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LOADING AND BRACING[⊕] IN END OPENING ISO CONTAINERS OF ROCKET AND LAUNCHER, 83MM (SMAW-D) M141 BUNKER DEFEAT MUNITION, PACKED IN CYLINDRI- CAL METAL CONTAINERS, PALLET- ZED

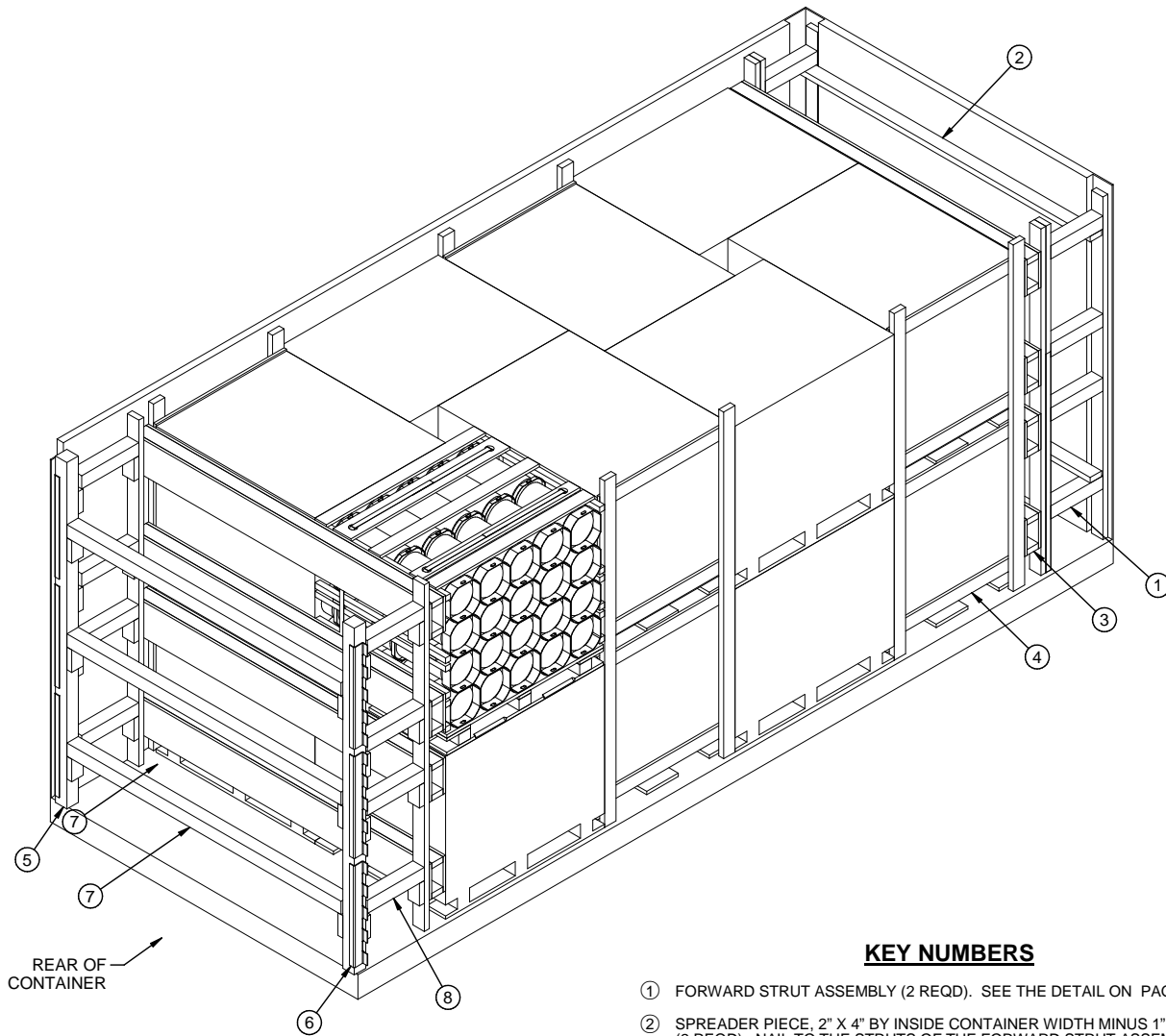
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*THE PROCEDURES SHOWN HEREIN ARE APPLICABLE TO LOADS THAT ARE TO BE SHIPPED BY CONTAINER-ON-FLATCAR(COFC) RAIL, MOTOR, OR WATER CARRIERS.

U.S. ARMY MATERIEL COMMAND DRAWING

APPROVED, U.S. ARMY JOINT MUNITIONS COMMAND RUS.ALLEN.J .1230354282 <small>Digitally signed by RUS.ALLEN.J.1230354282 DN: c=US, ou=U.S. Government, ou=DoD, ou=PKI, ou=USA, cn=RUS.ALLEN.J.1230354282 Date: 2010.09.28 09:18:11 -05'00'</small>		CAUTION: VERIFY PRIOR TO USE AT WWW.DAC.ARMY.MIL THAT THIS IS THE MOST CURRENT VERSION OF THIS DOCUMENT. THIS IS PAGE 1 OF 8.			
		DO NOT SCALE		JUNE 2010	
		ENGINEER OR TECHNICIAN	BASIC REV.	RANDY DUNN	
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U.S. ARMY DEFENSE AMMUNITION CENTER		ENGINEERING DIRECTORATE	BEAVER.JERRY .W.1230949952 <small>Digitally signed by BEAVER.JERRY.W.1230949952 DN: c=US, ou=U.S. Government, ou=PKI, ou=USA, cn=BEAVER.JERRY.W.1230949952 Date: 2010.09.07 14:45:16 -05'00'</small>	19	48
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ISOMETRIC VIEW

KEY NUMBERS

- ① FORWARD STRUT ASSEMBLY (2 REQD). SEE THE DETAIL ON PAGE 7.
- ② SPREADER PIECE, 2" X 4" BY INSIDE CONTAINER WIDTH MINUS 1" (REF: 7'-7") (2 REQD). NAIL TO THE STRUTS OF THE FORWARD STRUT ASSEMBLIES W/2-10d NAILS AT EACH END.
- ③ FORWARD/REAR BLOCKING ASSEMBLY (2 REQD). SEE THE DETAIL ON PAGE 6. NAIL THROUGH THE BUFFER PIECES INTO THE VERTICAL PIECES OF THE FORWARD STRUT ASSEMBLIES W/6-10d NAILS. **NOTE:** STRUT LEDGERS NOT REQUIRED ON THE FORWARD BLOCKING ASSEMBLY, ONLY ON THE REAR BLOCKING ASSEMBLY.
- ④ SIDE FILL ASSEMBLY (4 REQD). SEE THE DETAIL ON PAGE 7.
- ⑤ DOOR POST VERTICAL (2 REQD). SEE THE DETAIL ON PAGE 7, "DETAIL A" ON PAGE 8, AND GENERAL NOTE "Q" ON PAGE 3.
- ⑥ UNIVERSAL LOAD RETAINER (6 REQD, 3 PER SIDE). NAIL THROUGH THE HOLES INTO THE DOOR POST VERTICAL W/2-10d NAILS. SEE DEPARTMENT OF ARMY DRAWING DA-116, "DETAIL A" ON PAGE 8, AND GENERAL NOTE "Q" ON PAGE 3.
- ⑦ DOOR SPANNER, 4" X 4" MATERIAL CUT TO A LENGTH THAT WILL PROVIDE A DRIVE FIT (REF: 7'-1-1/4") (3 REQD). TOENAIL TO THE DOOR POST VERTICAL W/2-12d NAILS AT EACH END. SEE THE "BEVEL-CUT" DETAIL ON PAGE 8.
- ⑧ STRUT, 4" X 4" BY CUT-TO-FIT (REF: 17'-1/2") (8 REQD). TOENAIL TO THE BUFFER PIECES OF THE REAR BLOCKING ASSEMBLY AND TO THE DOOR POST VERTICAL W/2-12d NAILS AT EACH END. SEE THE "BEVEL-CUT" DETAIL ON PAGE 8.

BILL OF MATERIAL

LUMBER	LINEAR FEET	BOARD FEET
1" X 4"	27	9
2" X 4"	270	180
4" X 4"	59	78
NAI LS	NO. REQD	POUNDS
6d (2")	384	2-1/4
10d (3")	140	2-1/4
12d (3-1/4")	76	1-1/4
PLYWOOD, 3/4" - -	96.06 SQ FT REQD - - -	199 LBS
UNIVERSAL LOAD RETAINER - -	6 REQD - - -	39 LBS

LOAD AS SHOWN

ITEM	QUANTITY	WEIGHT (APPROX)
PALLET UNIT - - - - -	16 - - - - -	18,400 LBS
DUNNAGE - - - - -	- - - - -	777 LBS
CONTAINER - - - - -	- - - - -	4,700 LBS
TOTAL WEIGHT - - - - -		23,877 LBS (APPROX)

GENERAL NOTES

- A. THIS DOCUMENT HAS BEEN PREPARED AND ISSUED IN ACCORDANCE WITH AR 740-1 AND AUGMENTS TM 743-200-1 (CHAPTER 5).
- B. THE OUTLOADING PROCEDURES SPECIFIED IN THIS DRAWING ARE APPLICABLE TO PALLETIZED SMAW-D BUNKER DEFEAT ROCKETS AND LAUNCHERS PACKED IN CYLINDRICAL METAL CONTAINERS. SUBSEQUENT REFERENCE TO PALLET UNIT THEREIN MEANS PALLET UNIT WITH SMAW-D. SEE PAGE 4 AND U. S. ARMY MATERIEL COMMAND DRAWING 19-48-4079/13-20PM 1002. **CAUTION:** REGARDLESS OF THE QUANTITY OF PALLET UNITS TO BE SHIPPED, THE "MAXIMUM GROSS WEIGHT" OF THE END OPENING ISO CONTAINER MUST NOT BE EXCEEDED.
- C. THE LOAD AS SHOWN IS BASED ON A 4,700 POUND 20' LONG BY 8' WIDE BY 8'-6" HIGH END OPENING ISO CONTAINER WITH INSIDE DIMENSIONS OF 19'-4" LONG BY 92" WIDE BY 93" HIGH, WITH A MAXIMUM GROSS WEIGHT OF 52,910 POUNDS. OLDER/OTHER CONTAINERS MAY HAVE A TOTAL INSIDE HEIGHT OF 95", BUT A CLEAR HEIGHT UNDER THE ROOF BOWS OF 93". VERIFY INSIDE CONTAINER HEIGHT PRIOR TO FABRICATING DUNNAGE. THE LOAD IS DESIGNED FOR TRAILER/CONTAINER-ON-FLATCAR (T/COFC) SHIPMENT, HOWEVER, THE LOAD AS DESIGNED CAN ALSO BE MOVED BY OTHER SURFACE MODES OF TRANSPORT. **NOTICE:** OTHER CONTAINERS OF THE SAME DESIGN CONFIGURATION CAN BE USED.
- D. WHEN LOADING PALLET UNITS, THEY ARE TO BE POSITIONED SO AS TO ACHIEVE A TIGHT LOAD (TIGHT AGAINST THE DUNNAGE ASSEMBLIES). THE UNBLOCKED SPACE ACROSS THE WIDTH OF A LOAD BAY IS NOT TO EXCEED 1-1/2". EXCESSIVE SLACK CAN BE ELIMINATED FROM A LOAD BY LAMINATING ADDITIONAL PIECES OF APPROPRIATE THICKNESS TO THE HORIZONTAL PIECES ON THE SIDE FILL ASSEMBLIES. NAIL EACH ADDITIONAL PIECE W/1 APPROPRIATELY SIZED NAIL EVERY 12". ADDITIONALLY, THE THICKNESS OF THE SIDE FILL ASSEMBLIES MAY BE ADJUSTED AS REQUIRED TO FACILITATE VARIANCE IN THE SIZE OF THE PALLET UNIT.
- E. DUNNAGE LUMBER SPECIFIED IS OF NOMINAL SIZE. FOR EXAMPLE, 1" X 4" MATERIAL IS ACTUALLY 3/4" THICK BY 3-1/2" WIDE AND 2" X 6" MATERIAL IS ACTUALLY 1-1/2" THICK BY 5-1/2" WIDE.
- F. A STAGGERED NAILING PATTERN WILL BE USED WHENEVER POSSIBLE WHEN NAILS ARE DRIVEN INTO JOINTS OF DUNNAGE ASSEMBLIES OR WHEN LAMINATING DUNNAGE. ADDITIONALLY, THE NAILING PATTERN FOR AN UPPER PIECE OF LAMINATED DUNNAGE WILL BE ADJUSTED AS REQUIRED SO THAT A NAIL FOR THAT PIECE WILL NOT BE DRIVEN THROUGH ONTO OR RIGHT BE-SIDE A NAIL IN A LOWER PIECE.
- G. IN SOME CONTAINERS THERE IS A SLOT AT THE CORNERS OF THE FORWARD WALL. PIECES OF DUNNAGE MATERIAL MUST BE LAMINATED TO THE BUFFER PIECES ON THE FORWARD STRUT ASSEMBLIES TO PROVIDE A FLAT SURFACE FOR THE BUFFER PIECES. A PIECE OF 2" X 4", 2" X 3" OR A SPECIAL WIDTH PIECE CUT-TO-FIT CAN BE USED. THIS FILL PIECE WILL BE NAILED WITH ONE APPROPRIATELY SIZED NAIL EVERY 12". NOTE THAT SOME CONTAINERS ARE EQUIPPED WITH "TIE-BARS" IN THE CORNER SLOT, WHICH PRECLUDE THE USE OF A FULL HEIGHT FILL PIECE. WHEN "TIE-BARS" ARE PRESENT, THE FILL PIECE MUST BE INSTALLED IN SEGMENTS DESIGNED TO FIT BETWEEN THE "TIE-BARS" VERTICALLY. THE FILL PIECE(S) IS NOT REQUIRED WHEN THE CORNER PORTIONS OF THE CONTAINER FORWARD WALL ARE SMOOTH AND FLAT. DO NOT ALLOW ANY DUNNAGE ASSEMBLY TO CONTACT THE CONTAINER FORWARD WALL, ONLY THE CORNER POSTS OF THE CONTAINER SHOULD BE USED FOR FORWARD LONGITUDINAL BLOCKING.
- H. WHETHER A CONTAINER IS FULL OR IS LOADED WITH A REDUCED QUANTITY OF LADING UNITS, THE LENGTHWISE CENTER OF GRAVITY OF THE LOAD MUST BE WITHIN 12", IN EITHER DIRECTION, OF THE MID-POINT OF THE CONTAINER.
- J. **CAUTION:** DO NOT NAIL DUNNAGE MATERIAL TO THE CONTAINER WALLS OR FLOOR. ALL NAILING WILL BE WITHIN THE DUNNAGE.
- K. PORTIONS OF THE CONTAINER DEPICTED WITHIN THIS DRAWING, SUCH AS THE SIDEWALL, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.
- L. MAXIMUM LOAD WEIGHT CRITERIA:

THE MAXIMUM LOAD WEIGHTS ARE CONTROLLED BY EQUIPMENT CAPABILITY FACTORS. ALTHOUGH THE HEAVIEST MAXIMUM LOADS ARE DELINEATED IN THE LOAD VIEWS, PROVISIONS ARE INCLUDED WITHIN THIS DRAWING SO THAT THE BASIC LOADS CAN BE ADJUSTED TO SATISFY A LESSER QUANTITY OF LADING UNITS. DEPENDING ON TRANSPORTATION ROUTING, IT MAY BE NECESSARY TO REDUCE THE LOAD WEIGHT TO SATISFY "WEIGHT LAWS" OF CERTAIN STATES. ALSO, IT MAY BE NECESSARY TO REDUCE THE LOAD WEIGHT TO SATISFY OTHER WEIGHT RESTRICTIONS IMPOSED ON THE INTERMODAL CONTAINER SYSTEM.

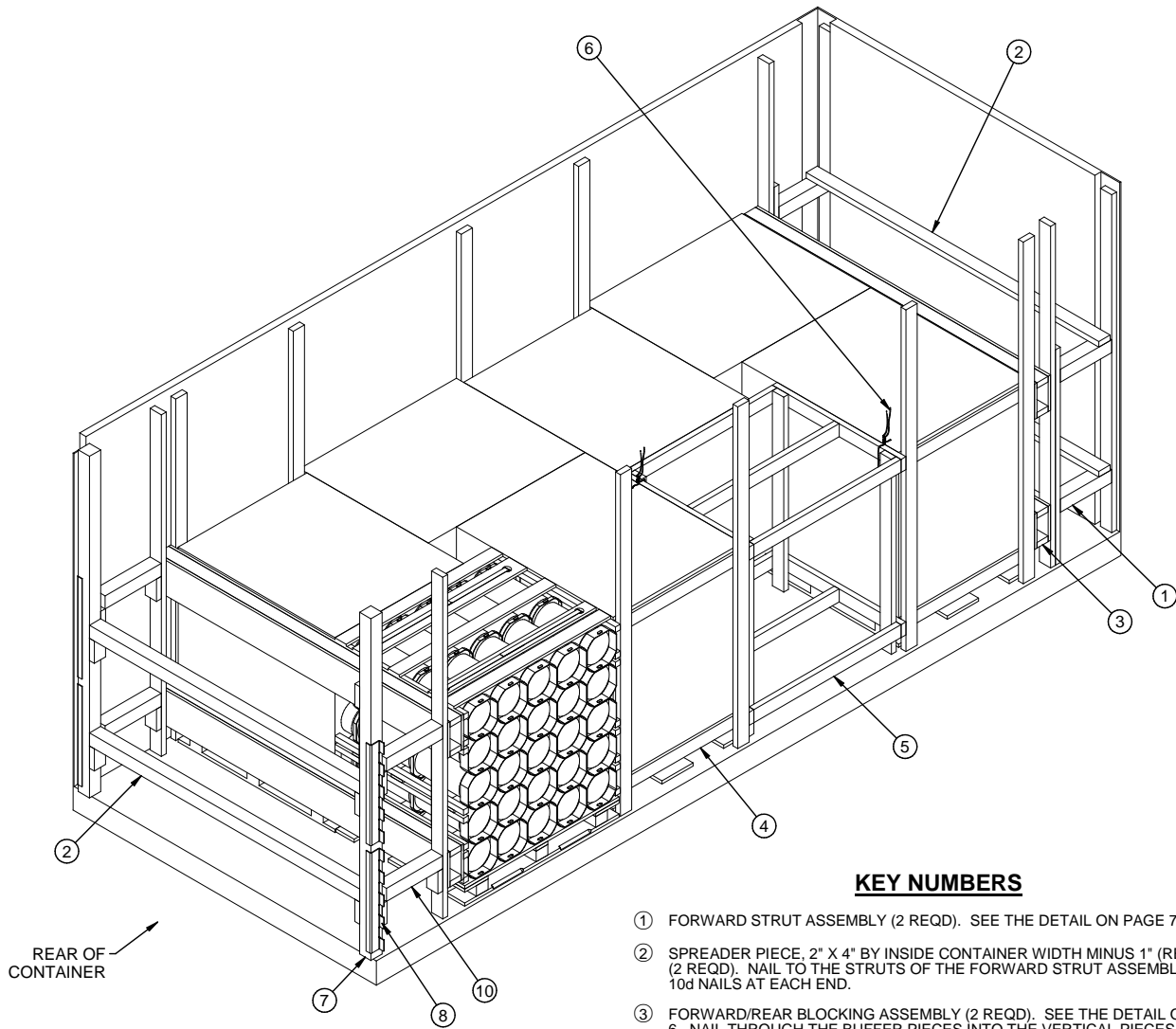
(CONTINUED AT RIGHT)

(GENERAL NOTES CONTINUED)

- M. REQUIREMENTS CITED WITHIN THE ASSOCIATION OF AMERICAN RAILROADS (AAR) INTERMODAL LOADING GUIDE APPLY WHEN THE SHIPMENT MOVES BY TRAILER/CONTAINER-ON-FLATCAR (T/COFC). SPECIAL T/COFC NOTES FOLLOW:
1. A LOADED CONTAINER MUST BE ON A CHASSIS EQUIPPED WITH TWO BOGIE ASSEMBLIES WHEN BEING MOVED IN TOFC SERVICE.
 2. THE LOAD LIMIT OF A T/COFC RAILCAR MUST NOT BE EXCEEDED, NOR WILL A CAR BE LOADED SO THAT THE TRUCK UNDER ONE END OF THE CAR CARRIES MORE THAN ONE-HALF OF THE LOAD LIMIT FOR THAT CAR.
- N. DURING INTRASTATE AND/OR INTERSTATE MOVES BY MOTOR CARRIER, A PROPER CHASSIS OR MODIFIED FLATBED TRAILER MUST BE USED TO PRECLUDE VIOLATION OF ONE OR MORE "WEIGHT LAWS" APPLICABLE TO THE STATE OR STATES INVOLVED.
- O. CONVERSION TO METRIC EQUIVALENTS: DIMENSIONS WITHIN THIS DOCUMENT ARE EXPRESSED IN INCHES AND WEIGHTS ARE EXPRESSED IN POUNDS. WHEN NECESSARY, THE METRIC EQUIVALENTS MAY BE COMPUTED ON THE BASIS OF ONE INCH EQUALS 25.4MM AND ONE POUND EQUALS 0.454 KG.
- P. THE QUANTITY OF PALLET UNITS SHOWN IN THE LOADS ON PAGES 2 AND 4 MAY BE REDUCED OR INCREASED FOR SHIPMENT, IF DESIRED. SEE THE FILLER ASSEMBLY ON PAGE 5.
1. IF A LOAD IS REDUCED BY ONLY A SMALL AMOUNT (ONE, TWO, OR THREE LADING UNITS), LADING UNITS NORMALLY MAY BE ELIMINATED FROM THE CENTER OF THE LOAD.
 2. IF A LOAD IS REDUCED BY A LARGE AMOUNT (MORE THAN THREE LADING UNITS), LADING UNITS SHOULD BE ELIMINATED AS REQUIRED AND THE TOTAL LOAD SHIFTED FORE OR AFT, AS NECESSARY, TO ACHIEVE A SYMMETRICAL WEIGHT DISTRIBUTION. THE DEPICTED PROCEDURES WILL BE FOLLOWED AS CLOSELY AS POSSIBLE, MAKING ONLY THOSE ADJUSTMENTS TO THE DUNNAGE WHICH ARE REQUIRED TO ACCOMMODATE THE NUMBER OF UNITS TO BE SHIPPED.
- Q. SIX UNIVERSAL LOAD RETAINERS, AS DEPICTED IN THE LOAD ON PAGE 2, ARE REQUIRED FOR TWO HIGH LOADS, AND FOUR ARE REQUIRED FOR SINGLE LAYER LOADS. REFER TO DAC DRAWING ACV00682 FOR DETAILS OF THE UNIVERSAL LOAD RETAINER CONSTRUCTION, AND TO DEPARTMENT OF THE ARMY DRAWING DA-116 FOR DETAILS FOR INSTALLATION TO THE DOOR POST VERTICAL, PLACEMENT INTO THE CONTAINER, AND FOR OTHER METHODS OF REAR-OF-LOAD RESTRAINT.
- R. ANTI-CHAFING MATERIAL MAY BE INSTALLED AT POINTS OF CONTACT BETWEEN PALLET UNITS OR BETWEEN PALLET UNITS AND THE END OPENING CONTAINER, IF DESIRED, TO PREVENT CHAFING DAMAGE TO CONTAINER PAINT AND MARKINGS.

MATERIAL SPECIFICATIONS

- LUMBER** - - - - - : SEE TM 743-200-1 (DUNNAGE LUMBER) AND VOLUNTARY PRODUCT STANDARD PS 20.
- NAILS** - - - - - : ASTM F1667; COMMON STEEL NAIL (NLCMS OR NLCMMS).
- PLYWOOD** - - - - - : COMMERCIAL ITEM DESCRIPTION A-A-55057, INDUSTRIAL PLYWOOD, INTERIOR WITH EXTERIOR GLUE, GRADE C-D. IF SPECIFIED GRADE IS NOT AVAILABLE, A BETTER INTERIOR OR AN EXTERIOR GRADE MAY BE SUBSTITUTED.
- WIRE, CARBON STEEL** - : ASTM A853; ANNEALED AT FINISH, BLACK OXIDE FINISH, 0.0800" DIA, GRADE 1006 OR BETTER.
- ANTI-CHAFING MATERIAL** - - - - - : MIL-PRF-121 (OR EQUAL); NEUTRAL BARRIER MATERIAL.
- STEEL, STRUCTURAL** - - - - - : ASTM A36; 36,000 PSI MINIMUM YIELD OR BETTER.



ISOMETRIC VIEW

KEY NUMBERS

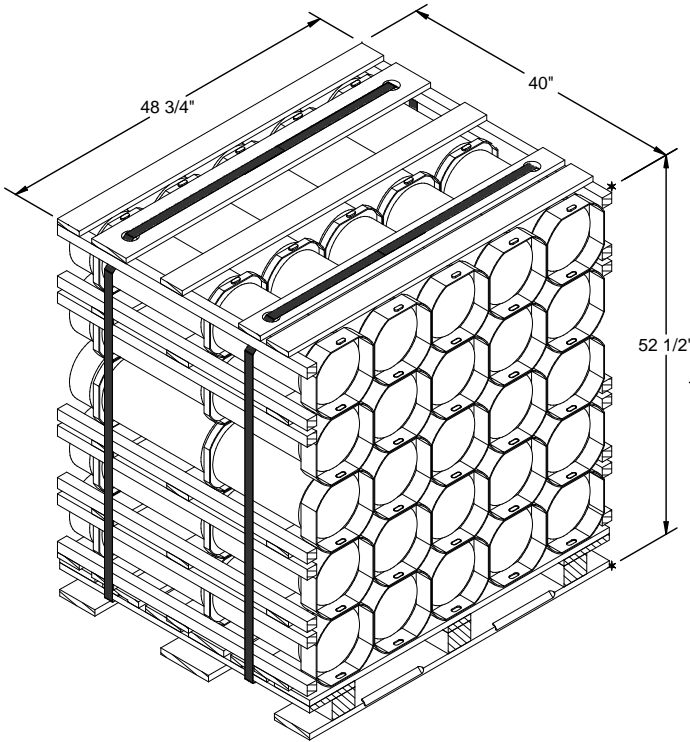
- ① FORWARD STRUT ASSEMBLY (2 REQD). SEE THE DETAIL ON PAGE 7.
- ② SPREADER PIECE, 2" X 4" BY INSIDE CONTAINER WIDTH MINUS 1" (REF: 7'-7") (2 REQD). NAIL TO THE STRUTS OF THE FORWARD STRUT ASSEMBLIES W/2-10d NAILS AT EACH END.
- ③ FORWARD/REAR BLOCKING ASSEMBLY (2 REQD). SEE THE DETAIL ON PAGE 6. NAIL THROUGH THE BUFFER PIECES INTO THE VERTICAL PIECES OF THE FORWARD STRUT ASSEMBLIES W/6-10d NAILS. **NOTE:** STRUT LEDGERS NOT REQUIRED ON THE FORWARD BLOCKING ASSEMBLY, ONLY ON THE REAR BLOCKING ASSEMBLY.
- ④ SIDE FILL ASSEMBLY (4 REQD). SEE THE DETAIL ON PAGE 7.
- ⑤ FILLER ASSEMBLY (1 SHOWN). SEE THE DETAIL ON PAGE 5.
- ⑥ TIE WIRE, .0800" DIA WIRE 24" LONG (2 REQD). INSTALL AROUND BOARDS IN BOTH THE FILLER ASSEMBLY AND THE PALLET UNIT, OR AROUND PALLET UNIT STRAP. BRING ENDS TOGETHER AND TWIST TAUT.
- ⑦ DOOR POST VERTICAL (2 REQD). SEE THE DETAIL ON PAGE 7, "DETAIL A" ON PAGE 8, AND GENERAL NOTE "Q" ON PAGE 3.
- ⑧ UNIVERSAL LOAD RETAINER (4 REQD, 2 PER SIDE). NAIL THROUGH THE HOLES INTO THE DOOR POST VERTICAL W/2-10d NAILS. SEE DEPARTMENT OF ARMY DRAWING DA-116, "DETAIL A" ON PAGE 8, AND GENERAL NOTE "Q" ON PAGE 3.
- ⑨ DOOR SPANNER, 4" X 4" MATERIAL CUT TO A LENGTH THAT WILL PROVIDE A DRIVE FIT (REF: 7'-1-1/4") (2 REQD). TOENAIL TO THE DOOR POST VERTICAL W/2-12d NAILS AT EACH END. SEE THE "BEVEL-CUT" DETAIL ON PAGE 8.
- ⑩ STRUT, 4" X 4" BY CUT-TO-FIT (REF: 17-1/2") (4 REQD). TOENAIL TO THE BUFFER PIECES OF THE REAR BLOCKING ASSEMBLY AND TO THE DOOR POST VERTICAL W/2-12d NAILS AT EACH END. SEE THE "BEVEL-CUT" DETAIL ON PAGE 8.

BILL OF MATERIAL

LUMBER	LINEAR FEET	BOARD FEET
1" X 4"	14	5
2" X 4"	266	177
4" X 4"	41	54
NAI LS	NO. REQD	POUNDS
6d (2")	192	1-1/4
10d (3")	124	2
12d (3-1/4")	56	1
PLYWOOD, 3/4"	48.03 SQ FT REQD	100 LBS
UNIVERSAL LOAD RETAINER	4 REQD	26 LBS

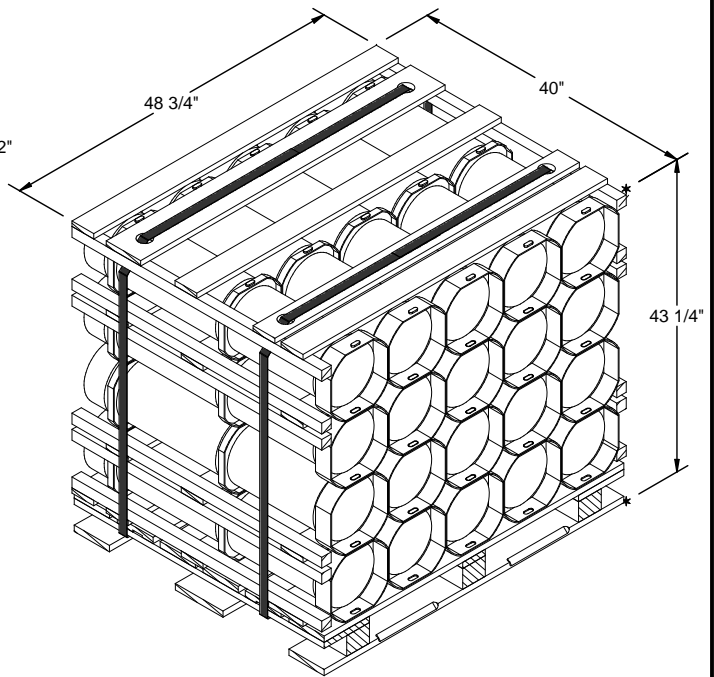
LOAD AS SHOWN

ITEM	QUANTITY	WEIGHT (APPROX)
PALLET UNIT	7	9,625 LBS
DUNNAGE		599 LBS
CONTAINER		4,700 LBS
TOTAL WEIGHT		14,924 LBS (APPROX)



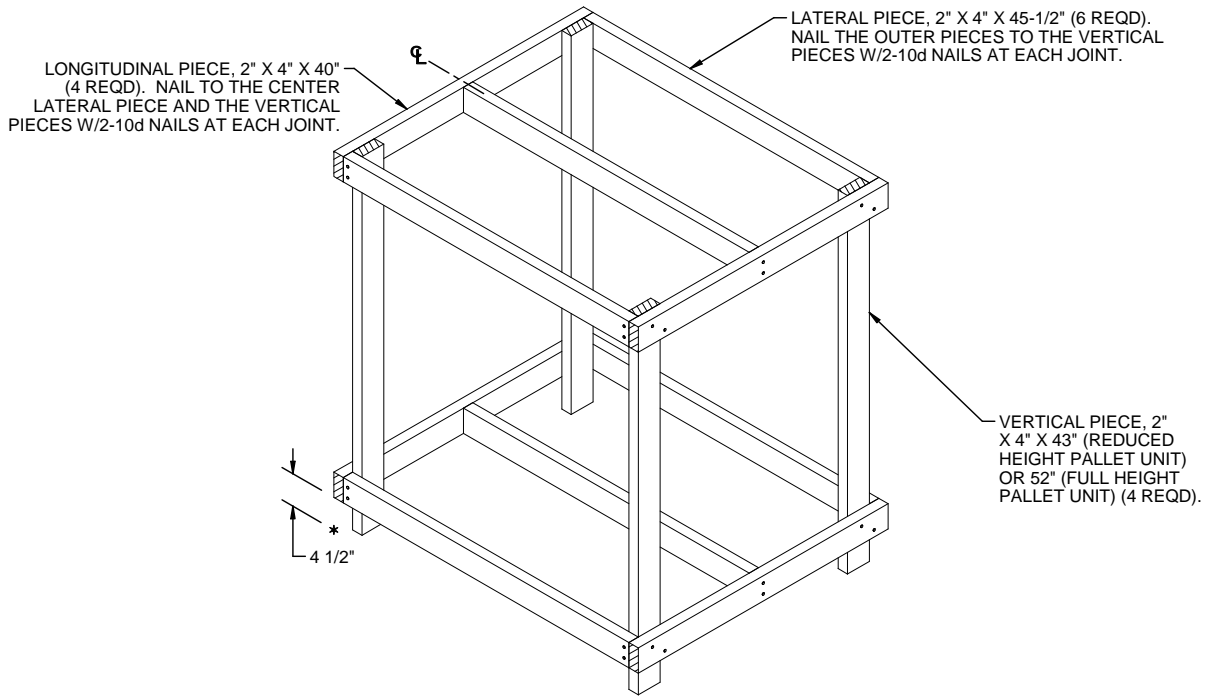
FULL HEIGHT PALLET UNIT

GROSS WEIGHT - - - - - 1,375 LBS (APPROX)
 CUBE - - - - - 48.8 CU FT (APPROX)



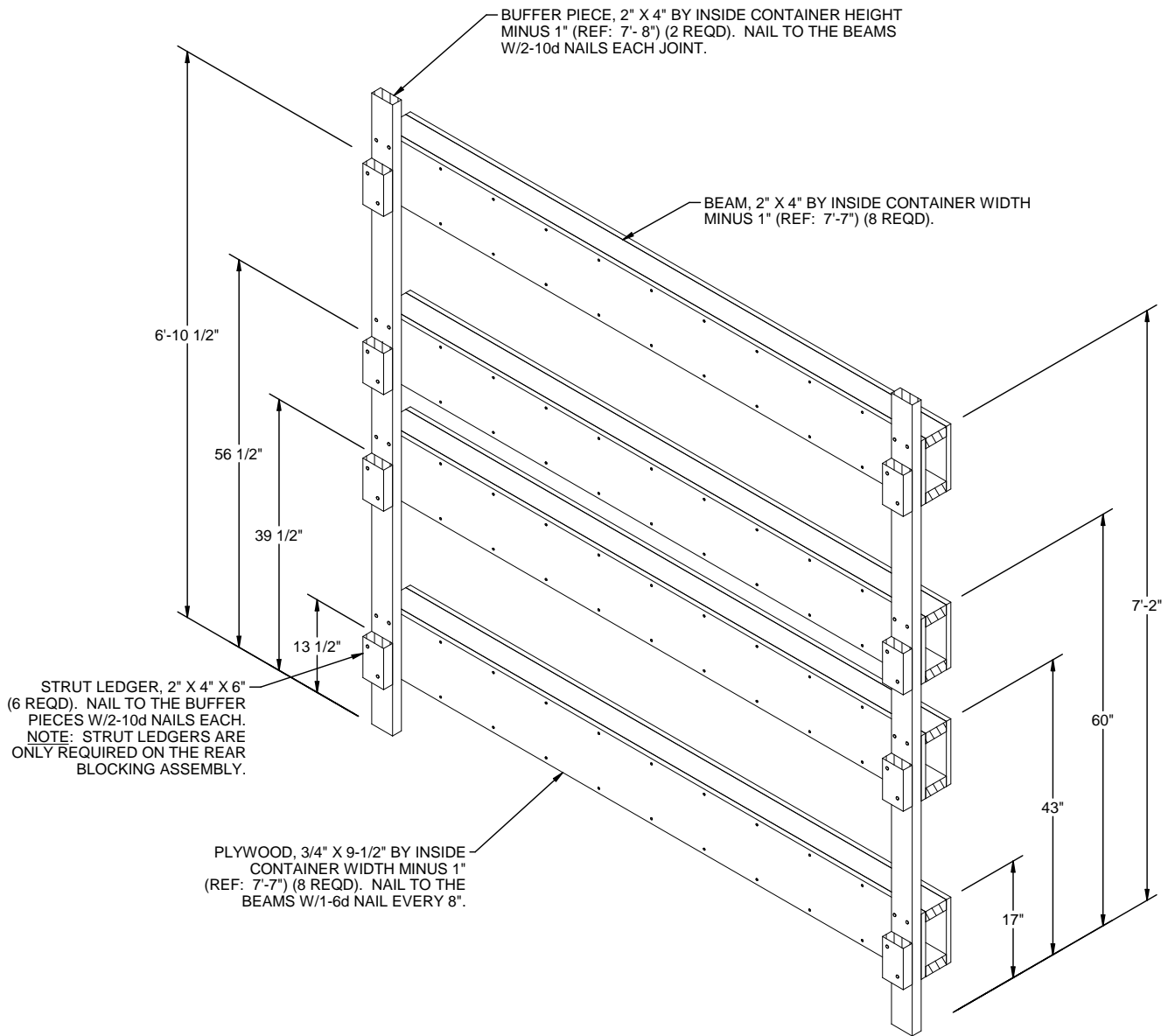
REDUCED HEIGHT PALLET UNIT DETAIL

GROSS WEIGHT - - - - - 1,150 LBS (APPROX)
 CUBE - - - - - 48.8 CU FT (APPROX)



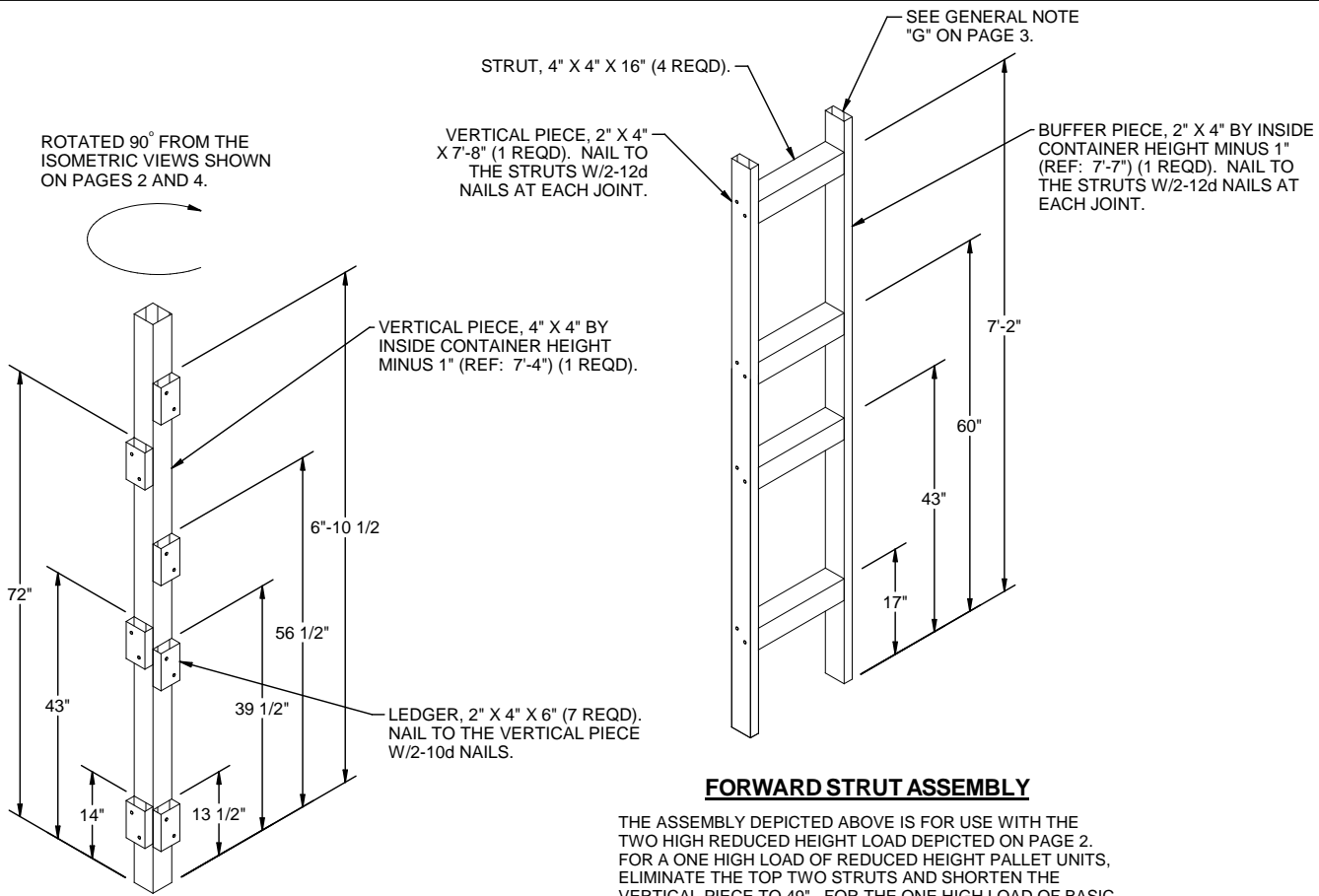
FILLER ASSEMBLY

FOR MINUS ONE PALLET UNIT



FORWARD/REAR BLOCKING ASSEMBLY

THE ASSEMBLY DEPICTED ABOVE IS FOR USE WITH THE TWO HIGH REDUCED HEIGHT LOAD DEPICTED ON PAGE 2. FOR A ONE HIGH LOAD OF REDUCED HEIGHT PALLET UNITS, ELIMINATE THE TOP TWO BOX BEAM ASSEMBLIES AND THE TOP FOUR STRUT LEDGERS (WHERE APPLICABLE). FOR THE ONE HIGH LOAD OF BASIC HEIGHT PALLET UNITS DEPICTED ON PAGE 4, ELIMINATE THE TOP TWO BOX BEAM ASSEMBLIES AND THE TOP FOUR STRUT LEDGERS (WHERE APPLICABLE), AND REPOSITION THE UPPERMOST BOX BEAM ASSEMBLY AT 52" AND THE TWO UPPERMOST STRUT LEDGERS (WHERE APPLICABLE) AT 48-1/2".

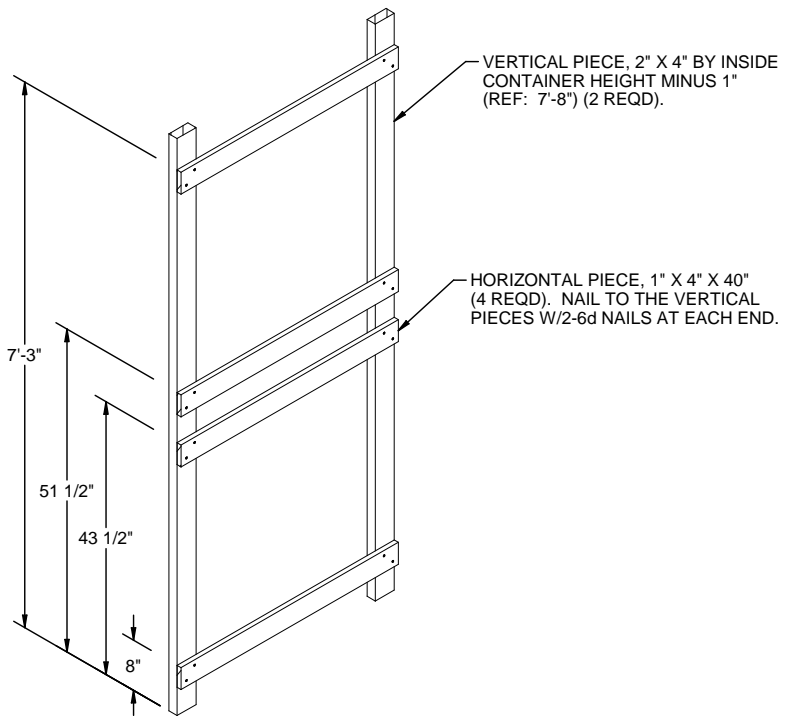


FORWARD STRUT ASSEMBLY

THE ASSEMBLY DEPICTED ABOVE IS FOR USE WITH THE TWO HIGH REDUCED HEIGHT LOAD DEPICTED ON PAGE 2. FOR A ONE HIGH LOAD OF REDUCED HEIGHT PALLET UNITS, ELIMINATE THE TOP TWO STRUTS AND SHORTEN THE VERTICAL PIECE TO 49". FOR THE ONE HIGH LOAD OF BASIC HEIGHT PALLET UNITS DEPICTED ON PAGE 4, ELIMINATE THE TOP TWO STRUTS, REPOSITION THE UPPERMOST STRUT AT 52" AND SHORTEN THE VERTICAL PIECE TO 58".

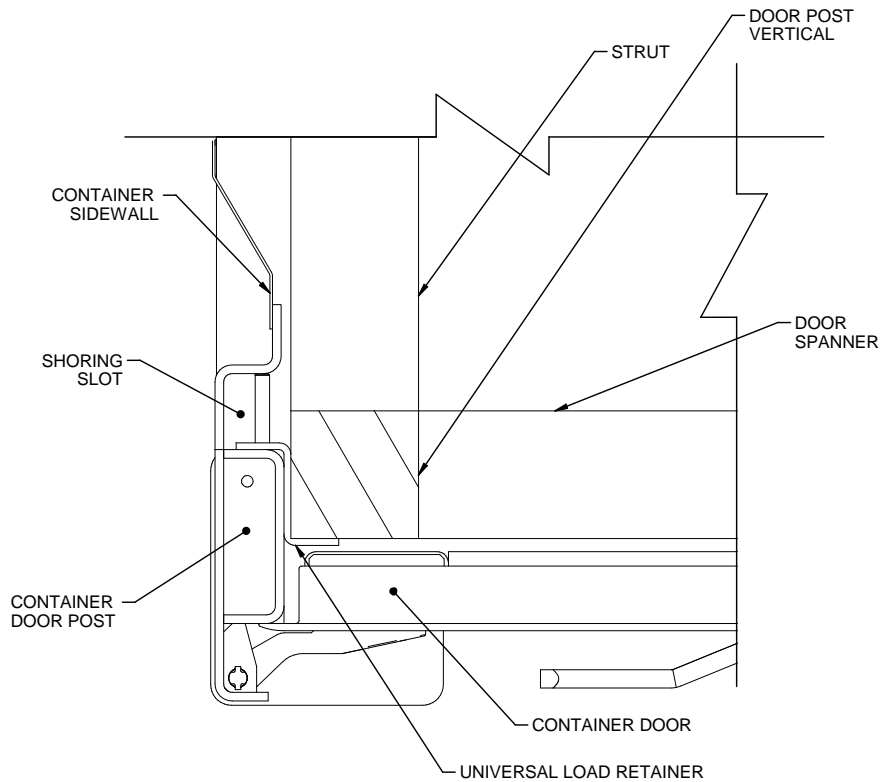
DOOR POST VERTICAL

A LEFT-HAND ASSEMBLY IS DEPICTED ABOVE, A RIGHT-HAND ASSEMBLY IS ALSO REQUIRED. THE ASSEMBLY DEPICTED ABOVE IS FOR USE WITH THE TWO HIGH REDUCED HEIGHT LOAD DEPICTED ON PAGE 2. FOR A ONE HIGH LOAD OF REDUCED HEIGHT PALLET UNITS, ELIMINATE THE TOP TWO STRUT LEDGERS AND THE TOP DOOR SPANNER LEDGER. FOR THE ONE HIGH LOAD OF BASIC HEIGHT PALLET UNITS DEPICTED ON PAGE 4, ELIMINATE THE TOP TWO STRUT LEDGERS AND THE TOP DOOR SPANNER LEDGER.



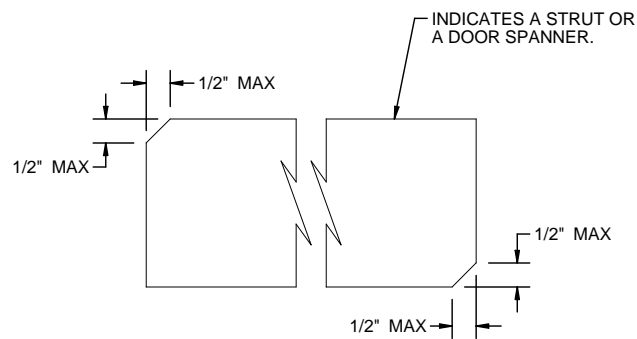
SIDE FILL ASSEMBLY

THE ASSEMBLY DEPICTED ABOVE IS FOR USE WITH THE TWO HIGH REDUCED HEIGHT LOAD DEPICTED ON PAGE 2. FOR A ONE HIGH LOAD OF REDUCED HEIGHT PALLET UNITS, ELIMINATE THE TOP TWO HORIZONTAL PIECES. FOR THE ONE HIGH LOAD OF BASIC HEIGHT PALLET UNITS DEPICTED ON PAGE 4, ELIMINATE THE TOP TWO HORIZONTAL PIECES AND REPOSITION THE UPPERMOST HORIZONTAL PIECE AT 45".



DETAIL A

A PARTIAL PLAN VIEW OF THE LEFT REAR PORTION OF THE CONTAINER ON PAGE 2 OR 4 IS SHOWN DEPICTING THE PROPER POSITION OF THE DOOR POST VERTICAL, UNIVERSAL LOAD RETAINER, AND ADJACENT DUNNAGE PIECES.



BEVEL CUT

IF DESIRED, EACH END OF A STRUT OR DOOR SPANNER MAY BE BEVEL-CUT AS SHOWN ABOVE TO FACILITATE THE ACHIEVEMENT OF A TIGHT END OF LOAD FIT.