

APPROVED BY
 BUREAU OF EXPLOSIVES
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 DATE *2/14/67*

LOADING & BRACING (TL & LTL) IN CLOSED OR OPEN TOP VAN TRAILERS OF DEMOLITION KIT, PROJECTED CHARGE, MI73, AND PROJECTED CHARGE, PRACTICE, MI74 (PALLETIZED)

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DO NOT SCALE

GENERAL NOTES

- A. THE LOAD AS SHOWN ON PAGES 4 AND 5 IS BASED ON A CLOSED OR OPEN TOP VAN TRAILER 38'-0" LONG X 7'-6" WIDE (INSIDE DIMENSION) WITH A WOOD OR A WOOD AND METAL OR A METAL FLOOR. A WIDER OR NARROWER TRAILER AND/OR A LONGER TRAILER THAN SHOWN CAN BE USED FOR SHIPPING THE DEPICTED LOAD. FOUR (4) DEMOLITION KITS CAN BE OUTLOADED IN A TRAILER WHICH IS 26'-0" LONG OR GREATER.
- B. DETAILS OF DEMOLITION KIT PALLET UNIT:
PALLET UNIT DIMENSIONS ---- 145-1/4" LONG X 57-3/8" WIDE X 35-3/4" HIGH.
PALLET UNIT GROSS WEIGHT -- 3,400 POUNDS (APPROX).
PALLET UNIT CUBE ----- 172.1 CUBIC FEET.
- C. THE PROJECTED CHARGE, M173, IS CLASSIFIED AS ICC NOMENCLATURE "EXPLOSIVE MINE"; ICC CLASS A EXPLOSIVE. THE PROJECTED CHARGE, PRACTICE, M174, IS CLASSIFIED AS ICC NOMENCLATURE "JET THRUST UNIT, CLASS B EXPLOSIVE".
- D. GROSS WEIGHT AND AXLE DISTRIBUTION OF WEIGHT WILL BE THE RESPONSIBILITY OF THE CARRIER. THE CARRIER WILL ADVISE THE SHIPPER OF THE APPLICABLE LOADING REQUIREMENTS AND THE SHIPPER WILL LOAD ACCORDINGLY.
- E. THE NUMBER OF LADING UNITS MAY BE ADJUSTED TO FIT THE SIZE OF THE TRAILER BEING LOADED OR THE QUANTITY TO BE SHIPPED. HOWEVER, THE APPROVED METHODS SPECIFIED HEREIN FOR BLOCKING AND BRACING FULL OR PARTIAL LOADS OF THE DESIGNATED ITEM MUST BE FOLLOWED AS CLOSELY AS POSSIBLE.
- F. EXCEPT FOR THE STEEL STRAPPING USED FOR SECURING THE DEMOLITION KIT TO THE PALLET, A MINIMUM OF TWO (2) SEALS WITH TWO (2) CRIMPS PER SEAL MUST BE USED TO SEAL AN END-OVER-END LAP JOINT MADE WITH 1-1/4" X .035" STRAPPING.
- G. DUNNAGE LUMBER SPECIFIED THROUGHOUT THIS PROCEDURAL DRAWING IS OF NOMINAL SIZE. FOR EXAMPLE, 2" X 4" MATERIAL IS ACTUALLY 1-5/8" THICK BY 3-5/8" WIDE AND 4" X 4" MATERIAL IS ACTUALLY 3-5/8" THICK BY 3-5/8" WIDE.
- H. PORTIONS OF THE SEMITRAILER BODY DEPICTED WITHIN THIS PROCEDURAL DRAWING, SUCH AS SIDE WALLS, HAVE NOT BEEN SHOWN IN THE LOAD VIEWS FOR CLARITY PURPOSES.

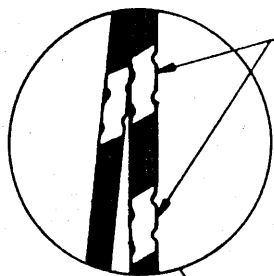
UNITIZING AND HANDLING PROCEDURAL GUIDANCE
(CONTINUED FROM BOTTOM OF PAGE 3)

SUFFICIENT SIX (6) INCH WIDE MATERIAL SHOULD BE POSITIONED ON EDGE ON TOP OF THE BASE END OF THE FORK TINES TO PROVIDE CLEARANCE BETWEEN THE FORKLIFT PACKAGE GUARD AND THE DEMOLITION KIT. THE REAR END OF THE KIT(S) CAN THEN BE RAISED SLIGHTLY AND PUSHED INTO THE TRAILER INTO THE FINAL POSITION.

- c. POSITIONING OF KITS AT THE REAR OF THE TRAILER CAN BE ACCOMPLISHED WITH A CRANE IN LIEU OF A FORKLIFT TRUCK. THE WIRE ROPE SLING USED FOR LIFTING SHOULD PASS UNDER THE PALLET SKID AT SUCH A LOCATION THAT IT WILL BE IN CONTACT WITH THE EXTERIOR END OF THE FOUR ENDWARD RUBBING STRIPS IN ORDER TO PROVIDE FOR A STABLE LIFTING OPERATION. IF LIFTING A TWO-HIGH STACK THE WIRE ROPE SLING MUST BE POSITIONED UNDER THE PALLET SKID OF THE LOWER DEMOLITION KIT. BURLAP OR COMPARABLE CUSHIONING MATERIAL MUST BE POSITIONED BETWEEN THE CONTACTING LIFTING SLING LEG AND THE SIDE OF THE DEMOLITION KIT TO PREVENT CHAFING. FINAL POSITIONING OF THE KITS WILL BE ACCOMPLISHED AS DELINEATED ABOVE. IF ALL THE HANDLING IS ACCOMPLISHED WITH A CRANE THE SHIPMENT MUST BE MADE IN AN OPEN TOP VAN TRAILER.

MATERIAL SPECIFICATIONS

- LUMBER-----: SEE TM 715-5500-1; DUNNAGE LUMBER, FED SPEC MM-L-751 AND ML-STD-731.
- NAILS-----: COMMON, CEMENT COATED OR CHEMICALLY ETCHED, FED SPEC FF-N-105.
ALT: ANNULAR-RING TYPE NAIL OF SAME SIZE.
- STRAPPING, STEEL: TYPE I OR IV, CLASS B FOR PALLETIZING AND CLASS A, B, OR C FOR OUTLOADING, FED SPEC QQ-5-781.



SEALS FOR 1-1/4" X .035" STEEL STRAPPING (2 REQD PER UNITIZING STRAP). DOUBLE CRIMP EACH SEAL AS SHOWN.

LONG STRAPPING (5 PER PALLET).

67 1/2"

PALLET SKID

57 1/2"

SHORT STRAPPING (5 PER PALLET).

FORWARD PALLET RUBBING STRIP

1-1/4" X .035" UNITIZING STEEL STRAPPING (2 REQD PER STACK).

FORKLIFT ENTRY AREA

12-1/4"

STACK DETAIL

UNITIZING AND HANDLING PROCEDURAL GUIDANCE

1. STACKING DEMOLITION KITS FOR UNITIZING.

- a. A DEMOLITION KIT MUST BE LIFTED FROM THE SIDE AND POSITIONED WITH THE FORWARD END EXTENDING AS FAR AS POSSIBLE INTO THE REAR OF THE TRAILER. THE DEMOLITION KIT MAY THEN BE SET DOWN WITH THE FORWARD END RESTING ON THE TRAILER FLOOR AND THE REAR END RESTING ON A TRAILER BED HEIGHT LOADING DOCK. DUNNAGE MATERIAL MAY BE PLACED UNDER THE REAR RUBBING STRIP TO OBTAIN A LEVEL STACK. IF THE OUTLOADING OPERATION DOES NOT TAKE PLACE WHERE THERE IS A LOADING DOCK, THE REAR END OF THE DEMOLITION KIT MUST BE SUPPORTED WITH ANOTHER FORK LIFT.
- b. THE UPPER LAYER DEMOLITION KIT MUST BE PLACED AS CLOSE AS POSSIBLE IN VERTICAL ALIGNMENT WITH THE LOWER DEMOLITION KIT, WITH FORWARD END ABOVE FORWARD END.

2. INSTALLATION OF 1-1/4" X .035" UNITIZING STEEL STRAPPING.

- a. THE UNITIZING STRAPS SHOULD BE POSITIONED AROUND THE DEMOLITION KITS AS SHOWN, EACH CENTERED ON TOP OF A 4" X 4" STRAPPING BATTEN ON THE UPPER LAYER DEMOLITION KIT AND EXTENDING UNDER THE SKID OF THE LOWER PALLET. THE STRAPPING SHOULD LAY VERTICAL ALONG THE SIDES AND STRAIGHT ACROSS THE TOP AND BOTTOM OF THE STACK.

UNITIZING AND HANDLING PROCEDURAL GUIDANCE (CONTINUED)

- b. STRAPPING WILL BE FIRMLY TENSIONED, AND THE END-OVER-END LAP JOINT WILL BE SEALED WITH TWO DOUBLE-CRIMPED STRAP SEALS AS SHOWN. THE LAP JOINT WILL BE MADE ALONG THE SIDE OF THE STACK. DURING STRAP TENSIONING, CARE SHOULD BE EXERCISED TO INSURE THAT THE DEMOLITION KITS ARE NOT DAMAGED (EXCESS TENSION MAY DAMAGE THE COVER SEAL). EXCESS STRAPPING (LOOSE ENDS) SHOULD BE CUT OFF OR BROKEN OFF NEAR THE JOINT SEALS.

3. DEMOLITION KIT HANDLING.

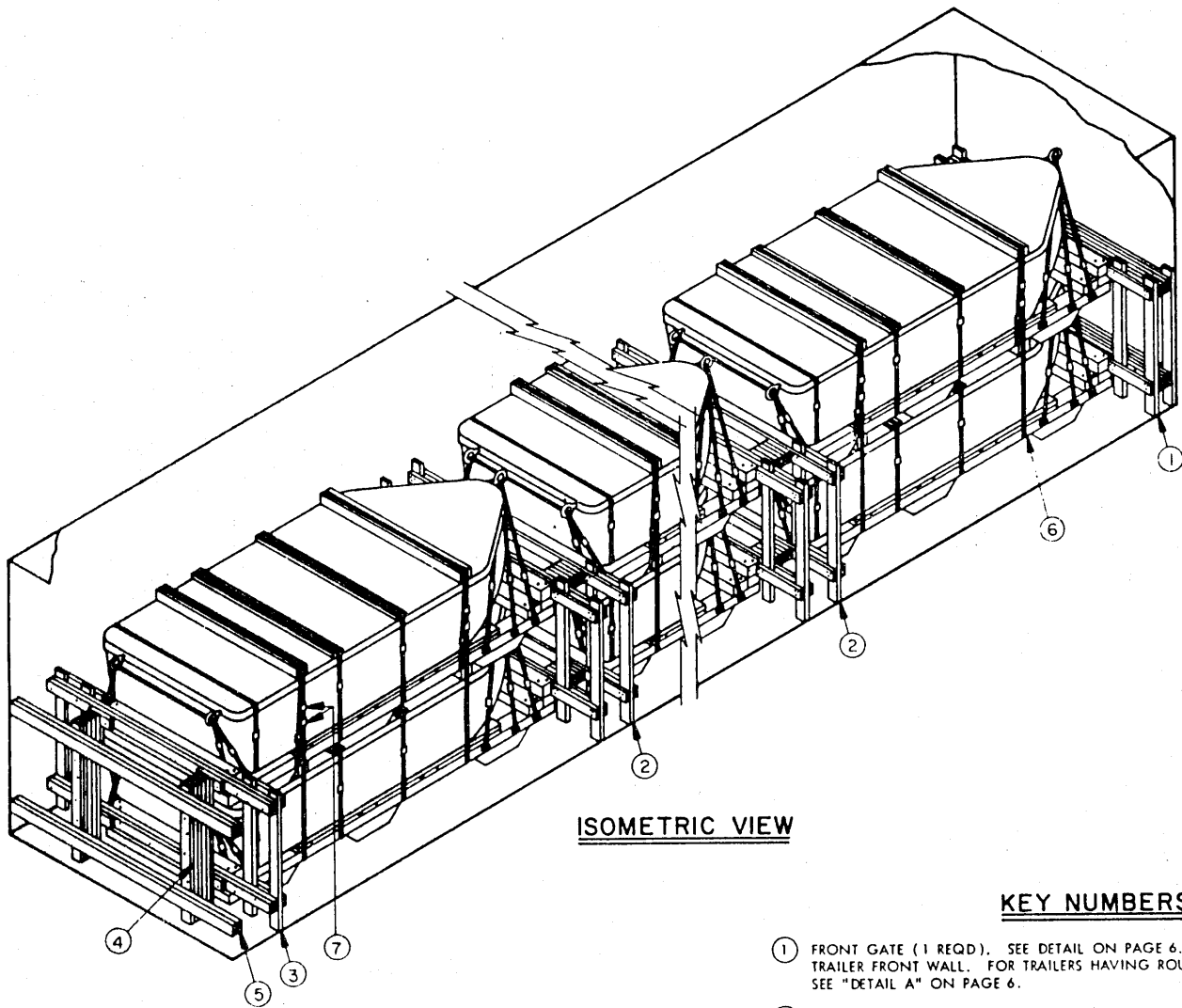
NOTES: (1) APPROVED MATERIALS HANDLING EQUIPMENT (FORKLIFT TRUCKS, CRANES, HAND TRUCKS, DOLLIES, ROLLER ASSEMBLIES, SLINGS, SPREADER BARS, ETC.) IS SPECIFIED ELSEWHERE.

(2) PRECAUTIONARY HANDLING TECHNIQUES NORMALLY EMPLOYED OR AS SPECIFIED FOR THE TYPE OF COMMODITY INVOLVED WILL BE OBSERVED.

- a. ONLY APPROVED AND APPROPRIATELY SIZED MATERIALS HANDLING EQUIPMENT WILL BE USED FOR HANDLING THE DEPICTED DEMOLITION KITS.
- b. IF HANDLING IS ACCOMPLISHED WITH A FORKLIFT TRUCK, THE TRUCK MUST HAVE A CAPACITY OF AT LEAST 4,000 POUNDS AND MUST BE EQUIPPED WITH FORKS AT LEAST 54" IN LENGTH. FORK EXTENSIONS MAY BE USED TO OBTAIN THIS 54" MINIMUM LENGTH. THE DEMOLITION KITS MUST BE HANDLED FROM A SIDE POSITION WITH THE FORKS SPANNING THE CENTER RUBBING STRIP. CARE MUST BE EXERCISED WHEN INSERTING FORKS UNDER A DEMOLITION KIT, TO PREVENT DAMAGE TO THE KIT BY THE FORK TINES OR THE FORKLIFT PACKAGE GUARD. NOTE: THE DEMOLITION KITS MUST NOT BE LIFTED BY INSERTING THE FORKS IN THE VOID BETWEEN A PALLET SKID AND THE 2" X 4" X 80-1/2" SIDE BLOCKING PIECE FOR THE KIT. ONE DEMOLITION KIT OR A TWO-HIGH STACK WILL BE HANDLED IN LOADING THE VAN BY INSERTING THE FORKS OF THE FORKLIFT TRUCK BETWEEN THE RUBBING STRIPS AT THE END OF THE PALLET.

(CONTINUED ON PAGE 2)

UNITIZING AND HANDLING PROCEDURES



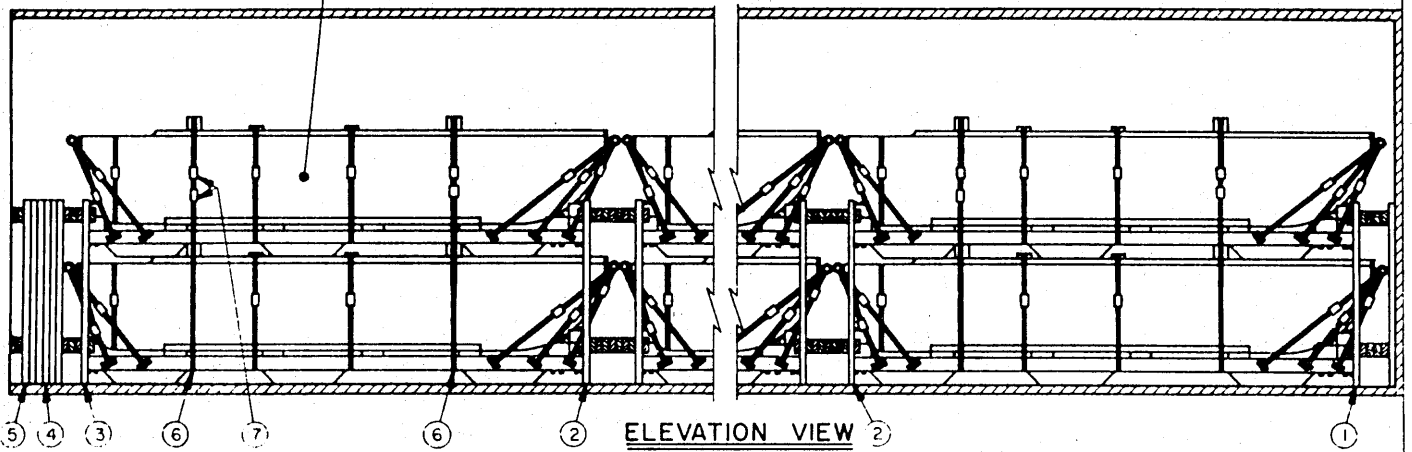
ISOMETRIC VIEW

KEY NUMBERS

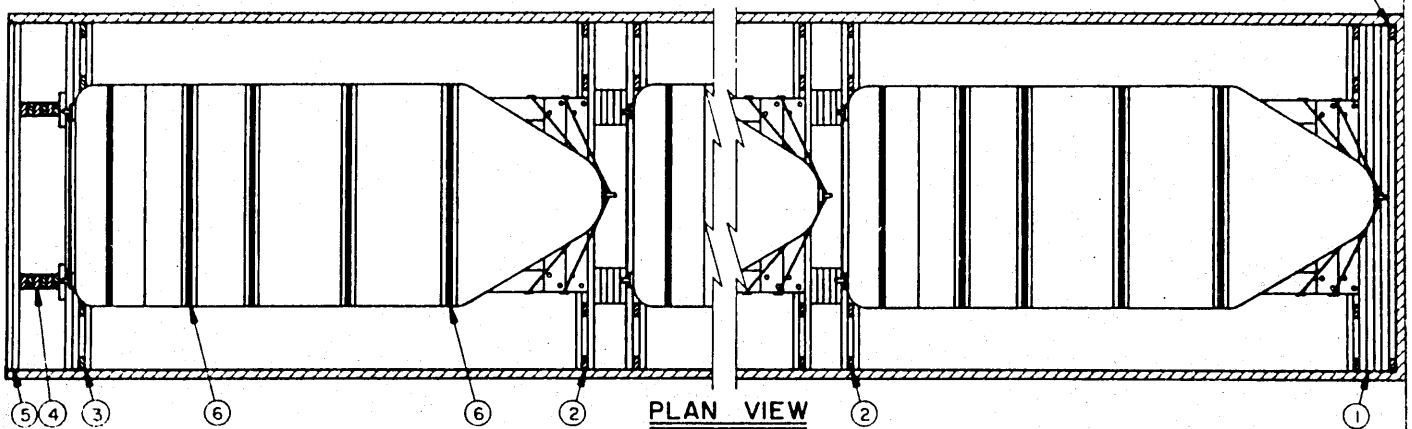
- ① FRONT GATE (1 REQD). SEE DETAIL ON PAGE 6. INSTALL AGAINST THE TRAILER FRONT WALL. FOR TRAILERS HAVING ROUNDED FRONT CORNERS SEE "DETAIL A" ON PAGE 6.
- ② INTERMEDIATE GATE (2 REQD). SEE DETAIL ON PAGE 6. INSTALL BETWEEN LONGITUDINALLY ADJACENT STACKS.
- ③ REAR-OF-LOAD GATE (1, REQD). SEE DETAIL ON PAGE 7. INSTALL AGAINST REAR STACK.
- ④ SOLID FILL, 4" WIDE X 48" LONG X THICKNESS REQUIRED TO FILL THE VOID BETWEEN THE VERTICAL PIECES OF REAR-OF-LOAD GATE AND REAR GATE (9" MAX). LAMINATE FIRST PIECE TO VERTICAL PIECE OF REAR-OF-LOAD GATE W/4-10d NAILS. LAMINATE EACH ADDITIONAL PIECE IN A LIKE MANNER. FOR AN EXCESS SPACE OF MORE THAN 9" BETWEEN GATES, REFER TO "REAR STRUT BLOCKING" DETAIL ON PAGE 12.
- ⑤ REAR GATE (1 REQD). SEE DETAIL ON PAGE 7. NAIL TO PIECES MARKED ④ W/4-10d NAILS AT EACH JOINT.
- ⑥ STEEL STRAPPING, 1-1/4" X .035" X 22'-0" (6 REQD). SEE "UNITIZING AND HANDLING PROCEDURAL GUIDANCE" ON PAGE 3.
- ⑦ SEAL FOR 1-1/4" STRAP (12 REQD). DOUBLE CRIMP.

TL (6-UNIT LOAD)

SEE "FILLER ASSEMBLY" AND "APPLICATION OF FILLER ASSEMBLY" DETAILS ON PAGE 8 FOR PROCEDURES WHICH ARE APPLICABLE IF A DEMOLITION KIT IS OMITTED FROM THE TOP LAYER.



SEE "DETAIL A" ON PAGE 6 FOR FRONT GATE CONSTRUCTION DETAILS WHEN SHIPPING IN A TRAILER HAVING ROUNDED FRONT CORNERS.



BILL OF MATERIAL

LUMBER	LINEAR FEET	BOARD FEET
2" X 4"	419	280
NAIL	NO. REQD	POUNDS
10d (3")	606	9-1/2
STEEL STRAPPING, 1-1/4" X .035"	132' REQD	19 LBS
SEAL FOR 1-1/4" STRAP	12 REQD	1 LB

LOAD AS SHOWN

ITEM	QUANTITY	WEIGHT (APPROX)
DEMOLITION KIT	6	20,400 LBS
DUNNAGE		730 LBS
TOTAL WEIGHT		21,130 LBS

TL (6-UNIT LOAD)

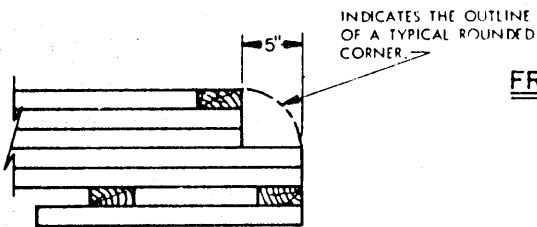
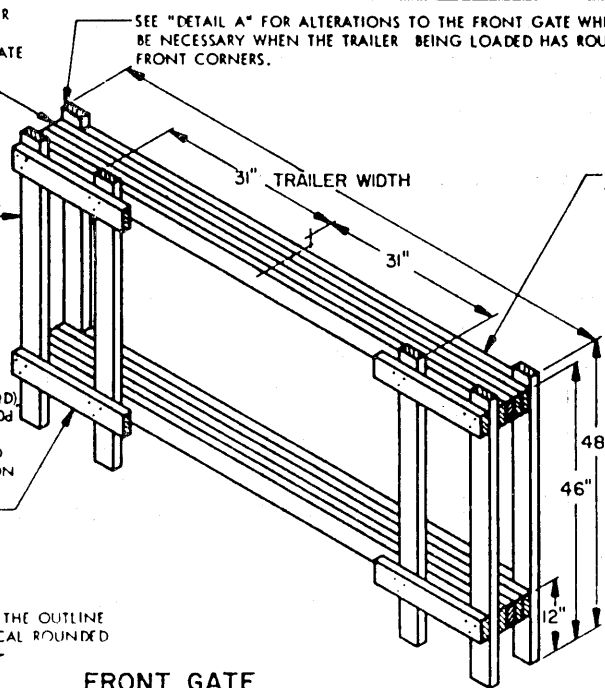
LATERAL PIECE, 2" X 4" X TRAILER WIDTH (CUT TO FIT) (8 REQD, 4 AT EACH LOCATION). LAMINATE W/1-10d NAIL EVERY 12".

SEE "DETAIL A" FOR ALTERATIONS TO THE FRONT GATE WHICH WILL BE NECESSARY WHEN THE TRAILER BEING LOADED HAS ROUNDED FRONT CORNERS.

VERTICAL PIECE, 2" X 4" X 48" (6 REQD). NAIL TO LATERAL PIECES W/3-10d NAILS AT EACH JOINT.

SIDE BLOCKING PIECE, 2" X 4" X CUT TO FIT BETWEEN THE TRAILER SIDE WALL AND THE LADING PALLET (4 REQD). NAIL TO VERTICAL PIECES W/3-10d NAILS AT EACH JOINT. NOTE: THESE PIECES SHOULD BE APPLIED AFTER THE ADJACENT DEMOLITION KITS HAVE BEEN POSITIONED IN THE TRAILER.

FRONT LATERAL PIECE, 2" X 4" X TRAILER WIDTH MINUS 7-1/4" (2 REQD). LAMINATE TO 2" X 4" X TRAILER WIDTH PIECES W/1-10d NAIL EVERY 12".



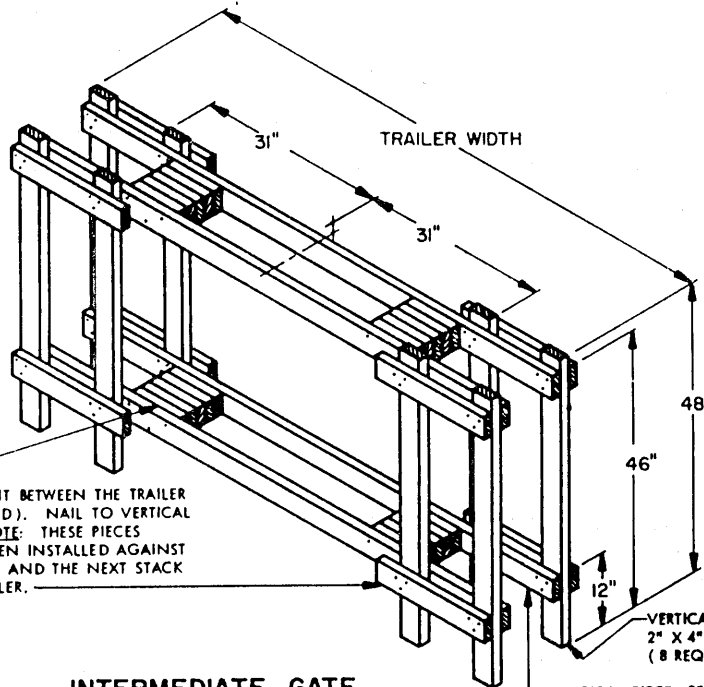
FRONT GATE

DETAIL A

ALTERATIONS TO THE FRONT GATE FOR A 5" CORNER RADIUS ARE SHOWN. LAMINATE AN ADDITIONAL 2" X 4" X LENGTH TO SUIT PIECE TO EACH GATE HORIZONTAL FOR A RADIUS BETWEEN 5" AND 6-1/2", TWO PIECES FOR A RADIUS BETWEEN 6-1/2" AND 8", ETC. NAIL W/1-10d NAIL EVERY 12".

FILLER BLOCK, 2" X 4" X 9" (20 REQD, 5 AT EACH LOCATION). LAMINATE SECOND PIECE TO FIRST W/3-10d NAILS. LAMINATE EACH ADDITIONAL PIECE IN A LIKE MANNER.

SIDE BLOCKING PIECE, 2" X 4" X CUT TO FIT BETWEEN THE TRAILER SIDE WALL AND THE LADING PALLET (8 REQD). NAIL TO VERTICAL PIECES W/3-10d NAILS AT EACH JOINT. NOTE: THESE PIECES SHOULD BE APPLIED AFTER THE GATE HAS BEEN INSTALLED AGAINST THE REAR OF A STACK OF DEMOLITION KITS AND THE NEXT STACK OF KITS HAS BEEN POSITIONED IN THE TRAILER.



VERTICAL PIECE, 2" X 4" X 48" (8 REQD).

LATERAL PIECE, 2" X 4" X TRAILER WIDTH (CUT TO FIT) (4 REQD). NAIL TO VERTICAL PIECES W/3-10d NAILS AT EACH JOINT. NAIL TO FILLER BLOCKS W/3-10d NAILS AT EACH JOINT.

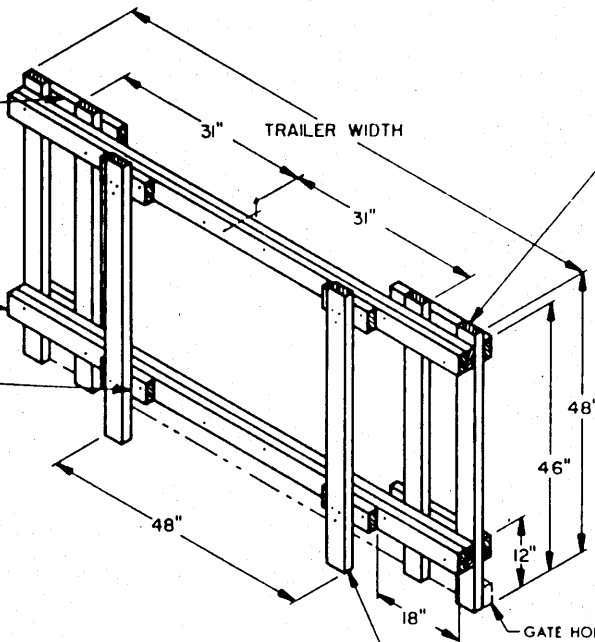
INTERMEDIATE GATE

DETAILS (TL)

SIDE BLOCKING PIECE, 2" X 4" X CUT TO FIT BETWEEN THE TRAILER SIDE WALL AND THE LADING PALLET (4 REQD). NAIL TO VERTICAL PIECES W/3-10d NAILS AT EACH JOINT. NOTE: THESE PIECES SHOULD BE APPLIED AFTER THE LAST STACK OF DEMOLITION KITS HAS BEEN POSITIONED IN THE TRAILER.

LATERAL PIECE, 2" X 4" X TRAILER WIDTH (CUT TO FIT) DOUBLED (2 REQD). LAMINATE W/1-10d NAIL EVERY 12".

FILLER BLOCK, 2" X 4" X 9" (4 REQD). NAIL TO A LATERAL PIECE W/3-10d NAILS.



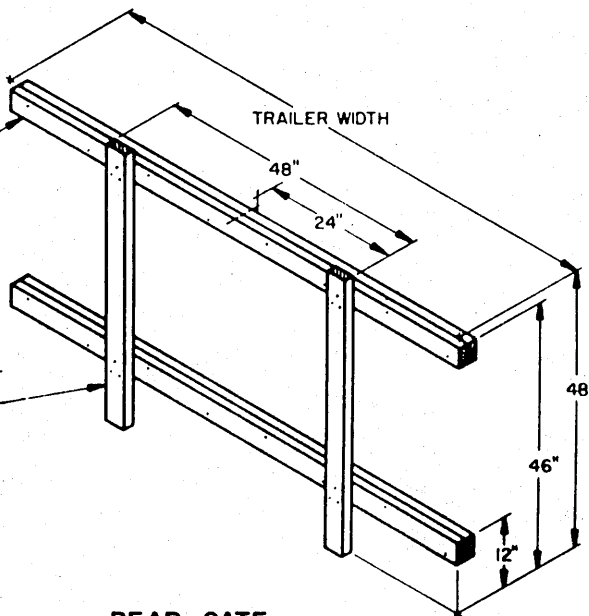
VERTICAL PIECE, 2" X 4" X 48" (4 REQD). NAIL TO LATERAL PIECES W/3-10d NAILS AT EACH JOINT.

GATE HOLD-DOWN, 2" X 4" X TRAILER WIDTH (CUT TO FIT) (1 REQD). NAIL TO VERTICAL PIECES W/3-10d NAILS AT EACH JOINT. CAUTION: THIS PIECE IS ONLY REQUIRED ON THE GATE WHEN USING LTL BRACING PROCEDURES AS SHOWN ON PAGE 9.

REAR-OF-LOAD GATE

REAR VERTICAL, 2" X 4" X 48" (2 REQD). NAIL TO FILLER BLOCKS W/3-10d NAILS AT EACH JOINT. NOTE: WHEN REAR-OF-LOAD GATE IS TO BE USED IN CONJUNCTION WITH THE LTL BRACING PROCEDURE SHOWN ON PAGE 9, EACH OF THESE TWO (2) SHOULD BE OMITTED AND REPLACED BY A PIECE MARKED ① ON THAT PAGE.

LATERAL PIECE, 2" X 4" X TRAILER WIDTH (CUT TO FIT) DOUBLED (2 REQD). LAMINATE W/1-10d NAIL EVERY 12". IF THE TRAILER BEING LOADED IS EQUIPPED WITH REAR CORNER POSTS THESE PIECES WILL BE CUT TO FIT BETWEEN THE CORNER POSTS.

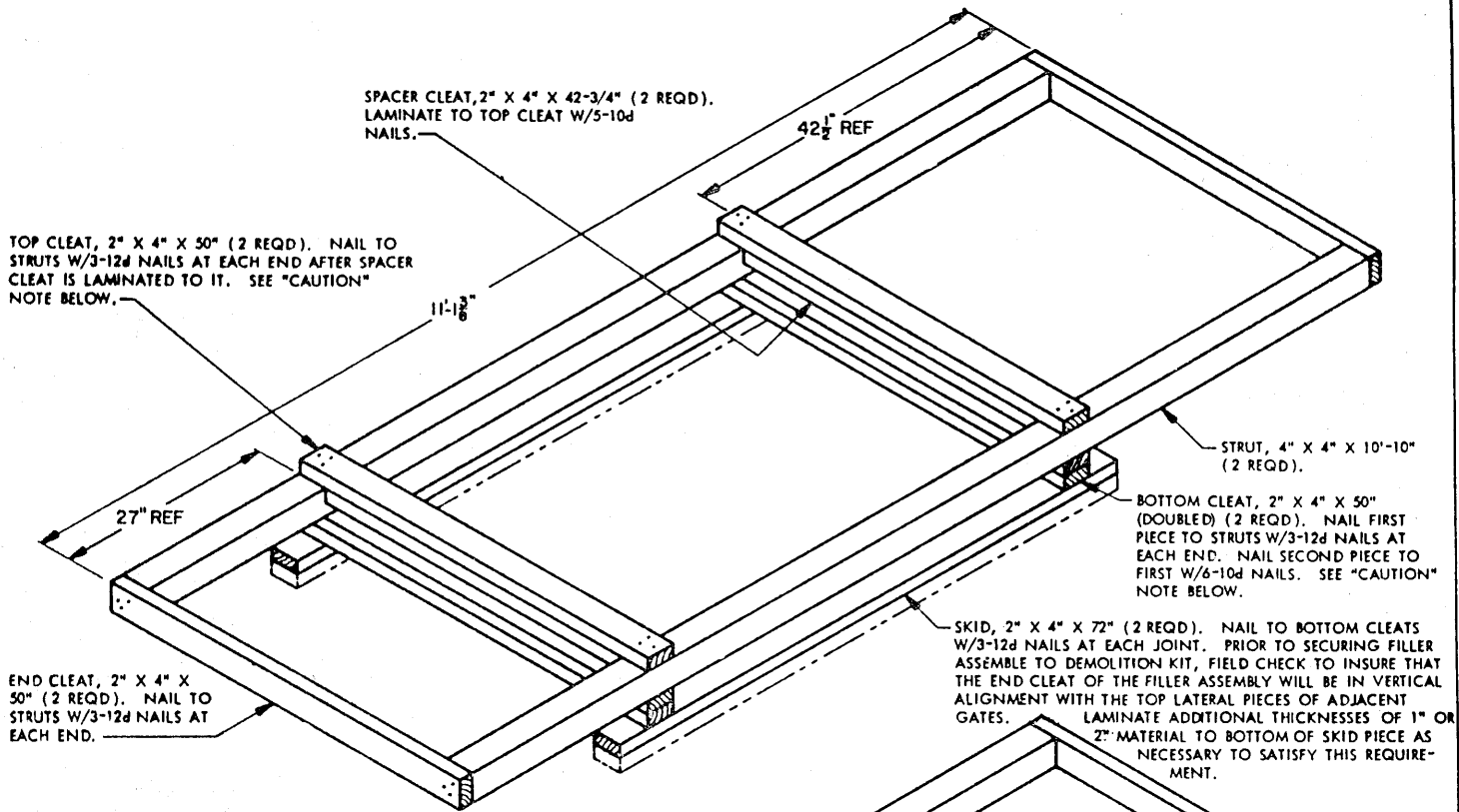


VERTICAL PIECE, 2" X 4" X 48" (2 REQD). NAIL TO LATERAL PIECES W/3-10d NAILS AT EACH JOINT.

REAR GATE

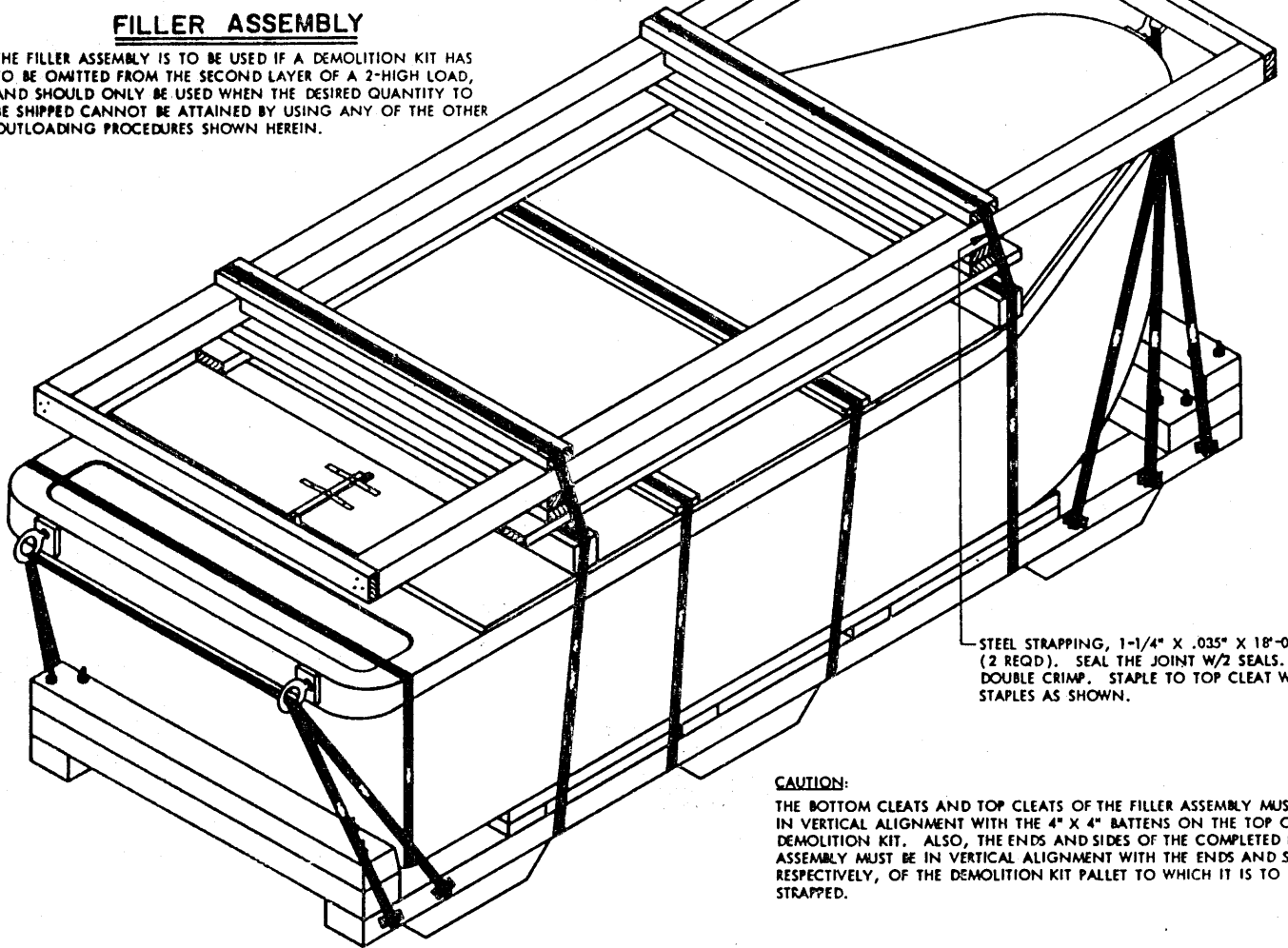
THIS GATE IS SHOWN ROTATED 180° FROM ITS NORMAL INSTALLATION POSITION FOR CLARITY PURPOSES.

DETAILS (TL)



FILLER ASSEMBLY

THE FILLER ASSEMBLY IS TO BE USED IF A DEMOLITION KIT HAS TO BE OMITTED FROM THE SECOND LAYER OF A 2-HIGH LOAD, AND SHOULD ONLY BE USED WHEN THE DESIRED QUANTITY TO BE SHIPPED CANNOT BE ATTAINED BY USING ANY OF THE OTHER UNLOADING PROCEDURES SHOWN HEREIN.

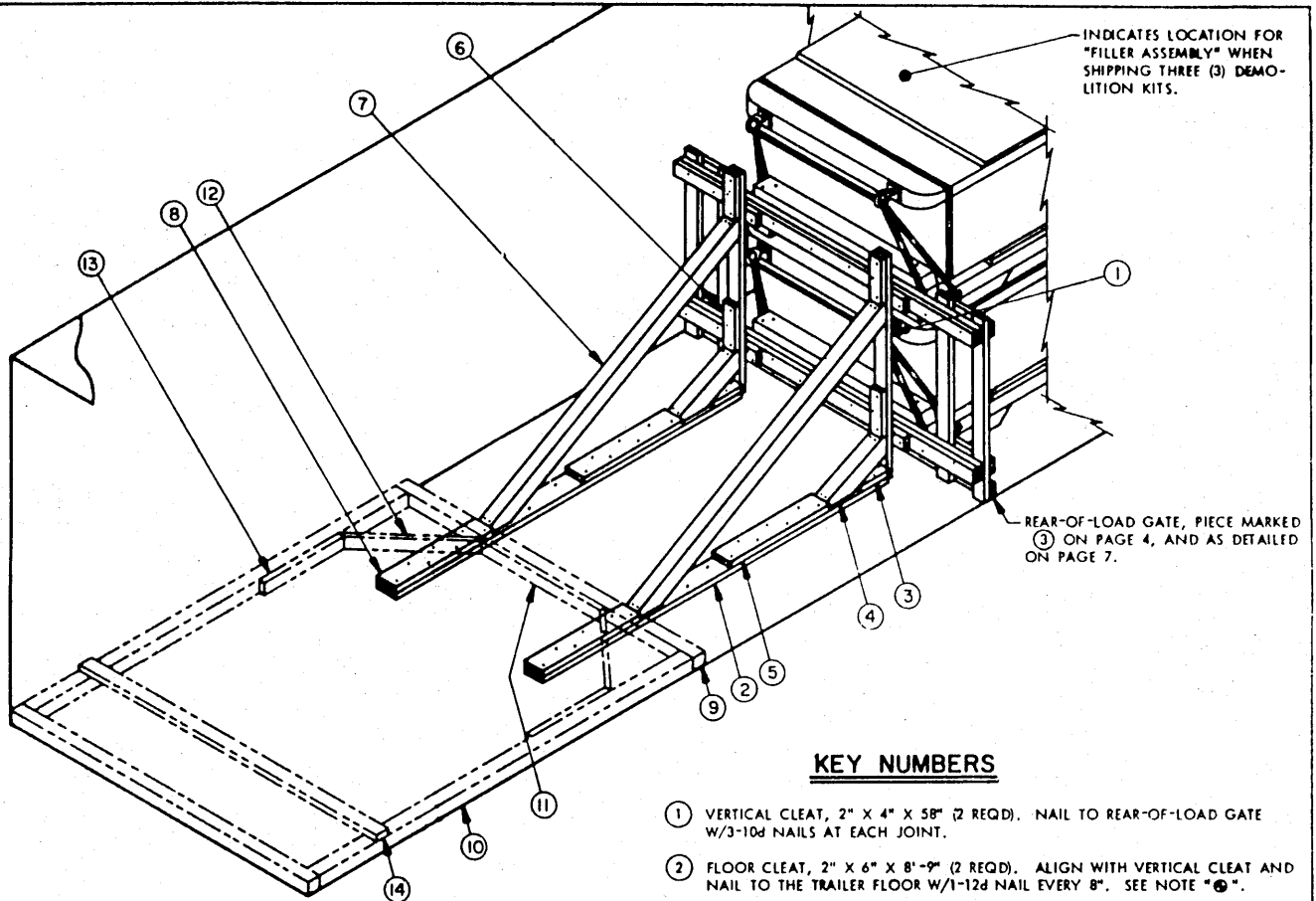


CAUTION:

THE BOTTOM CLEATS AND TOP CLEATS OF THE FILLER ASSEMBLY MUST BE IN VERTICAL ALIGNMENT WITH THE 4" X 4" BATTENS ON THE TOP OF THE DEMOLITION KIT. ALSO, THE ENDS AND SIDES OF THE COMPLETED FILLER ASSEMBLY MUST BE IN VERTICAL ALIGNMENT WITH THE ENDS AND SIDES, RESPECTIVELY, OF THE DEMOLITION KIT PALLET TO WHICH IT IS TO BE STRAPPED.

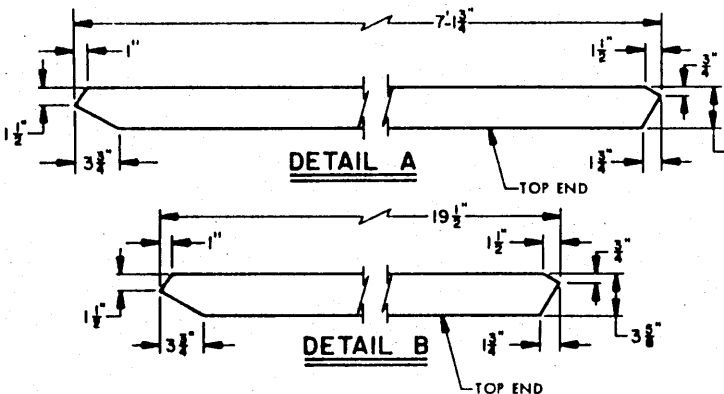
APPLICATION OF FILLER ASSEMBLY

DETAILS (LTL)



LTL BRACING

THIS PROCEDURE IS APPLICABLE TO THE BLOCKING OF THREE (3) OR FOUR (4) DEMOLITION KITS IN A VAN TRAILER WHICH HAS A WOOD, OR A WOOD AND METAL, OR AN ALL METAL FLOOR. FOUR (4) KITS WILL REQUIRE A TRAILER AT LEAST 34'-0" LONG IF THE PROCEDURE APPLICABLE TO A WOOD OR WOOD AND METAL FLOOR IS USED. REFER TO PAGE 12 FOR AN ALTERNATIVE PROCEDURE WHICH WILL PERMIT THE USE OF A TRAILER 27'-0" OR GREATER FOR THE SHIPMENT OF FOUR (4) KITS.



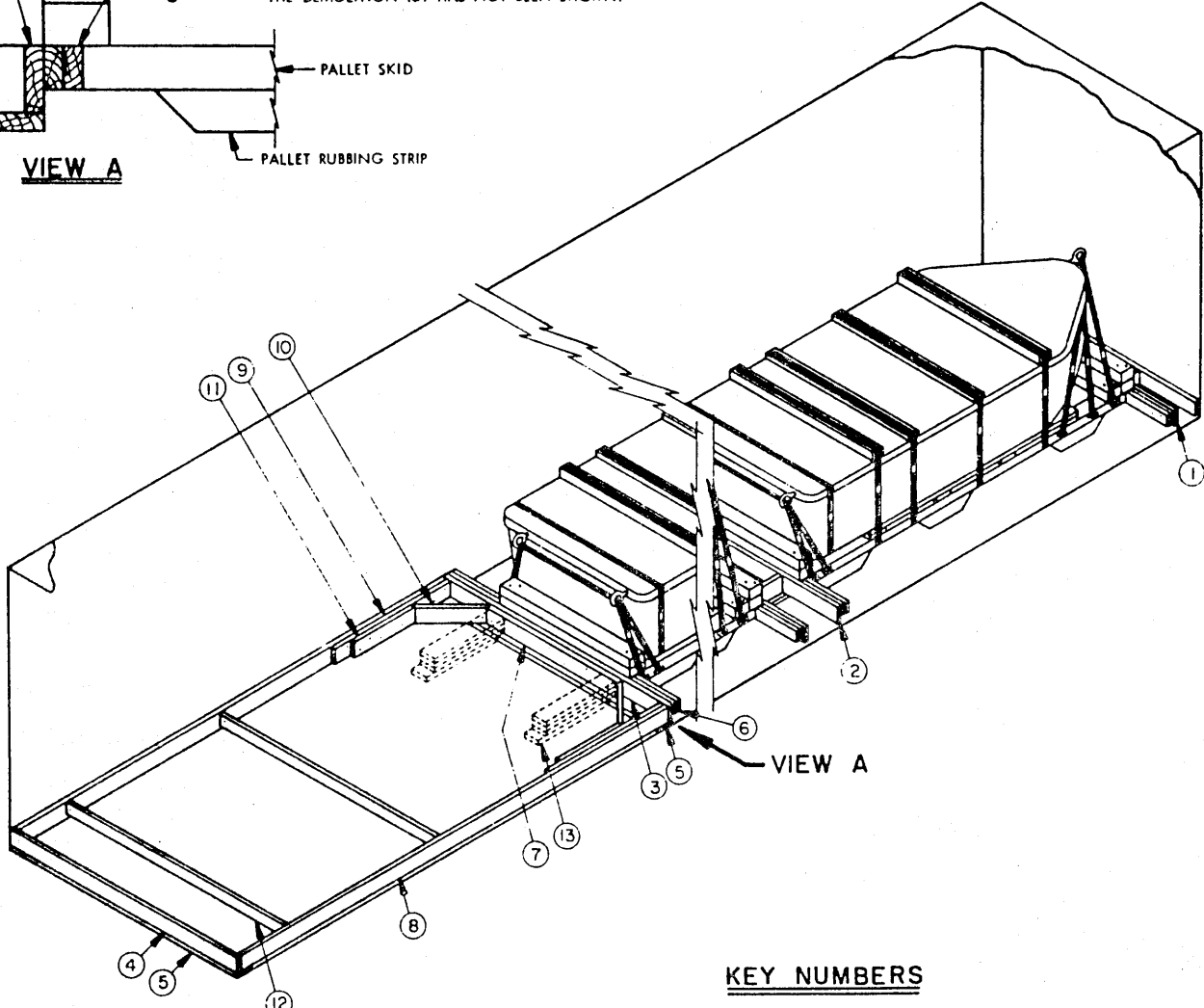
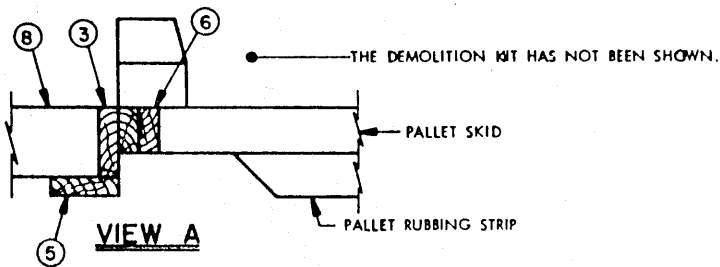
NOTE

WHEN LOADING A TRAILER HAVING AN ALL METAL FLOOR OR IF THE PROCEDURE IS PREFERRED IN A TRAILER HAVING A WOOD OR A WOOD AND METAL FLOOR, SHORTEN FLOOR CLEAT, PIECE MARKED (2), TO 6'-3" AND OMIT THE NAILING, OMIT PIECE MARKED (6) AND INSTALL PIECES MARKED (7) THRU (14).

KEY NUMBERS

- (1) VERTICAL CLEAT, 2" X 4" X 58" (2 REQD). NAIL TO REAR-OF-LOAD GATE W/3-10d NAILS AT EACH JOINT.
- (2) FLOOR CLEAT, 2" X 6" X 8'-9" (2 REQD). ALIGN WITH VERTICAL CLEAT AND NAIL TO THE TRAILER FLOOR W/1-12d NAIL EVERY 8". SEE NOTE "A".
- (3) POCKET CLEAT, 2" X 6" X 8" (2 REQD). NAIL TO FLOOR CLEAT W/2-12d NAILS. TOENAIL TO VERTICAL CLEAT W/2-12d NAILS.
- (4) LOWER KNEE BRACE, 4" X 4" X 19-1/2" (2 REQD). SEE "DETAIL B" FOR BEVEL CUTS REQUIRED. TOENAIL TO VERTICAL CLEAT AND FLOOR CLEAT W/2-16d NAILS AT EACH END.
- (5) BACK-UP CLEAT FOR LOWER KNEE BRACE, 2" X 6" X 30" (2 REQD). NAIL TO FLOOR CLEAT W/7-12d NAILS.
- (6) HOLD-DOWN CLEAT, 2" X 4" X 12" (4 REQD). NAIL TO VERTICAL CLEAT W/3-10d NAILS.
- (7) UPPER KNEE BRACE, 4" X 4" X 7'-1-3/4" (2 REQD). SEE "DETAIL A" FOR BEVEL CUTS REQUIRED. TOENAIL TO VERTICAL CLEAT AND FLOOR CLEAT W/2-16d NAILS AT EACH END.
- (8) BACK-UP CLEAT FOR UPPER KNEE BRACE, 2" X 6" X 30" (2 REQD). NAIL TO FLOOR CLEAT W/7-12d NAILS.
- (9) HEADER, 4" X 4" X TRAILER WIDTH (CUT TO FIT) (2 REQD).
- (10) STRUT, 4" X 4" X CUT TO FIT (2 REQD). TOENAIL TO HEADERS W/2-16d NAILS AT EACH END.
- (11) CENTER CLEAT, 2" X 4" X 36" (1 REQD). NAIL TO HEADER W/7-12d NAILS.
- (12) DIAGONAL BRACE, 2" X 4" X CUT TO FIT (2 REQD). DOUBLE BEVEL EACH END 45°. TOENAIL TO HEADER AND STRUT W/2-16d NAILS AT EACH END.
- (13) BACK-UP CLEAT FOR DIAGONAL BRACE, 2" X 4" X 24" (2 REQD). NAIL TO STRUT W/6-12d NAILS.
- (14) STRUT BRACING, 2" X 4" X TRAILER WIDTH (CUT TO FIT) (MIN 1 REQD). POSITION ONE (1) NEAR THE REAR OF THE TRAILER AND ONE (1) EVERY 7'-0" OF STRUT LENGTH. NAIL TO STRUTS W/3-12d NAILS AT EACH END.

TYPICAL LTL (2-HIGH LOAD)



TYPICAL LTL

THIS PROCEDURE IS APPLICABLE TO THE BLOCKING OF ONE (1), TWO (2) OR THREE (3) DEMOLITION KIT(S) LOADED ONE-HIGH IN A VAN TRAILER WHICH HAS A WOOD OR A WOOD AND METAL OR A METAL FLOOR.

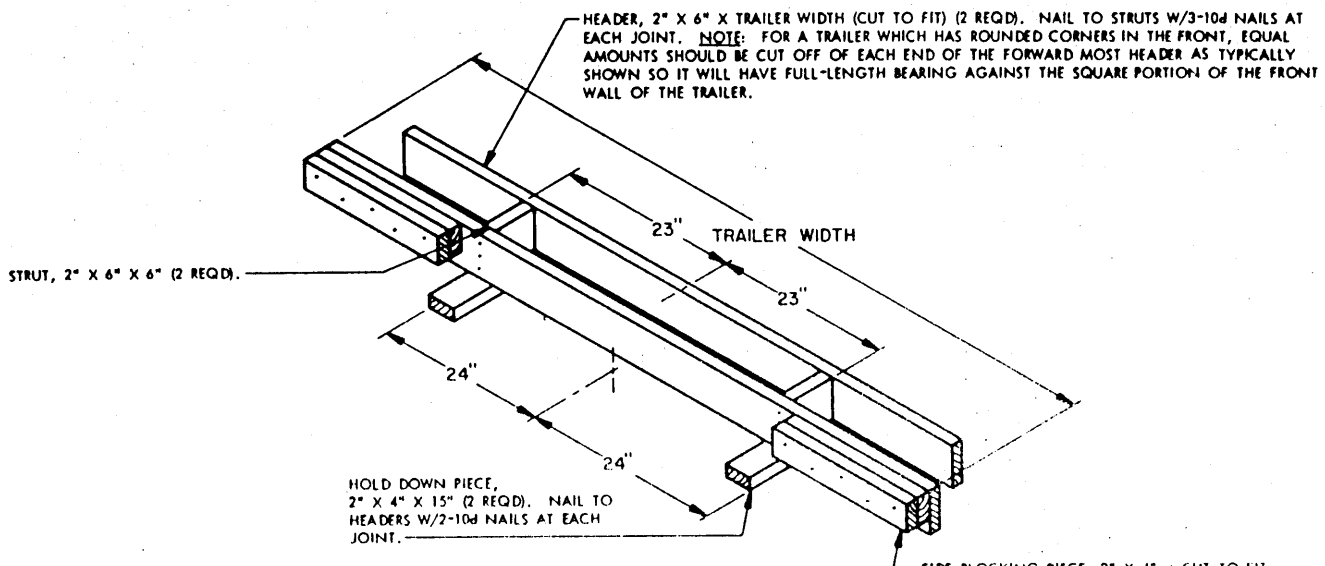
KEY NUMBERS

- ① FORWARD BLOCKING (1 REQD). SEE DETAIL ON PAGE 11. INSTALL AGAINST THE TRAILER FRONT WALL.
- ② INTERMEDIATE BLOCKING (1 REQD). SEE DETAIL ON PAGE 11. INSTALL BETWEEN LONGITUDINALLY ADJACENT PALLET UNITS.
- ③ REAR-OF-LOAD HEADER, 2" X 6" X TRAILER WIDTH (CUT TO FIT) (1 REQD). SEE "VIEW A" DETAIL ABOVE.
- ④ REAR HEADER, 2" X 6" X TRAILER WIDTH (CUT TO FIT) (1 REQD).
- ⑤ STRUT LEDGER, 2" X 6" X TRAILER WIDTH (CUT TO FIT) (2 REQD). NAIL TO REAR-OF-LOAD HEADER AND/OR REAR HEADER W/1-10d NAIL EVERY 12".
- ⑥ SIDE BLOCKING PIECE, 2" X 4" X CUT-TO-FIT BETWEEN THE TRAILER SIDE WALL AND THE LADING PALLET (DOUBLED) (2 REQD). NAIL FIRST PIECE TO THE REAR-OF-LOAD HEADER W/5-10d NAILS. NAIL SECOND PIECE TO THE FIRST IN A LIKE MANNER.
- ⑦ CENTER CLEAT, 2" X 6" X 45" (1 REQD). NAIL TO REAR-OF-LOAD HEADER W/7-12d NAILS.
- ⑧ STRUT, 2" X 6" X CUT FIT BETWEEN REAR-OF-LOAD HEADER AND REAR HEADER (2 REQD). TOENAIL W/2-12d NAILS AT EACH END.
- ⑨ POCKET CLEAT, 2" X 6" X 48" (2 REQD). NAIL TO STRUT W/8-10d NAILS.
- ⑩ DIAGONAL BRACE, 2" X 6" X CUT TO FIT (2 REQD). DOUBLE BEVEL EACH END WITH 45° CUTS. TOENAIL TO REAR-OF-LOAD HEADER AND TO POCKET CLEAT W/2-16d NAILS AT EACH END.
- ⑪ BACK-UP CLEAT, 2" X 6" X 24" (2 REQD). NAIL TO POCKET CLEAT W/6-12d NAILS.
- ⑫ STRUT BRACING, 2" X 4" X TRAILER WIDTH MINUS 3-1/4" (CUT TO FIT) (MIN 1 REQD). POSITION ONE (1) NEAR THE REAR OF THE TRAILER AND ONE (1) EVERY 7'-0" OF STRUT LENGTH. TOENAIL TO STRUTS W/2-12d NAILS AT EACH END.
- ⑬ FLOOR LINE BACK-UP CLEAT, 2" X 6" X 30" (QUADRUPLED) (2 REQD). ALIGN WITH EDGE OF LADING PALLET. NAIL FIRST PIECE TO THE TRAILER FLOOR W/8-12d NAILS. NAIL EACH ADDITIONAL PIECE IN A LIKE MANNER. TOENAIL TOP PIECE TO REAR-OF-LOAD HEADER W/2-12d NAILS. SEE NOTE "●".

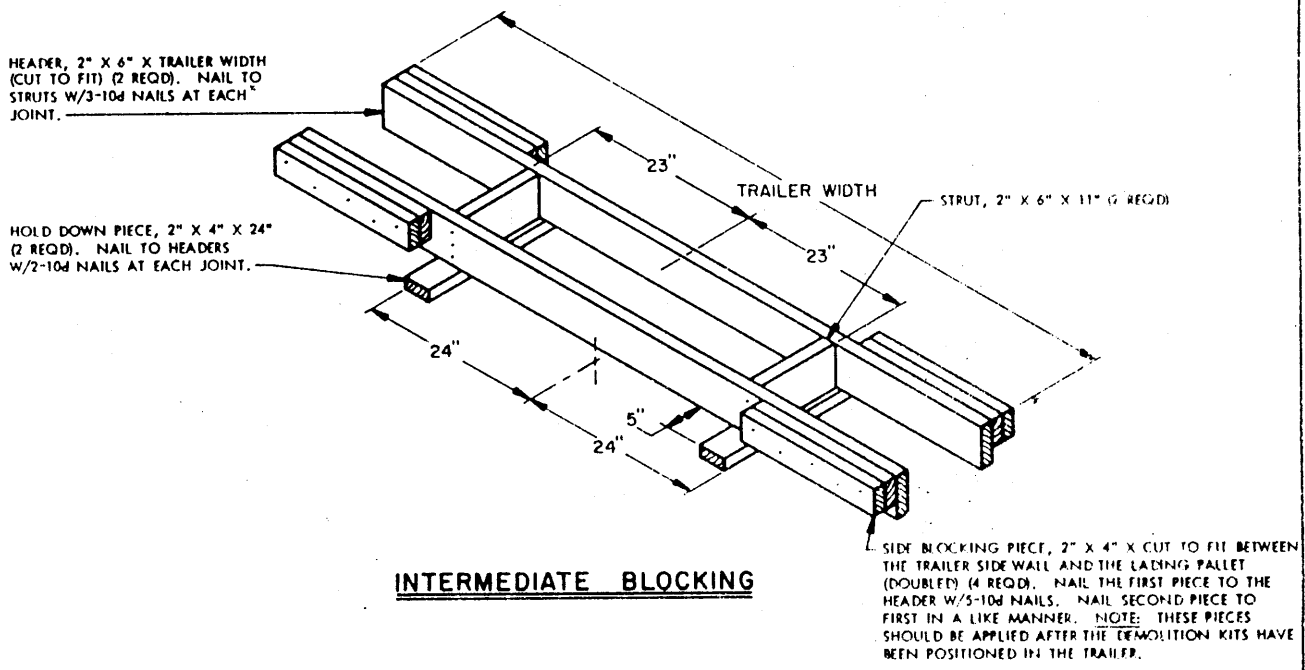
NOTE ●:

WHEN LOADING A TRAILER HAVING A WOOD OR WOOD AND METAL FLOOR. PIECE ⑬ MAY BE USED IN LIEU OF PIECE ④, REAR PIECE ⑤, AND PIECES ⑦ THRU ⑫ WHICH APPLY TO A TRAILER HAVING AN ALL METAL FLOOR. TWO (2) FLOOR LINE BACK-UP CLEATS, SHOWN AS PIECE ⑬, ARE ADEQUATE FOR RETAINING THREE (3) PALLETIZED DEMOLITION KITS.

TYPICAL LTL (1-HIGH LOAD)



FORWARD BLOCKING



INTERMEDIATE BLOCKING

DETAILS (LTL)

VERTICAL STRUT BRACING, 2" X 4" X 48" (2 REQD). POSITION NEAR THE CENTER OF THE STRUT LENGTH AND NAIL TO STRUTS W/3-10d NAILS AT EACH JOINT.

HORIZONTAL STRUT BRACING, 2" X 4" X TRAILER WIDTH (CUT TO FIT) (2 REQD). POSITION NEAR THE CENTER OF THE STRUT LENGTH AND NAIL TO STRUTS W/3-10d NAILS AT EACH JOINT.

STRUT, 4" X 4" X CUT TO FIT (9" MIN) (4 REQD). TOENAIL W/2-16d NAILS AT EACH END.

REAR-OF-LOAD GATE, PIECE MARKED ③ ON PAGES 4 AND 5.

STRUT LEDGER, 2" X 2" X 52" (4 REQD). NAIL TO VERTICAL PIECES OF REAR-OF-LOAD GATE AND REAR GATE TO ALIGN STRUTS WITH THE HORIZONTAL PIECES ON THE GATES.

REAR GATE, PIECE MARKED ③ ON PAGES 4 AND 5.

STRUT BRACING IS REQUIRED ONLY WHEN STRUTS ARE 7'-0" OR GREATER IN LENGTH. ONE (1) SET REQUIRED EVERY 7'-0" FOR STRUTS LONGER THAN 7'-0".

REAR STRUT BLOCKING

THIS BLOCKING IS TO BE USED AT THE REAR OF THE TRAILER WHEN THE EXCESS SPACE BETWEEN REAR-OF-LOAD GATE AND REAR GATE IS GREATER THAN NINE (9) INCHES. IF THE EXCESS SPACE BETWEEN REAR-OF-LOAD GATE AND REAR OF THE TRAILER IS AT LEAST 10'-10" (FOR AN ALL METAL FLOOR) OR 9'-3" (FOR A WOOD OR WOOD AND METAL FLOOR) THE LTL BRACING METHOD OF BLOCKING AS SHOWN ON PAGE 9 MAY BE USED IN LIEU OF THE BLOCKING PROCEDURE SHOWN ABOVE.