

# ALL SYSTEMS

## LOADING AND BRACING (TL & LTL) IN CLOSED OR OPEN TOP VAN TRAILERS OF TYPE 1 (HALF SIZE) AND TYPE 2 (STANDARD) CARGO TRANSPORTERS (CONEX CONTAINERS) WHEN LOADED WITH MISSILE/ROCKET ASSOCIATED COMMODITIES\*

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THIS DRAWING SUPERSEDES USAMC  
 DRAWING 19-48-5740-GM 11/16-2.

\* THE OUTLOADING PROCEDURES DELINEATED  
 HEREIN ARE APPLICABLE TO THE MOVEMENT  
 OF CONEX CONTAINERS WHICH ARE LOADED  
 WITH ANY RELATIVELY SMALL COMMODITY,  
 WHETHER AMMUNITION OR GROUND SUPPORT  
 EQUIPMENT ORIENTED. HOWEVER, OTHER  
 REGULATIONS / DIRECTIVES APPLY TO SUCH  
 MATTERS AS SELECTION OF ITEM FOR  
 CONEX MOVEMENT, ITEM PACKAGING  
 SPECIFICATIONS AND SO FORTH.

DO NOT SCALE

DESIGNED <i>WJM</i>	PROJ ENGR <i>RWK</i>	SUBMITTED <i>[Signature]</i>			
DATE <i>6/21</i>	SAC ARMO CTR <i>RTI</i>	CHECKED <i>JPP/WCH</i>	COMMANDER, SARINNA ARMY DEPOT		
<b>REVISIONS</b>			APPROVED <i>[Signature]</i>		
1	28 JUN 68	<i>[Signature]</i>	ARMY MISSILE COMMAND		
			APPROVED BY ORDER OF COMMANDER, ARMY MATERIEL COMMAND		
			<i>[Signature]</i>		
			ARMY SUPPLY & MAINTENANCE COMMAND		
			MATERIEL COMMAND U.S.A.		
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## GENERAL NOTES

(GENERAL NOTES CONTINUED)

- A. THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AMCR 740-13, AND AUGMENTS TM 743-200-1 (CHAPTER 5).
- B. THE OUTLOADING PROCEDURES SPECIFIED HEREIN ARE APPLICABLE TO THE TYPE 1 AND TYPE 2 CARGO TRANSPORTERS. BOTH TYPE 1 AND 2 TRANSPORTERS MAY BE SHIPPED WITHIN THE SAME LOAD BY USING A COMBINATION OF THE PROCEDURES SHOWN HEREIN. SUBSEQUENT REFERENCE TO TYPE 1 OR 2 TRANSPORTER HEREIN MEANS THE CARGO TRANSPORTER WITH CONTENTS. **CAUTION:** THE CONTENTS OF A TYPE 1 OR TYPE 2 TRANSPORTER ARE LIMITED TO NOT MORE THAN 7,800 POUNDS OF LADING EVENLY DISTRIBUTED OVER THE TRANSPORTER FLOOR.
- C. THE LOAD AS SHOWN ON PAGE 3 IS BASED ON THE TYPE 1 TRANSPORTER IN A CLOSED OR OPEN TOP VAN TRAILER 40'-0" LONG BY 7'-6" WIDE (INSIDE DIMENSION) WITH A WOOD, A WOOD AND METAL, OR A METAL FLOOR. THE DELINEATED OUTLOADING PROCEDURES ARE ALSO APPLICABLE TO TRAILERS WHICH ARE 90" THROUGH 93" WIDE.
- D. THE LOAD AS SHOWN ON PAGE 4 IS BASED ON THE TYPE 2 TRANSPORTER IN A CLOSED OR OPEN TOP VAN TRAILER 36'-0" LONG BY 7'-6" WIDE (INSIDE DIMENSION) WITH A WOOD, A WOOD AND METAL, OR A METAL FLOOR. THE DELINEATED OUTLOADING PROCEDURES ARE ALSO APPLICABLE TO TRAILERS WHICH ARE 90" THROUGH 93" WIDE.
- E. FOR DETAILS OF THE CARGO TRANSPORTER, TYPE 1 (1/2 SIZE), SEE MILITARY SPECIFICATION MIL-B-11886 (SEE GENERAL NOTE "S").  
OUTSIDE DIMENSIONS ---- 51" LONG X 75" WIDE X 82-1/2" HIGH.  
TARE WEIGHT ----- 1,050 POUNDS (APPROX).  
MAXIMUM CONTENT WEIGHT ---- 7,800 POUNDS.
- FOR DETAILS OF THE CARGO TRANSPORTER, TYPE 2 (STANDARD), SEE MILITARY SPECIFICATION MIL-B-11886.  
OUTSIDE DIMENSIONS ---- 102" LONG X 75" WIDE X 82-1/2" HIGH.  
TARE WEIGHT ----- 1,430 POUNDS (APPROX).  
MAXIMUM CONTENT WEIGHT ---- 7,800 POUNDS.
- F. SELECTION OF A VEHICLE TO BE USED TO TRANSPORT GUIDED MISSILE AMMUNITION AND/OR COMPONENTS THEREOF MUST COMPLY WITH AR 55-355, CHAPTER 213, FOR EXPLOSIVES OR OTHER DANGEROUS ARTICLES, IN FULL.
- G. THE GROSS WEIGHT AND AXLE DISTRIBUTION OF WEIGHT FOR A LOAD WILL BE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THE APPLICABLE LOADING REQUIREMENTS AND THE SHIPPER WILL LOAD ACCORDINGLY.
- H. **NOTICE:** A SHIPMENT WILL BE POSITIONED IN THE TRAILER CONSISTENT WITH STATE WEIGHT LAWS. THE NUMBER OF UNITS MAY BE ADJUSTED TO FIT THE SIZE OF THE TRAILER TO BE LOADED OR THE QUANTITY TO BE SHIPPED; HOWEVER, THE APPROVED METHODS SHOWN HEREIN MUST BE FOLLOWED AS CLOSELY AS POSSIBLE FOR BLOCKING, BRACING, AND STAYING OF THE DESIGNATED ITEM.
- J. THE SPECIFIED OUTLOADING PROCEDURES ARE APPLICABLE FOR THE SHIPMENT OF CARGO TRANSPORTERS LOADED WITH EXPLOSIVE OR NON-EXPLOSIVE ITEMS AND/OR EMPTY CARGO TRANSPORTERS.
- K. LOADING, BLOCKING AND BRACING METHODS SHOWN HEREIN ARE APPROVED FOR HIGH DENSITY, LOW VOLUME MATERIAL INCLUDING DOT CLASS "A" EXPLOSIVES WHICH ARE INTERNALLY BLOCKED AND BRACED INSIDE THE TRANSPORTER IN ACCORDANCE WITH U. S. ARMY MATERIEL COMMAND APPROVED "FILLING" DRAWINGS OF THE 19-48 SERIES, OR APPLICABLE PARAGRAPHS OF DOT REGULATIONS (TARIFF NO. 19 AND SUBSEQUENT ISSUES THEREOF).
- L. OTHER TYPES OF LADING ITEMS MAY BE LOADED IN THE TRAILERS WHICH ARE PARTIALLY LOADED WITH CARGO TRANSPORTERS, PROVIDING THE TOTAL LOAD IS COMPATIBLE, EXISTING DIRECTIVES ARE NOT VIOLATED, AND THE OTHER LADING ITEMS ARE BLOCKED AND BRACED TO EQUAL THE BLOCKING AND BRACING CRITERIA SPECIFIED HEREIN.
- M. WHEN ANY STRAP IS SEALED AT AN END-OVER-END LAP JOINT, A MINIMUM OF TWO (2) SEALS, BUTTED TOGETHER, WITH TWO (2) CRIMPS PER SEAL MUST BE USED TO SEAL THE JOINT.
- N. FOR TRAILERS NOT EQUIPPED WITH REAR CORNER POSTS, THE REAR BLOCKING MUST CONTACT THE REAR DOORS WHEN THEY ARE CLOSED.
- O. FOR TRAILERS HAVING ROUNDED CORNERS AT THE FORWARD END, THE TRAILER WIDTH 2" X 4" AND/OR 4" X 4" PIECES AT THE FRONT OF THE LOAD SHOULD BE CUT TO A LENGTH WHICH WILL PROVIDE FOR FULL BEARING OF THE PIECES AGAINST THE "SQUARE" PORTION OF THE TRAILER FRONT WALL. FOR TRAILERS HAVING A ROUND FRONT, REFER TO THE "FORWARD BLOCKING" DETAIL ON PAGE 8 FOR THE APPLICABLE BLOCKING SPECIFICATIONS.
- P. PORTIONS OF THE TRAILER BODIES DEPICTED WITHIN THIS PROCEDURAL DRAWING, SUCH AS ONE OF THE SIDE WALLS, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.
- Q. DUNNAGE LUMBER SPECIFIED THROUGHOUT THIS PROCEDURAL DRAWING IS OF NOMINAL SIZE. FOR EXAMPLE, 2" X 4" MATERIAL IS ACTUALLY 1-5/8" THICK BY 3-5/8" WIDE AND 4" X 4" MATERIAL IS ACTUALLY 3-5/8" THICK BY 3-5/8" WIDE.
- R. **NOTICE:** A STAGGERED NAILING PATTERN WILL BE USED WHEN DUNNAGE IS NAILED TO THE FLOOR OF THE TRANSPORTING VEHICLE, OR WHEN LAMINATING DUNNAGE. ALSO, THE NAILING PATTERN FOR AN UPPER PIECE OF LAMINATED DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN THROUGH ONTO OR RIGHT BESIDE A NAIL IN A LOWER PIECE.
- S. CARGO TRANSPORTERS MANUFACTURED IN COMPLIANCE WITH MILITARY SPECIFICATION MIL-B-21560 MAY BE USED FOR THE TRANSPORTATION OF MILITARY EXPLOSIVES AND HAZARD MUNITIONS AND GROUND SUPPORT EQUIPMENT ITEMS; HOWEVER, THE LIMITATIONS SPECIFIED WITHIN GENERAL NOTE "B" WILL APPLY.

## REVISIONS

REVISION NO. 1, DATED 28 JUNE 1968, CONSISTS OF:

1. CHANGE IN DRAWING TITLE.
2. FILE NUMBER CHANGE.
3. ADDITION OF WEIGHT LIMITATION FOR TRANSPORTER CONTENTS.
4. CHANGE IN MATERIAL SPECIFICATIONS.

(GENERAL NOTES CONTINUED AT RIGHT)

## MATERIAL SPECIFICATIONS

LUMBER ----- : SEE TM 743-200-1, DUNNAGE LUMBER; FED SPEC MM-L-751.

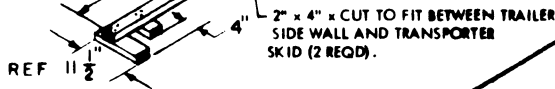
NAILS ----- : COMMON, CEMENT COATED OR CHEMICALLY ETCHED,  
FED SPEC FF-N-105.  
ALT: ANNULAR-RING TYPE NAIL OF SAME SIZE.

STEEL STRAPPING: TYPE I OR IV, CLASS A OR B OR C, FED SPEC QQ-S-781.  
FOR FSN SEE SB-38-100.

STRAP SEAL ---- : COMMERCIAL GRADE.

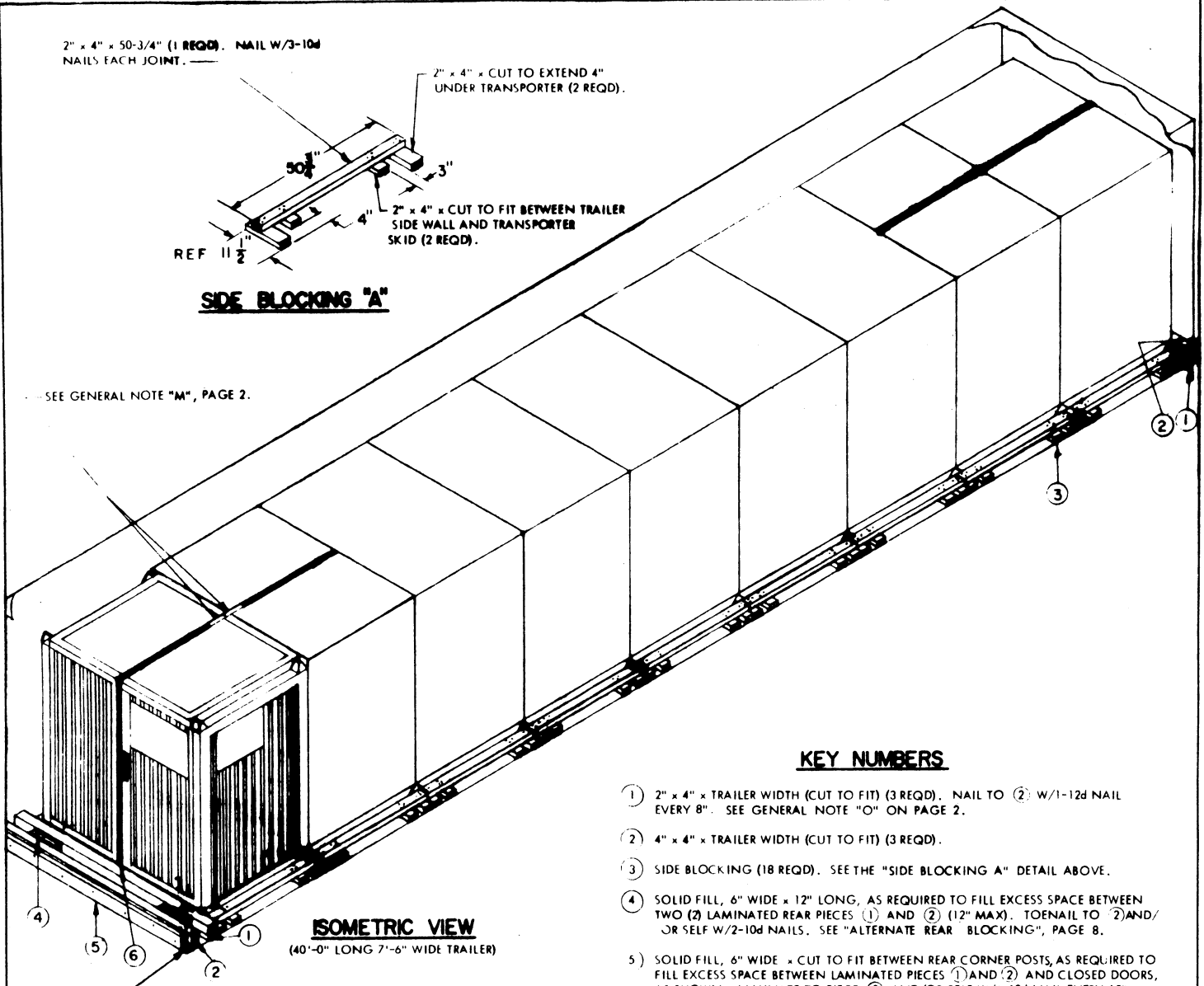
2" x 4" x 50-3/4" (1 REQD). NAIL W/3-10d  
NAILS EACH JOINT.

2" x 4" x CUT TO EXTEND 4"  
UNDER TRANSPORTER (2 REQD).



**SIDE BLOCKING "A"**

SEE GENERAL NOTE "M", PAGE 2.



**ISOMETRIC VIEW**

(40'-0" LONG 7'-6" WIDE TRAILER)

REAR CORNER POST OF TRAILER.  
SEE GENERAL NOTE "N", PAGE 2.

**KEY NUMBERS**

- ① 2" x 4" x TRAILER WIDTH (CUT TO FIT) (3 REQD). NAIL TO ② W/1-12d NAIL EVERY 8". SEE GENERAL NOTE "O" ON PAGE 2.
- ② 4" x 4" x TRAILER WIDTH (CUT TO FIT) (3 REQD).
- ③ SIDE BLOCKING (18 REQD). SEE THE "SIDE BLOCKING A" DETAIL ABOVE.
- ④ SOLID FILL, 6" WIDE x 12" LONG, AS REQUIRED TO FILL EXCESS SPACE BETWEEN TWO ② LAMINATED REAR PIECES ① AND ② (12" MAX). TO NAIL TO ② AND/OR SELF W/2-10d NAILS. SEE "ALTERNATE REAR BLOCKING", PAGE 8.
- ⑤ SOLID FILL, 6" WIDE x CUT TO FIT BETWEEN REAR CORNER POSTS, AS REQUIRED TO FILL EXCESS SPACE BETWEEN LAMINATED PIECES ① AND ② AND CLOSED DOORS, AS SHOWN. LAMINATE TO PIECE ② AND/OR SELF W/1-10d NAIL EVERY 12". NOT REQUIRED FOR TRAILERS WITHOUT REAR CORNER POSTS.
- ⑥ STEEL STRAPPING, 1-1/4" x .035" x 31'-0" (2 REQD). TO ENCIRCLE THE TWO MOST FORWARD TRANSPORTERS AND TWO MOST REARWARD TRANSPORTERS ADJACENT TO HEADERS.

**BILL OF MATERIAL**

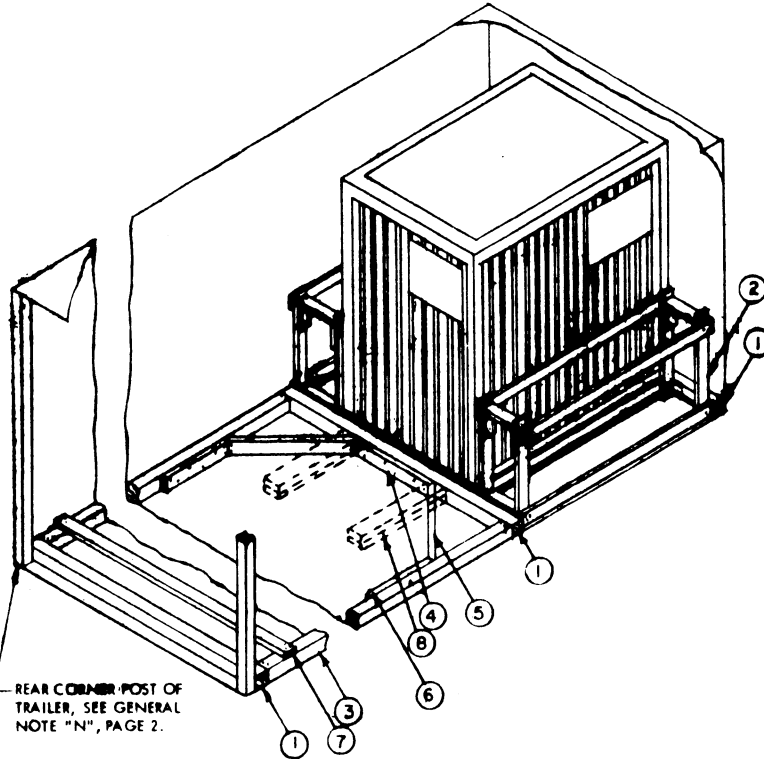
LUMBER	LINEAR FEET	BOARD FEET
1" x 6"	2	1
2" x 4"	156	103
2" x 6"	18	18
4" x 4"	23	31
NAILS		
	NO. REQD	POUNDS
10d (3")	246	4
12d (3-1/4")	24	1/2
STEEL STRAPPING, 1 1/4" x .035" --- 62' REQD --- 9 LBS		

**LOAD AS SHOWN**

ITEM	QUANTITY	WEIGHT (APPROX)
TYPE I CARGO TRANSPORTER	9	39,492 LBS SEE NOTE "O"
DUNNAGE		397 LBS
<b>TOTAL WEIGHT</b>		<b>39,889 LBS (APPROX)</b>

(\*) WEIGHT OF TRUCK LOAD SHOWN WAS COMPUTED BY USING 4,388 POUNDS OF GROSS WEIGHT PER TRANSPORTER. THE NUMBER OF TRANSPORTERS THAT MAY BE SHIPPED PER VEHICLE AND THE GROSS WEIGHT OF THE LOAD WILL DEPEND UPON THE WEIGHT OF THE COMMODITIES BEING SHIPPED. SEE GENERAL NOTE "G", PAGE 2.

**9-UNIT LOAD IN A 40'-0" LONG BY 7'-6" WIDE  
TRAILER (TYPE I CARGO TRANSPORTER)**



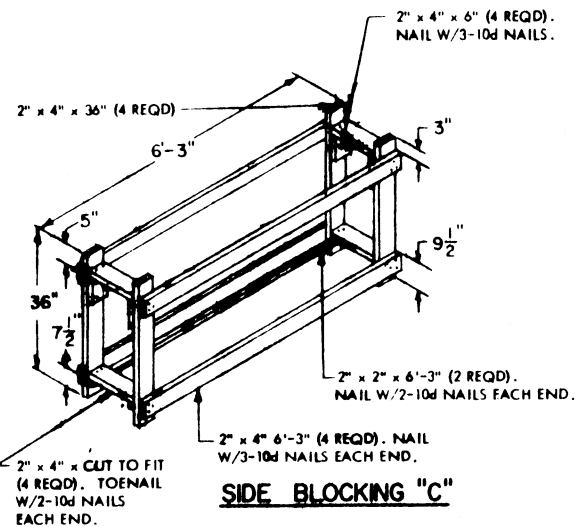
REAR CORNER POST OF TRAILER, SEE GENERAL NOTE "N", PAGE 2.

**ISOMETRIC VIEW**

**KEY NUMBERS**

- ① 4" x 4" x TRAILER WIDTH (CUT TO FIT) (3 REQD). SEE GENERAL NOTE "O" ON PAGE 2.
- ② SIDE BLOCKING (2 REQD). SEE "SIDE BLOCKING "C" AND NOTE "●" BELOW.
- ③ 4" x 4" x CUT TO FIT (2 REQD).
- ④ 2" x 4" x 30" (1 REQD). NAIL TO ① W/6-12d NAILS.
- ⑤ 2" x 4" x CUT TO FIT (2 REQD). DOUBLE BEVEL EACH END 45°. TOENAIL TO ① AND ③ W/2-10d NAILS EACH END.
- ⑥ 2" x 4" x 24" (2 REQD). NAIL TO ③ W/6-12d NAILS.
- ⑦ 2" x 4" x TRAILER WIDTH (CUT TO FIT) (MIN 1 REQD). POSITION EVERY 7'-0". NAIL TO ③ W/3-12d NAILS EACH END.
- ⑧ 2" x 4" AS REQD (DOUBLED) FOR WOOD OR WOOD AND METAL FLOOR TRAILERS. DELETE REAR PIECE ① AND PIECES ③ THRU ⑦. NAIL FIRST PIECE TO FLOOR WITH 12d NAILS AND SECOND PIECE TO FIRST IN LIKE MANNER. SEE CHART "A" BELOW. SEE GENERAL NOTE "R" ON PAGE 2.

**NOTE ●:**  
WHEN MORE THAN ONE TYPE I CARGO TRANSPORTER IS SHIPPED, USE THE METHOD SHOWN ON PAGE 3 AND USE CHART A. SEE THE "ALTERNATE REAR BLOCKING" DETAIL ON PAGE 8 FOR BLOCKING SPECIFICATIONS WHICH MUST BE USED IF THE LOAD WEIGHT EXCEEDS 26,000 POUNDS.

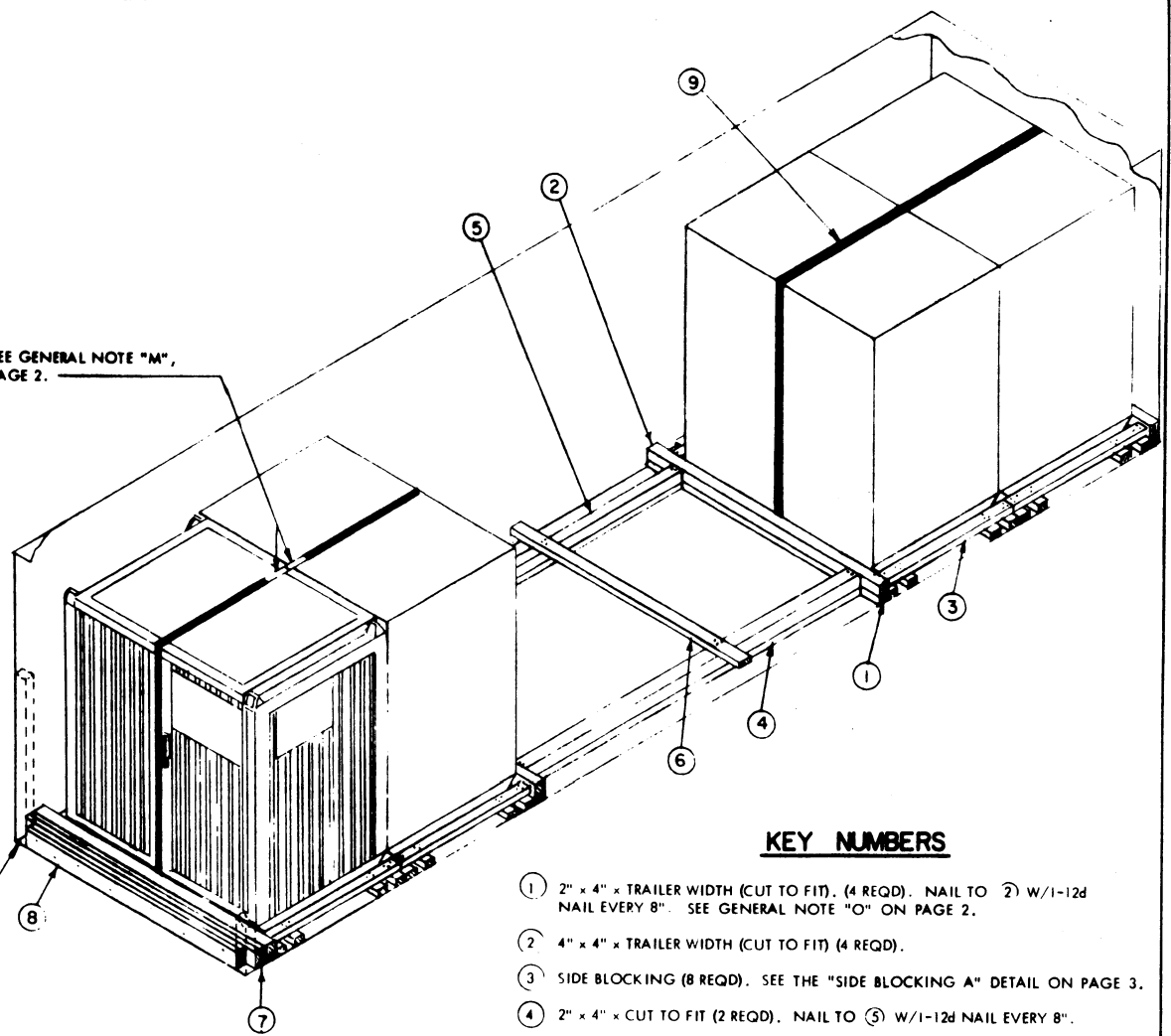


**SIDE BLOCKING "C"**

CHART "A"		
MAX GROSS WT	BACKUPS	NAILS
0 TO 10,000	2 - 30"	7
10,000 TO 13,000	2 - 36"	9
13,000 TO 15,000	2 - 42" OR 3 - 30"	11 OR 7
15,000 TO 20,000	4 - 30"	7

SEE GENERAL NOTE "M",  
PAGE 2.

REAR CORNER POST OF  
TRAILER. SEE GENERAL  
NOTE "N", PAGE 2.



**ISOMETRIC VIEW**

METHOD OF BLOCKING SHOWN ABOVE IS TYPICAL FOR TYPE I TRANSPORTER WHEN COMMODITY WEIGHT IN TRANSPORTER IS MAXIMUM AND/OR NEAR MAXIMUM. SEE GENERAL NOTE "G", PAGE 2.

**KEY NUMBERS**

- ① 2" x 4" x TRAILER WIDTH (CUT TO FIT). (4 REQD). NAIL TO ② W/1-12d NAIL EVERY 8". SEE GENERAL NOTE "O" ON PAGE 2.
- ② 4" x 4" x TRAILER WIDTH (CUT TO FIT) (4 REQD).
- ③ SIDE BLOCKING (8 REQD). SEE THE "SIDE BLOCKING A" DETAIL ON PAGE 3.
- ④ 2" x 4" x CUT TO FIT (2 REQD). NAIL TO ⑤ W/1-12d NAIL EVERY 8".
- ⑤ 4" x 4" x CUT TO FIT (2 REQD). TOENAIL TO ② W/2-16d NAILS EACH END.
- ⑥ 2" x 4" x TRAILER WIDTH (CUT TO FIT). POSITION EVERY 7'-0". NAIL TO ⑤ W/3-12d NAIL EACH JOINT.
- ⑦ SOLID FILL, 6" WIDE x TRAILER WIDTH (CUT TO FIT) (AS REQD) (12" MAX). NAIL TO ② AND/OR SELF W/1-12d NAIL EVERY 12". SEE "ALTERNATE REAR BLOCKING", PAGE 8 FOR BLOCKING SPECIFICATIONS WHICH ARE APPLICABLE IF THE SPACE AT THE REAR OF THE LOAD EXCEEDS 12".
- ⑧ SOLID FILL, 6" WIDE x CUT TO FIT BETWEEN REAR CORNER POSTS, AS REQUIRED TO FILL EXCESS SPACE BETWEEN PIECE ② AND CLOSED DOORS, AS SHOWN. LAMINATE TO PIECE ② AND/OR SELF W/1-10d NAIL EVERY 12". NOT REQUIRED FOR TRAILERS WITHOUT REAR CORNER POSTS.
- ⑨ STEEL STRAPPING, 1 1/4" x .035" x 31'-0" (2 REQD).

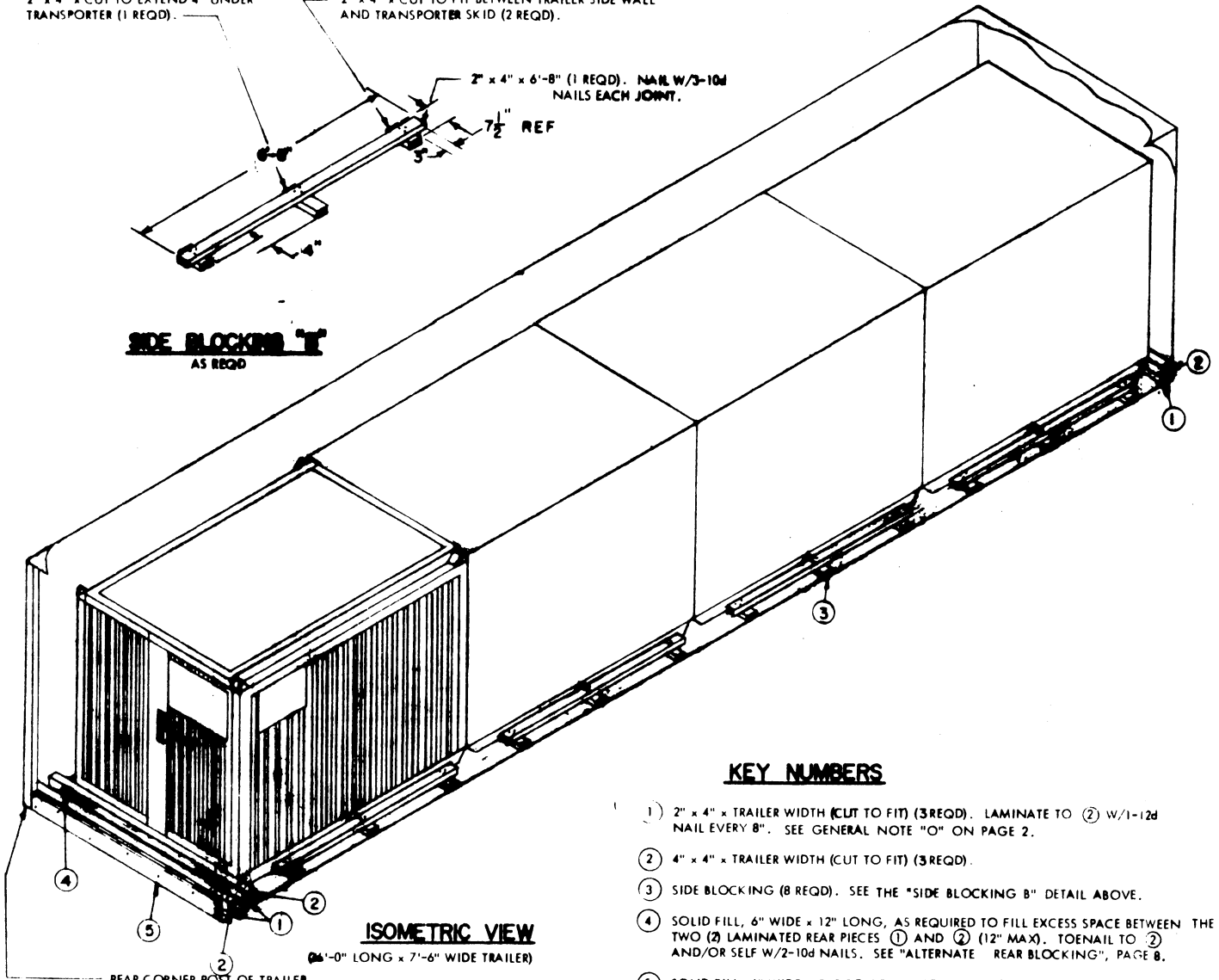
2" x 4" x CUT TO EXTEND 4" UNDER TRANSPORTER (1 REQD).

2" x 4" x CUT TO FIT BETWEEN TRAILER SIDE WALL AND TRANSPORTER SKID (2 REQD).

2" x 4" x 6'-8" (1 REQD). NAIL W/3-10d NAILS EACH JOINT.

7 1/2" REF

**SIDE BLOCKING "B"**  
AS REQD



**ISOMETRIC VIEW**

36'-0" LONG x 7'-6" WIDE TRAILER

REAR CORNER POST OF TRAILER. SEE GENERAL NOTE "N", PAGE 2.

**KEY NUMBERS**

- 1) 2" x 4" x TRAILER WIDTH (CUT TO FIT) (3 REQD). LAMINATE TO 2) W/1-12d NAIL EVERY 8". SEE GENERAL NOTE "O" ON PAGE 2.
- 2) 4" x 4" x TRAILER WIDTH (CUT TO FIT) (3 REQD).
- 3) SIDE BLOCKING (8 REQD). SEE THE "SIDE BLOCKING B" DETAIL ABOVE.
- 4) SOLID FILL, 6" WIDE x 12" LONG, AS REQUIRED TO FILL EXCESS SPACE BETWEEN THE TWO (2) LAMINATED REAR PIECES 1) AND 2) (12" MAX). TOENAIL TO 2) AND/OR SELF W/2-10d NAILS. SEE "ALTERNATE REAR BLOCKING", PAGE 8.
- 5) SOLID FILL, 6" WIDE x CUT TO FIT BETWEEN REAR CORNER POSTS, AS REQUIRED TO FILL EXCESS SPACE BETWEEN LAMINATED PIECES 1) AND 2) AND CLOSED DOORS, AS SHOWN. LAMINATE TO PIECE 2) AND/OR SELF W/1-10d NAIL EVERY 12". NOT REQUIRED FOR TRAILERS WITHOUT REAR CORNER POSTS.

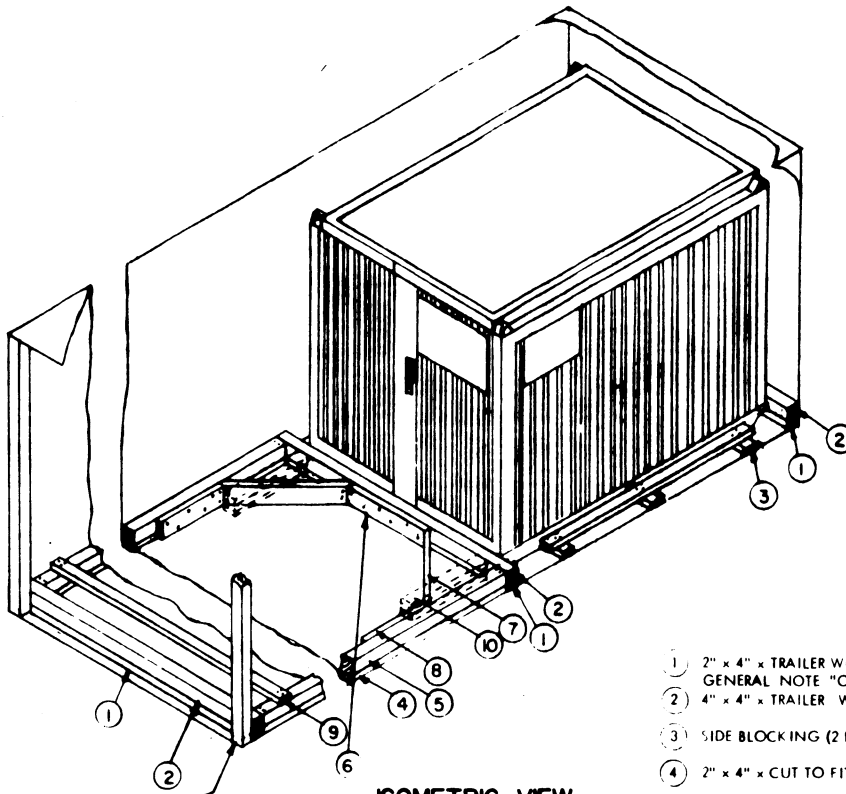
**BILL OF MATERIAL**

LUMBER	LINEAR FEET	BOARD FEET
1" x 6"	6	3
2" x 4"	94	32
2" x 6"	20	20
4" x 4"	23	31
NAILS	NO. REQD	POUNDS
10d (3")	110	1-3/4
12d (3-1/4")	24	1/2

**LOAD AS SHOWN**

ITEM	QUANTITY	WEIGHT (APPROX)
TYPE 2 CARGO TRANSPORTER-----	4-----	41,720 LBS SEE NOTE "O"
DUNNAGE-----		197 LBS
<b>TOTAL WEIGHT-----</b>		<b>41,917 LBS (APPROX)</b>

⊗ WEIGHT OF TRUCKLOAD SHOWN WAS COMPUTED BY USING 9,000 POUNDS OF GROSS WEIGHT PER TRANSPORTER. SEE GENERAL NOTE "G", PAGE 2.



**ISOMETRIC VIEW**  
(TYPE 2 CARGO TRANSPORTER)

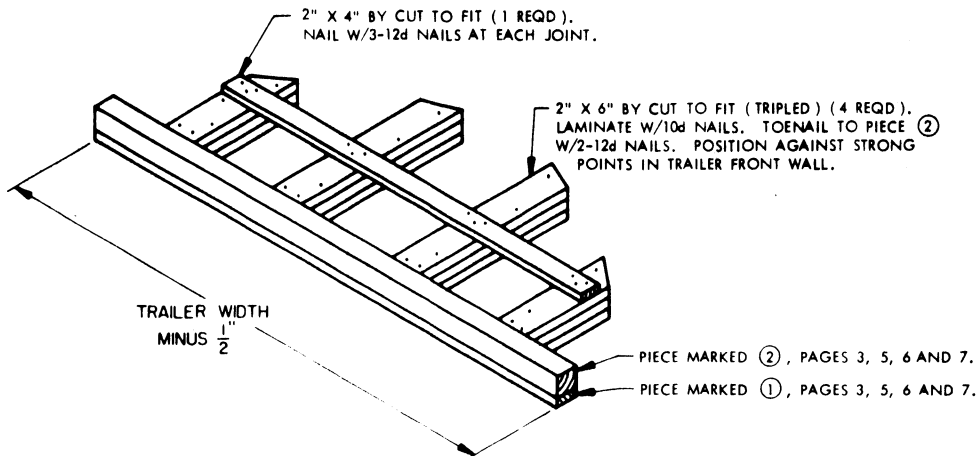
REAR CORNER POST OF TRAILER. SEE GENERAL NOTE "N", PAGE 2.

**KEY NUMBERS**

- ① 2" x 4" x TRAILER WIDTH (3 REQD). LAMINATE TO ② W/1-12d NAIL EVERY 8". SEE GENERAL NOTE "O" ON PAGE 2.
- ② 4" x 4" x TRAILER WIDTH (CUT TO FIT) (3 REQD).
- ③ SIDE BLOCKING (2 REQD). SEE THE "SIDE BLOCKING B" DETAIL ON PAGE 6.
- ④ 2" x 4" x CUT TO FIT (2 REQD). LAMINATE TO ⑤ W/1-12d NAIL EVERY 8".
- ⑤ 4" x 4" x CUT TO FIT (2 REQD). TOENAIL TO ② W/2-16d NAILS EACH END.
- ⑥ 2" x 6" x 30" (1 REQD). NAIL TO ① AND/OR ② W/6-12d NAILS.
- ⑦ 2" x 6" x CUT TO FIT (2 REQD). DOUBLE BEVEL EACH END 45°. TOENAIL TO ① AND/OR ② AND ④ AND/OR ⑤ W/2-16d NAILS EACH END.
- ⑧ 2" x 6" x 24" (2 REQD). NAIL TO ④ AND/OR ⑤ W/6-12d NAILS.
- ⑨ 2" x 4" x TRAILER WIDTH (CUT TO FIT) (MIN 1 REQD). POSITION EVERY 7'-0". NAIL TO ⑥ W/3-12d NAILS EACH END.
- ⑩ 2" x 4" x LENGTH TO SUIT (TRIPLED) (AS REQD). FOR WOOD OR WOOD AND METAL FLOOR TRAILERS DELETE REAR PIECES ① AND ② AND PIECES ④ THRU ⑨. NAIL FIRST PIECE TO TRAILER FLOOR W/12d NAILS. NAIL EACH ADDITIONAL PIECE TO FIRST IN LIKE MANNER. TOENAIL TO ② W/2-12d NAILS. SEE CHART "B" BELOW. SEE GENERAL NOTE "R" ON PAGE 2.

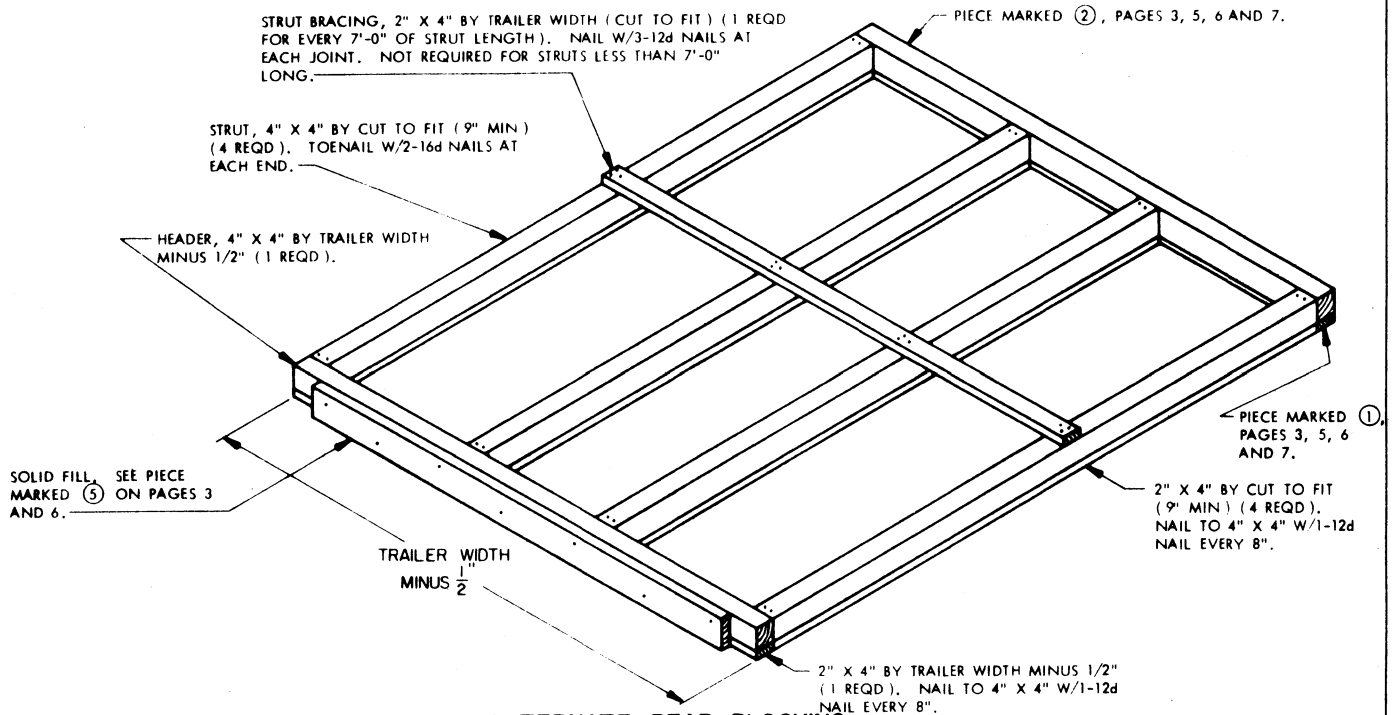
**CHART "B"**

MAX GROSS WT	BACKUPS	NAILS
0 TO 8,000	2 - 30"	7
8,000 TO 10,000	2 - 36"	9
10,000 TO 13,000	2 - 42"	11
13,000 TO 16,000	4 - 30"	7
16,000 TO 20,000	4 - 36"	9
20,000 TO 26,000	4 - 42"	11
LOADS TO 26,000	K-BRACE AS SHOWN MAY BE USED	
OVER 26,000	SEE "ALTERNATE REAR BLOCKING", PAGE 8.	



### FORWARD BLOCKING

THIS BLOCKING IS APPLICABLE FOR A TRAILER HAVING A ROUND FRONT.  
REFER TO GENERAL NOTE "O" ON PAGE 2 FOR PROVISIONS APPLICABLE  
TO A ROUNDED-CORNER TRAILER.



### ALTERNATE REAR BLOCKING

THIS BLOCKING IS TO BE USED AT THE REAR OF A TRAILER  
IN LIEU OF THE SOLID FILL BLOCKING SHOWN ON PAGES 3,  
5 AND 6 WHEN THE EXCESS SPACE IS GREATER THAN 12";  
OR, IT IS TO BE USED IN LIEU OF THE "K-BRACE" BLOCKING  
SHOWN ON PAGE 7 IF THE EXCESS SPACE AT THE REAR OF  
THE TRAILER IS LESS THAN 60", OR IF THE LADING WEIGHT  
IS GREATER THAN 26,000 POUNDS.

### DETAILS