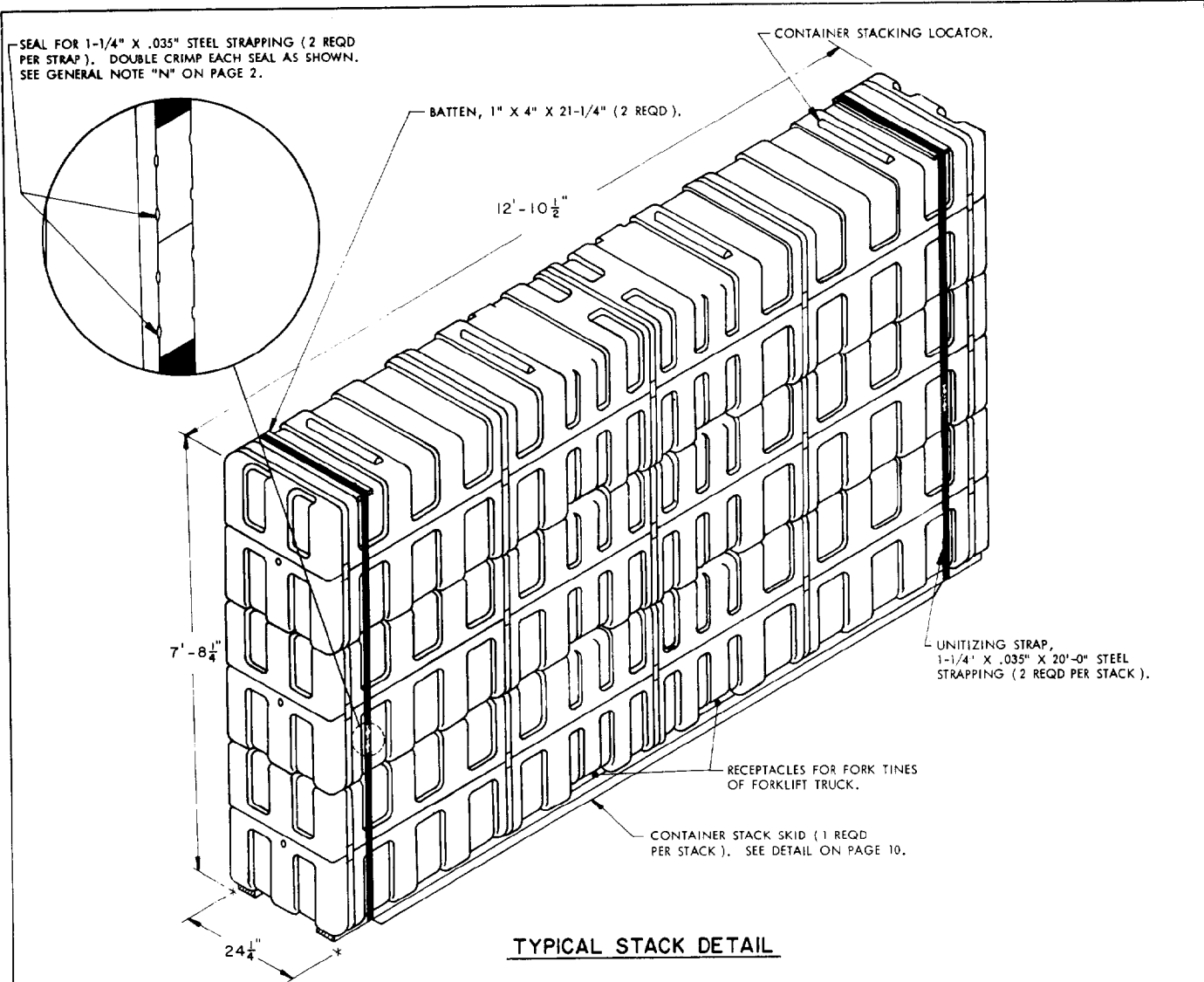
## MATERIAL SPECIFICATIONS

LUMBER ----- : SEE TM 743-200-1, DUNNAGE LUMBER; FED SPEC MM-L-751.

NAILS ----- : COMMON, CEMENT COATED OR CHEMICALLY ETCHED,  
REF: FED SPEC FF-N-105,  
ALT: ANNULAR-RING TYPE NAIL OF SAME SIZE.

STRAPPING, STEEL: TYPE I OR IV, CLASS A OR B, FED SPEC QQ-5-781.

STRAP SEAL; STRAP  
STAPLE ----- : COMMERCIAL GRADE.



**TYPICAL STACK DETAIL**

**UNITIZING AND HANDLING PROCEDURAL GUIDANCE**

1. STACKING CONTAINERS FOR UNITIZING.
  - A. THE BOTTOM CONTAINER WILL BE ALIGNED WITH AND POSITIONED ON A "CONTAINER STACK SKID".
  - B. AN UPPER CONTAINER WILL BE FULLY SEATED ON THE CONTAINER STACKING LOCATORS OF THE NEXT LOWER CONTAINER AND AS NEARLY AS POSSIBLE IN VERTICAL ALIGNMENT.
2. INSTALLATION OF 1-1/4" X .035" UNITIZING STEEL STRAPPING. SEE GENERAL NOTES "M" AND "N" ON PAGE 2.
  - A. THE UNITIZING STRAP SHOULD BE POSITIONED AROUND THE CONTAINERS AND SKID AS SHOWN, AND PLACED SO THAT THE STRAPPING LAYS FLAT AND STRAIGHT WITH THE BODY SURFACES OF THE CONTAINERS; I. E., VERTICAL ALONG SIDES AND STRAIGHT ACROSS TOP AND BOTTOM OF THE STACK.
  - B. PLACE THE BATTEN (1" X 4" X 21-1/4") UNDER THE STRAPPING ACROSS THE TOP OF THE STACK TO PREVENT STRAPPING FROM DAMAGING THE TOP CONTAINER.
  - C. STRAPPING WILL BE FIRMLY TENSIONED, AND EACH END-OVER-END LAP JOINT WILL BE SEALED WITH TWO DOUBLE CRIMPED STRAP SEALS AS SHOWN. THE LAP JOINT WILL BE MADE ALONG THE SIDE OF THE STACK SO THAT THE SEALS WILL NOT BE IN CONTACT WITH THE CONTAINERS. DURING STRAP TENSIONING, CARE SHOULD BE EXERCISED TO INSURE THAT THE CONTAINERS ARE NOT DAMAGED. EXCESS STRAPPING (STRAP ENDS) SHOULD BE CUT OFF OR BROKEN OFF NEAR THE JOINT SEALS.
3. CONTAINER OR CONTAINER STACK HANDLING.
 

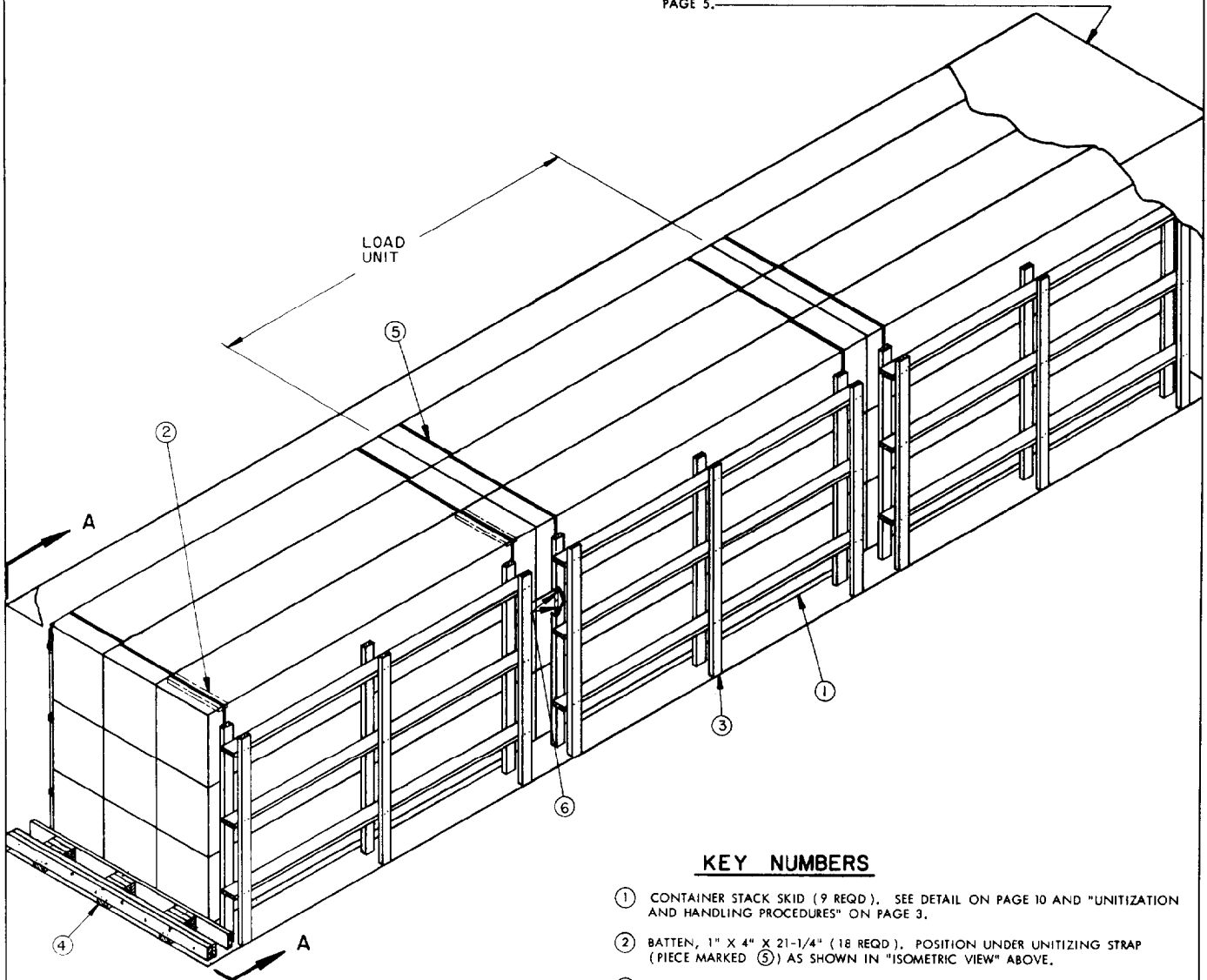
**CAUTION:** EXTREME CARE MUST BE EXERCISED DURING OUTLOADING OPERATIONS TO PREVENT DAMAGE TO THE EXPOSED PLASTIC AREAS OF THE CONTAINERS.

(UNITIZING AND HANDLING PROCEDURAL GUIDANCE CONTINUED)

- NOTES:** (1) APPROVED MATERIAL HANDLING EQUIPMENT (FORKLIFT TRUCKS, CRANES, HAND TRUCKS, DOLLIES, ROLLER ASSEMBLIES, SLINGS, SPREADER BARS, ETC.) IS SPECIFIED ELSEWHERE, OTHER THAN HEREIN.
- (2) PRECAUTIONARY HANDLING TECHNIQUES NORMALLY EMPLOYED OR AS SPECIFIED FOR THE TYPE OF COMMODITY INVOLVED WILL BE OBSERVED.
- A. ONLY APPROVED AND APPROPRIATELY SIZED MATERIALS HANDLING EQUIPMENT WILL BE USED FOR HANDLING THE DEPICTED CONTAINERS.
  - B. IF HANDLING IS ACCOMPLISHED WITH A FORKLIFT TRUCK, CONTAINERS SHOULD BE HANDLED FROM A SIDE POSITION WHENEVER POSSIBLE. IF DESIRED, STACKS MAY BE HANDLED BY CAREFULLY PLACING FORKLIFT TINES IN THE SIDE OPENINGS PROVIDED THEREFOR UNDER THE MIDDLE CONTAINER.
 

**CAUTION:** THE USUALLY APPLIED END-HANDLING IS NOT PERMITTED; HOWEVER, FORK TINES MAY BE PLACED UNDER THE "CONTAINER STACK SKID" FROM AN END DIRECTION. CARE MUST BE EXERCISED WHEN INSERTING FORKS UNDER OR WITHDRAWING FORKS FROM UNDER THE "CONTAINER STACK SKID", TO PREVENT DAMAGE TO THE CONTAINERS BY THE FORK TINES OR THE FORKLIFT PACKAGE GUARD.

FOR BLOCKING GUIDANCE APPLICABLE TO A TRAILER HAVING A ROUND FRONT, SEE SPECIAL NOTE 2 ON PAGE 5.

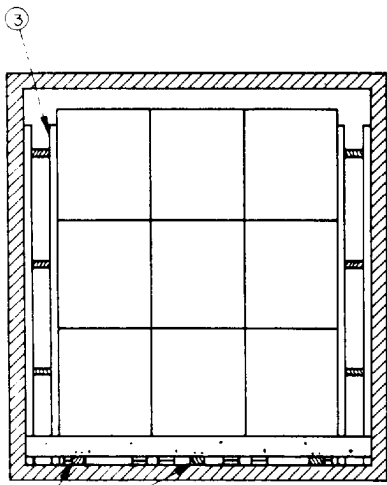


**ISOMETRIC VIEW**

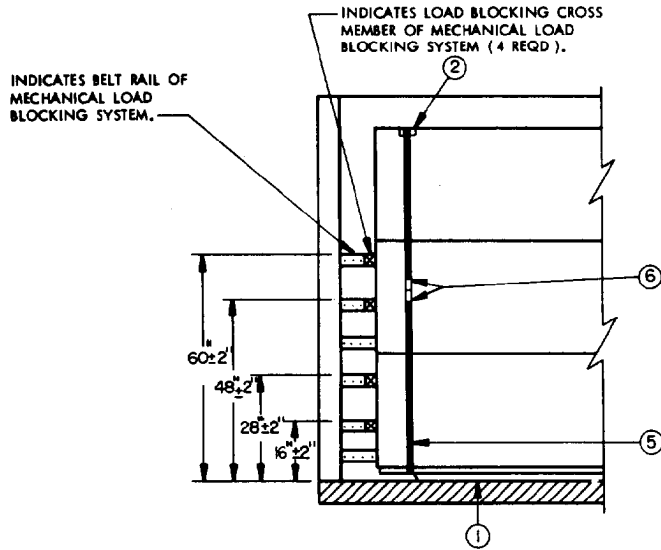
SEE THE "SPECIAL NOTES" ON PAGE 5.

**KEY NUMBERS**

- ① CONTAINER STACK SKID (9 REQD). SEE DETAIL ON PAGE 10 AND "UNITIZATION AND HANDLING PROCEDURES" ON PAGE 3.
- ② BATTEN, 1" X 4" X 21-1/4" (18 REQD). POSITION UNDER UNITIZING STRAP (PIECE MARKED ⑤) AS SHOWN IN "ISOMETRIC VIEW" ABOVE.
- ③ SIDE FILL BLOCKING ASSEMBLY (6 REQD). SEE DETAIL ON PAGE 8.
- ④ REAR FILL ASSEMBLY (1 REQD). SEE DETAIL ON PAGE 8. SEE SPECIAL NOTE 5.
- ⑤ UNITIZING STRAP, 1-1/4" X .035" X 20'-0" (18 REQD). SEE THE "UNITIZATION AND HANDLING PROCEDURES" ON PAGE 3.
- ⑥ SEAL FOR 1-1/4" STRAPPING (36 REQD; 2 PER STRAP JOINT). DOUBLE CRIMP EACH SEAL AS SPECIFIED WITHIN GENERAL NOTE "N" ON PAGE 2.



**SECTION A-A**



**SPECIAL PARTIAL ELEVATION**

SEE SPECIAL NOTE 4 TO THE RIGHT. "SIDE FILL" PIECE MARKED ③ IS OMITTED FOR CLARITY PURPOSES.

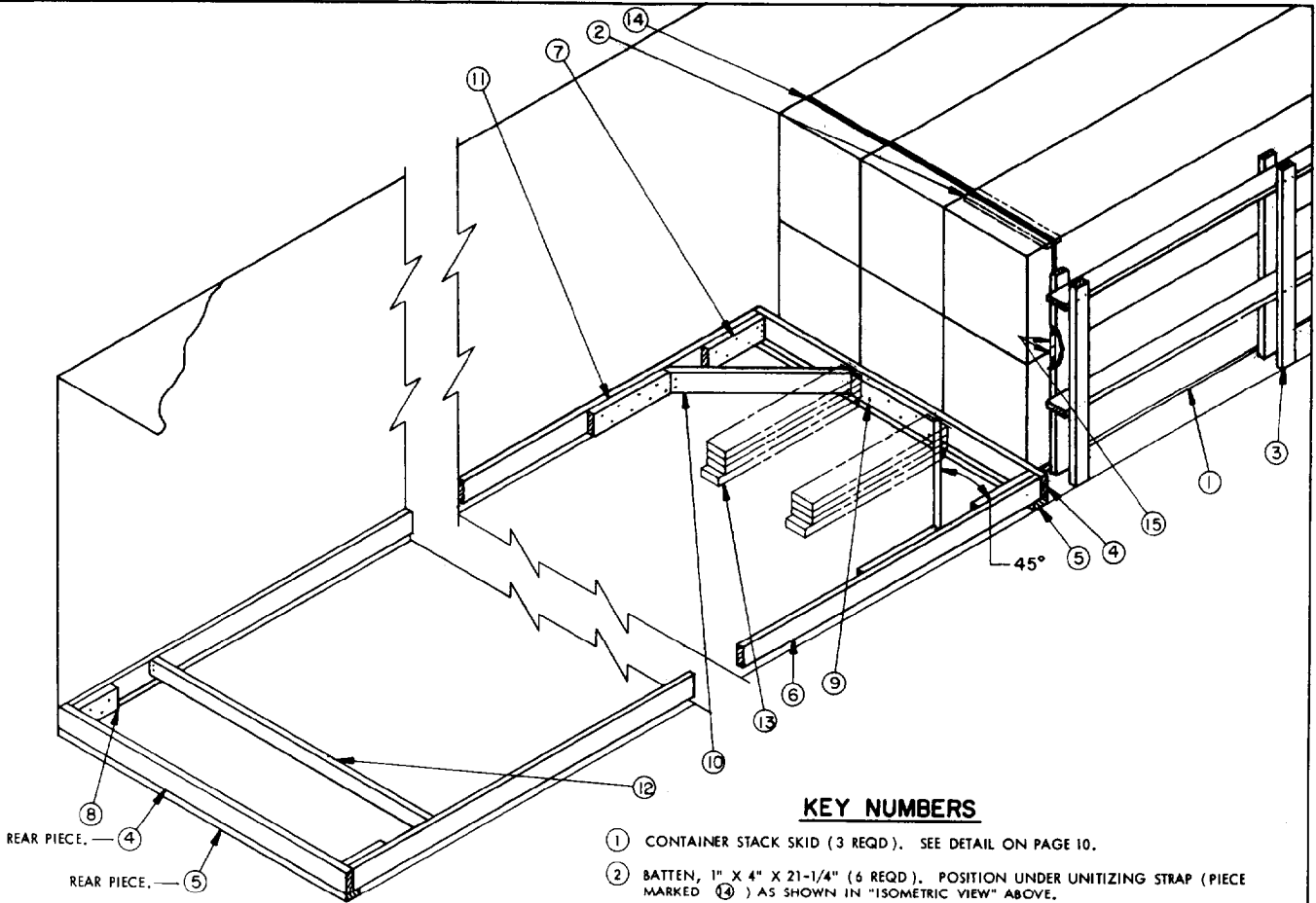
**SPECIAL NOTES:**

1. A 27-UNIT LOAD IS SHOWN IN A 40'-0" LONG BY 7'-8" WIDE (INSIDE DIMENSION) TRAILER. A WIDER OR NARROWER TRAILER MAY BE USED; HOWEVER, A TRAILER SHORTER THAN SHOWN CANNOT BE USED FOR THE SHIPMENT OF THREE (3) LOAD UNITS.
2. A TRAILER EQUIPPED WITH A SQUARE FRONT IS SHOWN. IF THE TRAILER TO BE OUTLOADED HAS ROUNDED FRONT CORNERS, NO CHANGE WILL BE REQUIRED IN THE BLOCKING SHOWN IN THE DEPICTED LOAD; HOWEVER, IF THE TRAILER HAS A ROUND FRONT, ONLY TWO (2) LOAD UNITS MAY BE SHIPPED IN THE TRAILER AND THE "FORWARD BLOCKING" ASSEMBLY SHOWN ON PAGE 9 MAY BE USED.
3. THE DEPICTED LOAD CONFIGURATION MAY BE ADJUSTED TO SATISFY THE QUANTITY OF ITEMS TO BE SHIPPED BY OMITTING A COMPLETE LAYER OR AN ENTIRE LOAD UNIT.
4. IF THE TRAILER BEING OUTLOADED CONTAINS MECHANICAL BRACING DEVICES, SUCH AS WALL BELT RAILS AND LOAD BLOCKING CROSS MEMBERS, WHICH CONFORMS TO SPECIFICATIONS SET FORTH WITHIN THE BUREAU OF EXPLOSIVES PAMPHLET 6C AND THE APPENDICES THERE-TO, THEY MAY BE USED AT THE REAR OF THE LOAD AT SPECIFIED HEIGHTS. SEE "SPECIAL PARTIAL ELEVATION" TO THE LEFT FOR CROSS MEMBER LOCATION AND NUMBER REQUIRED. THE TRAILER EQUIPPED WITH MECHANICAL BRACING DEVICES MUST HAVE A SYSTEM LENGTH OF AT LEAST 38'-11" AS MEASURED FROM THE FRONT WALL OF THE TRAILER. TRAILERS HAVING A SHORTER MECHANICAL SYSTEM WILL BE LIMITED TO A LOAD OF NOT MORE THAN 18 CONTAINERS. SEE GENERAL NOTES "E", "F" AND "G" ON PAGE 2.
5. WHEN THE DISTANCE BETWEEN THE REAR OF THE LOAD AND THE REAR DOORS OF THE TRAILER WHEN THEY ARE CLOSED MEASURES 9" OR MORE, THE ALTERNATE REAR BLOCKING ASSEMBLY AS DETAILED AT THE BOTTOM OF PAGE 10 MAY BE USED IN LIEU OF REAR FILL ASSEMBLY SHOWN AS PIECE MARKED ④.

BILL OF MATERIAL		
LUMBER	LINEAR FEET	BOARD FEET
1" X 4"	32	11
1" X 6"	67	34
2" X 4"	688	459
2" X 6"	291	291
NAILS	NO. REQD	POUNDS
6d (2")	396	2-1/2
10d (3")	668	10-1/2
STEEL STRAPPING, 1-1/4" X .035"	360' REQD	52 LBS
SEAL FOR 1-1/4" STRAPPING	36 REQD	2 LBS

**LOAD AS SHOWN**

ITEM	QUANTITY	WEIGHT (APPROX)
CONTAINER (W/CBU ITEMS)	27	31,185 LBS
DUNNAGE		2,055 LBS
TOTAL WEIGHT		33,240 LBS



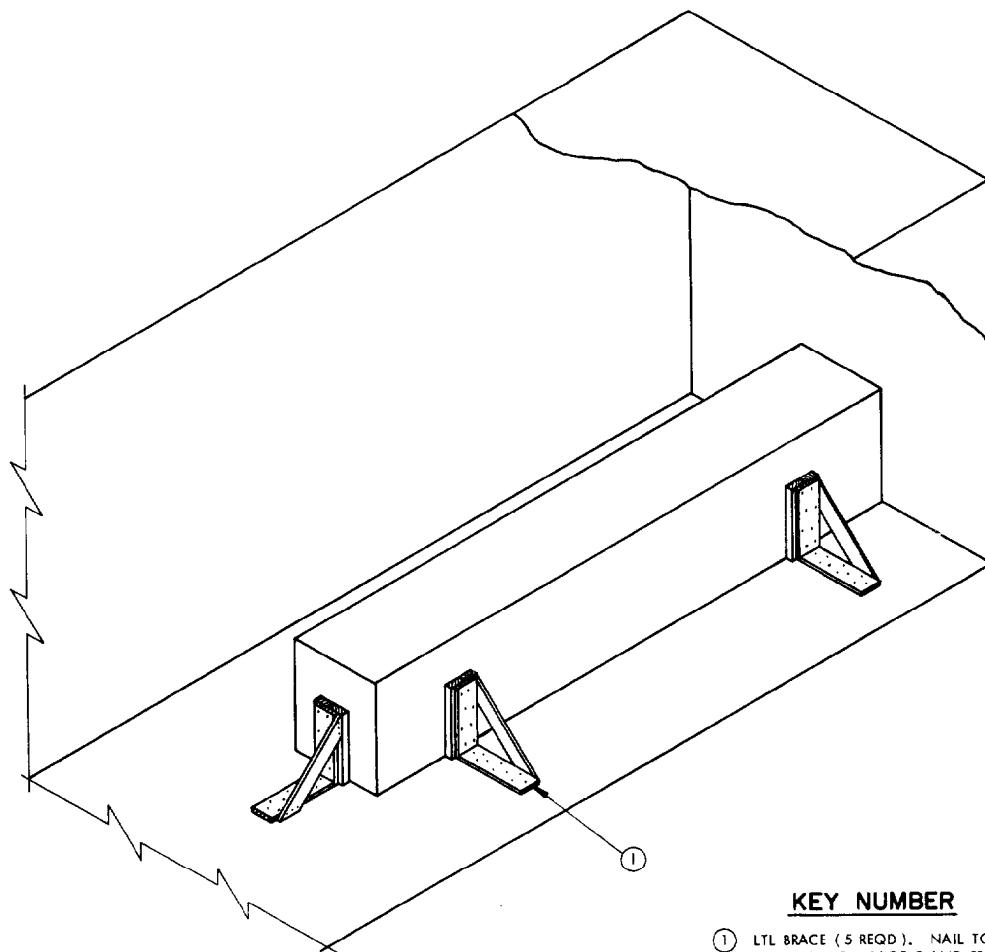
### ISOMETRIC VIEW

#### SPECIAL NOTES:

1. THESE LTL PROCEDURES DEPICT A "K-BRACE" TYPE OF BLOCKING AND A "NAILED-TO-FLOOR" TYPE OF BLOCKING IN A 7'-8" WIDE TRAILER. WIDER OR NARROWER TRAILERS CAN ALSO BE USED.
2. IF THE TRAILER TO BE LOADED HAS A ROUND FRONT, SEE THE "FORWARD BLOCKING" ASSEMBLY ON PAGE 9, WHICH MAY BE USED.
3. THE "K-BRACE" BLOCKING, SHOWN AS PIECES MARKED ⑥ THRU ⑫ IS ADEQUATE FOR RETAINING A MAXIMUM SIZE LTL LOAD OF 18,000 POUNDS.
4. PIECES MARKED ⑬ ARE FOR USE IN A TRAILER WHICH HAS A NAILABLE FLOOR AND MAY BE USED IN LIEU OF REAR PIECES MARKED ④ AND ⑤ AND PIECES MARKED ④ THRU ⑫ WHICH APPLY TO TRAILERS HAVING NON-NAILABLE FLOORS. TWO (2) BACK-UP CLEATS, SHOWN AS PIECES MARKED ⑬, ARE ADEQUATE FOR RETAINING A MAXIMUM SIZE LTL LOAD OF 12,000 POUNDS.

### KEY NUMBERS

- ① CONTAINER STACK SKID (3 REQD). SEE DETAIL ON PAGE 10.
- ② BATTEN, 1" X 4" X 21-1/4" (6 REQD). POSITION UNDER UNITIZING STRAP (PIECE MARKED ⑭) AS SHOWN IN "ISOMETRIC VIEW" ABOVE.
- ③ SIDE FILL BLOCKING (2 REQD). SEE DETAIL ON PAGE 8.
- ④ HEADER, 2" X 6" BY TRAILER WIDTH (CUT TO FIT) (2 REQD).
- ⑤ HEADER SUPPORT AND SIDE STRUT SUPPORT PIECE, 2" X 4" BY TRAILER WIDTH (CUT TO FIT) (2 REQD). NAIL TO BOTTOM EDGE OF HEADER MARKED ④ W/1-10d NAIL EVERY 10".
- ⑥ SIDE STRUT, 2" X 6" BY CUT TO FIT BETWEEN HEADERS MARKED ④ (2 REQD).
- ⑦ POCKET CLEAT, 2" X 6" X 18" (2 REQD). NAIL TO SIDE STRUT MARKED ⑥ W/5-10d NAILS. TOENAIL TO THE ADJACENT HEADER MARKED ④ W/3-12d NAILS.
- ⑧ STRUT RETAINING BLOCK, 2" X 6" X 12" (2 REQD). NAIL TO STRUT MARKED ⑥ W/3-10d NAILS. TOENAIL TO THE ADJACENT HEADER MARKED ④ W/3-12d NAILS.
- ⑨ CENTER CLEAT, 2" X 6" X 24" (1 REQD). NAIL TO HEADER MARKED ④ W/6-10d NAILS.
- ⑩ DIAGONAL BRACE, 2" X 6" BY CUT TO FIT (2 REQD). DOUBLE BEVEL EACH END WITH 45° CUTS. INSTALL AT 45° ANGLE AS SHOWN AND TOENAIL TO THE ADJACENT HEADER MARKED ④ AND TO A SIDE STRUT MARKED ⑥ W/2-16d NAILS AT EACH END.
- ⑪ BACK-UP CLEAT, 2" X 6" X 24" (2 REQD). NAIL TO A SIDE STRUT MARKED ⑥ W/8-10d NAILS.
- ⑫ STRUT BRACING, 2" X 4" BY TRAILER WIDTH MINUS 3-1/4" (CUT TO FIT) (MINIMUM OF 1 REQD). INSTALL ONE (1) NEAR REAR END OF SIDE STRUTS MARKED ⑥ AS SHOWN. ONE (1) ADDITIONAL PIECE IS REQUIRED FOR EVERY 7'-0" OF STRUT LENGTH. TOENAIL TO SIDE STRUTS MARKED ⑥ W/4-12d NAILS AT EACH END. CAUTION: USE CARE WHEN TOENAILING TO PREVENT NAILING THRU AND INTO THE SIDE WALL OF A TRAILER.
- ⑬ FLOOR LINE BACK-UP CLEAT, 2" X 6" X 42" (QUADRUPLED) (2 REQD). ALIGN AS SHOWN IN "ISOMETRIC VIEW" ABOVE. NAIL FIRST PIECE TO TRAILER FLOOR W/10-12d NAILS. NAIL THE SECOND PIECE TO THE FIRST, THE THIRD PIECE TO THE SECOND AND THE FOURTH PIECE TO THE THIRD IN A LIKE MANNER.
- ⑭ UNITIZING STRAP, 1-1/4" X .035" X 16'-0" (6 REQD). SEE THE "UNITIZATION AND HANDLING PROCEDURES" ON PAGE 3.
- ⑮ SEAL FOR 1-1/4" STRAPPING (12 REQD; 2/STRAP JOINT). DOUBLE CRIMP EACH SEAL AS SPECIFIED WITHIN GENERAL NOTE "N" ON PAGE 2.



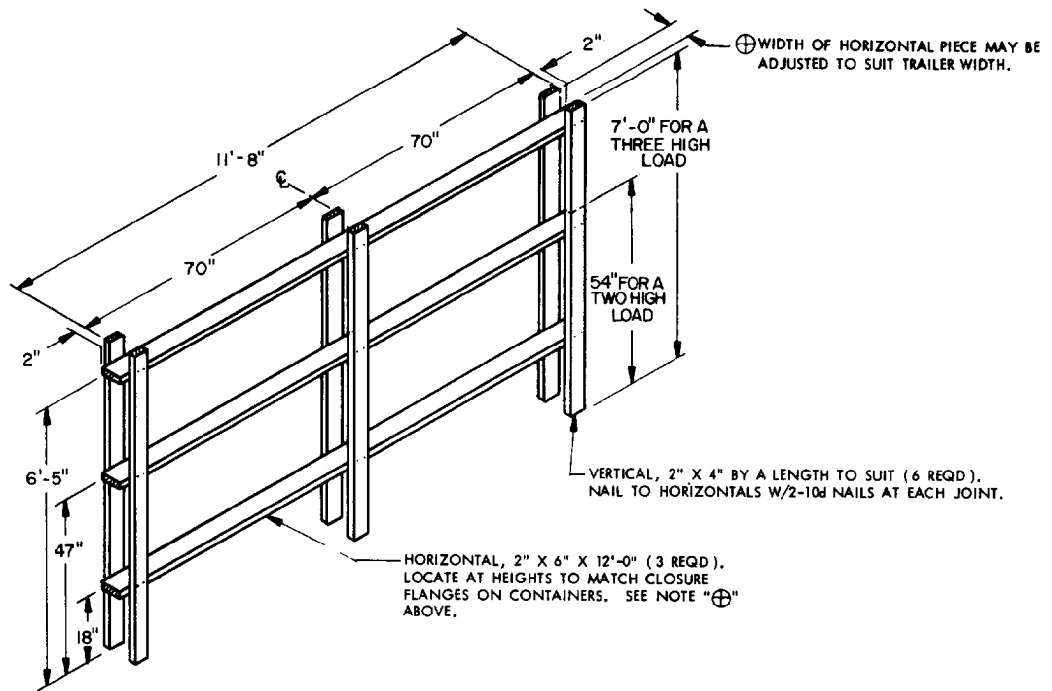
**ISOMETRIC VIEW**

**KEY NUMBER**

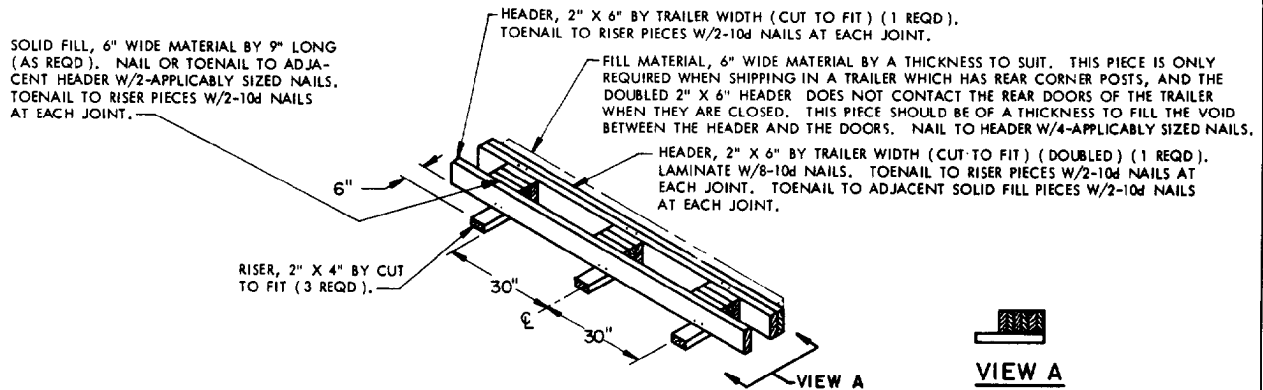
- ① LTL BRACE (5 REQD). NAIL TO TRAILER FLOOR W/10-6d NAILS. SEE DETAIL ON PAGE 9 AND SPECIAL NOTE 2 AT THE LEFT.

**SPECIAL NOTES:**

1. THESE OUTLOADING PROCEDURES DEPICT THE OUTLOADING OF ONE (1) CONTAINER IN A 7'-8" WIDE TRAILER WHICH HAS A NAILABLE FLOOR. WIDER OR NARROWER TRAILERS MAY BE USED; HOWEVER, TRAILERS WITH A NON-NAILABLE FLOOR CANNOT BE USED.
2. NOT LESS THAN TWO (2) LTL BRACES MARKED ① SHALL BE USED AGAINST A SIDE OF A CONTAINER. FOR LONGITUDINAL BRACING, ONE (1) LTL BRACE IS REQUIRED FOR EACH CONTAINER. EACH BRACE AS APPLIED FOR LONGITUDINAL OR LATERAL BRACING WILL SUPPORT 2,000 POUNDS. IF DESIRED, THE LADING ITEM MAY BE LOADED AGAINST ONE OF THE SIDE WALLS OF THE TRAILER AND THUS ELIMINATE THE NEED FOR TWO (2) OF THE SPECIFIED LTL BRACES MARKED AS PIECE ①.



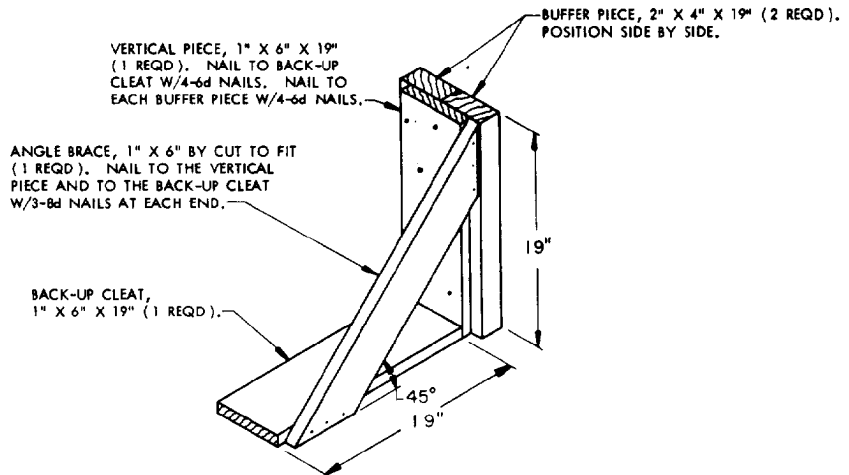
**SIDE FILL BLOCKING**



**REAR FILL ASSEMBLY**

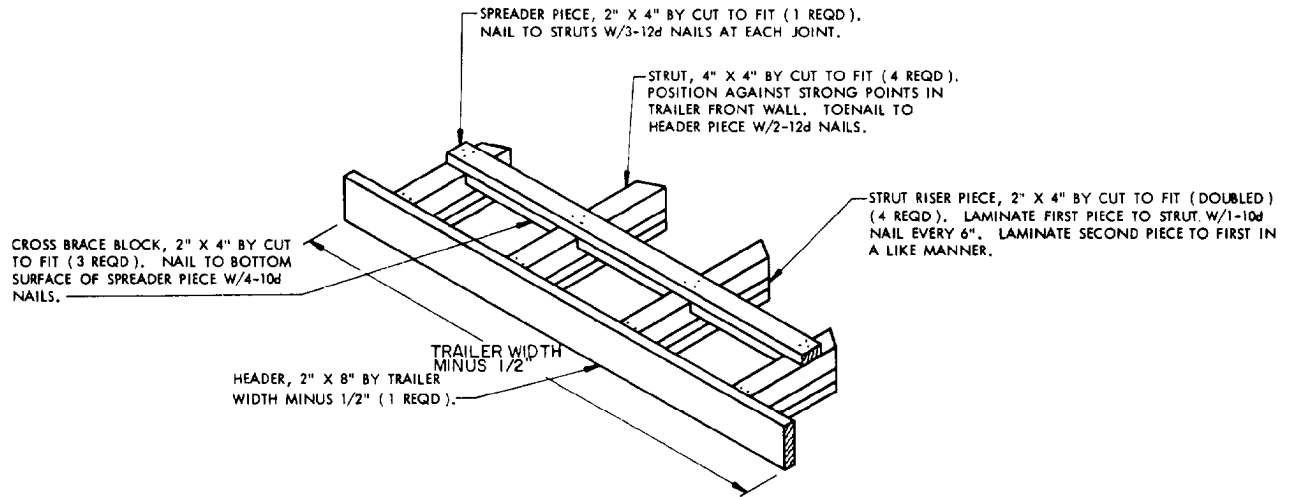
THIS ASSEMBLY SHOWN IN A REVERSED POSITION FOR CLARITY PURPOSES.





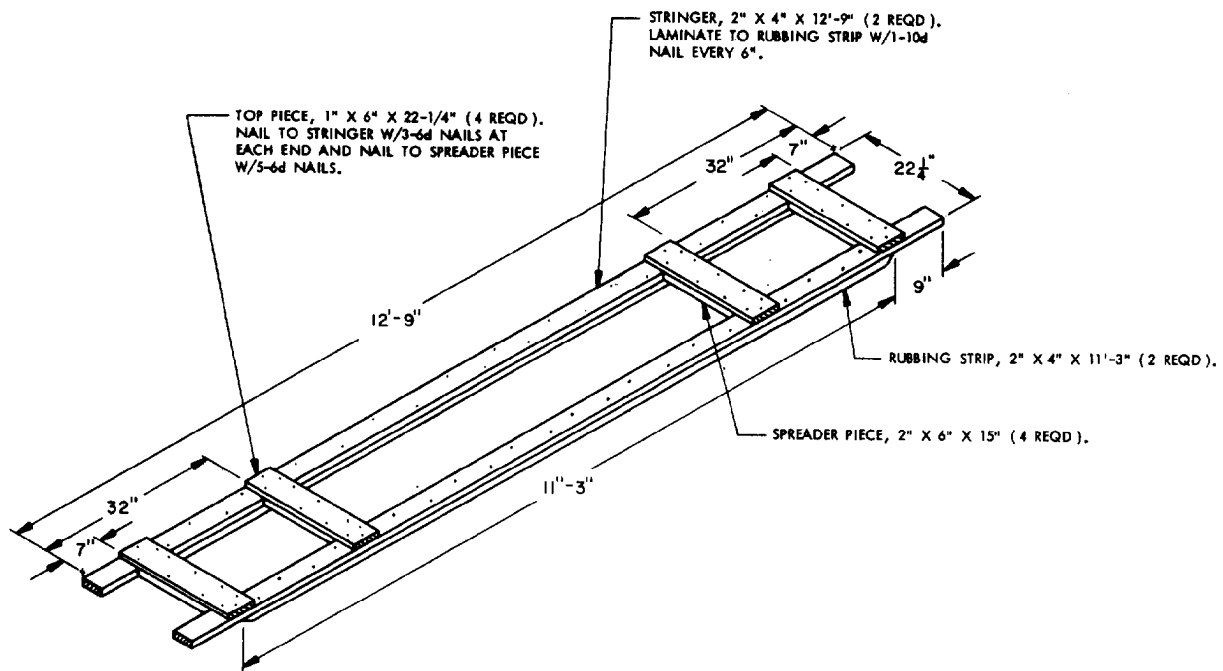
**LTL BRACE**

SEE SPECIAL NOTE 2 ON PAGE 7.



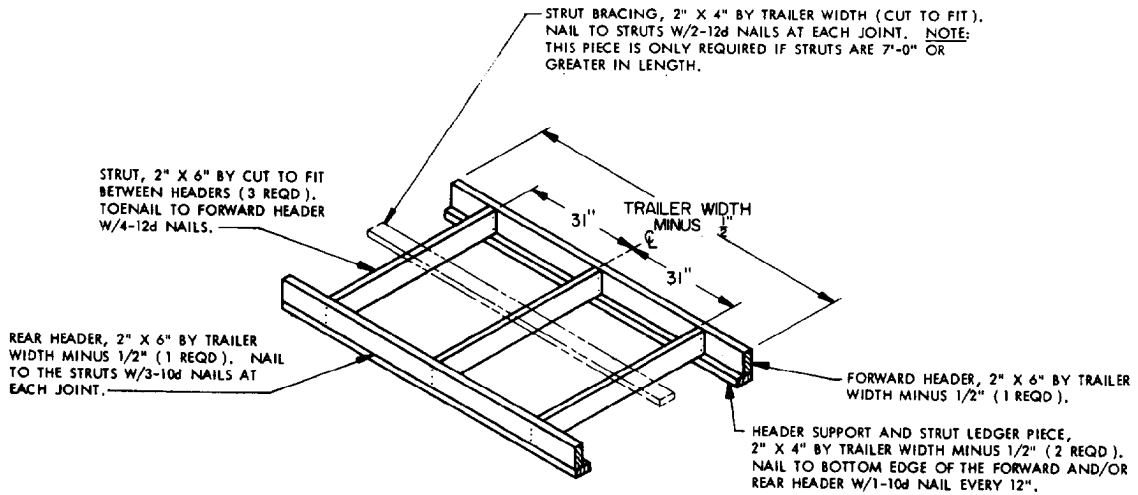
**FORWARD BLOCKING**

THIS "FORWARD BLOCKING" ASSEMBLY IS DESIGNED FOR USE AT THE FORWARD END OF THE LOAD WHICH IS TO BE SHIPPED IN A TRAILER HAVING A ROUND FRONT.



**CONTAINER STACK SKID**

FOR APPLICATION OF SKID, SEE "UNITIZATION AND HANDLING PROCEDURES" ON PAGE 3.



**ALT REAR BLOCKING**

THIS "REAR BLOCKING" ASSEMBLY IS DESIGNED FOR USE AT THE REAR END OF A LOAD WHEN THE DISTANCE BETWEEN THE REAR OF THE LOAD AND THE REAR DOORS OF THE TRAILER WHEN THEY ARE CLOSED MEASURES 9" OR MORE. IF THE DISTANCE AT THE REAR OF THE LOAD MEASURES 60" OR MORE, A K-BRACE TYPE OF BLOCKING, AS DEPICTED ON PAGE 6, MAY BE USED IN LIEU OF THE BLOCKING SHOWN ABOVE.